



AGENDA

Greater Asheville Regional Airport Authority Regular Meeting
Thursday, November 9, 2017, 8:30 a.m.
Conference Room at Administrative Offices

NOTICE TO THE PUBLIC: The Airport Authority welcomes comments from the public on any agenda item. Comments are received prior to the Board's discussion of the agenda item. Comments are limited to five minutes. If you wish to comment on an agenda item, please deliver a request card (available in the meeting room) to the Clerk to the Board prior to the agenda item being called by the Chair.

- I. CALL TO ORDER
- II. SERVICE AWARD PRESENTATION:
 - A. David Nantz – 20 Years
- III. PRESENTATIONS: None
- IV. FINANCIAL REPORT ([document](#))
- V. CONSENT ITEMS:
 - A. Approval of the Greater Asheville Regional Airport Authority October 6, 2017 Regular Meeting Minutes ([document](#))
 - B. Approval of the Greater Asheville Regional Airport Authority October 6, 2017 Closed Session Minutes Parts A, B and C
 - C. Approval of the Greater Asheville Regional Airport Authority September 8, 2017 Closed Session Minutes
 - D. Ratification on the Delay of New Parking Lot Rates ([document](#))



VI. OLD BUSINESS:

- A. Approval of the Greater Asheville Regional Airport Authority Board 2018 Schedule ([document](#))

VII. NEW BUSINESS:

- A. Presentation of the Annual Audited Financial Report for Fiscal Year 2016/2017 ([document](#))
- B. Approval of Lease Assignment, Assumption and Consent with Signature Flight Support ([document](#))
- C. Approval of Agreement for Professional Consulting Services with CHA Consulting, Inc. ([document](#))
- D. Approval of Change Order Nos. 7 and 8 to Contract with American South General Contractors ([document](#))

VIII. INFORMATION SECTION:

(Staff presentations will not be made on these items. Staff will be available to address questions from the Board.)

- A. September 2017 Traffic Report ([document](#))
- B. September 2017 Monthly Financial Report ([document](#))
- C. November 2017 Development/Project Status Report ([document](#))
- D. Airport Facilities Review for Third Quarter ([document](#))
- E. Potential Board Items for the Next Regular Meeting:
- Adoption of the Five-Year Capital Improvement Plan

IX. PUBLIC AND TENANTS' COMMENTS

X. CALL FOR NEXT MEETING



XI. CLOSED SESSION:

Pursuant to Subsections 143-318.11 (a) (3) and (4) of the General Statutes of North Carolina to Consult with Legal Counsel in Order to Preserve the Attorney-Client Privilege and to Discuss Matters Relating to the Location and/or Expansion of Industries or Other Businesses in the Area Served by the Authority, Including Agreement on a Tentative List of Economic Development Incentives that may be Offered by the Authority in Negotiations.

XII. AUTHORITY MEMBER REPORTS:

A. Key Strategic Elements ([document](#))

XIII. ADJOURNMENT

This agenda of the Greater Asheville Regional Airport Authority is provided as a matter of convenience to the public. It is not the official agenda. Although every effort is made to provide complete and accurate information in this agenda, the Greater Asheville Regional Airport Authority does not warrant or guarantee its accuracy or completeness for any purpose. The agenda is subject to change before and/or during the Board meeting.

**Asheville Regional Airport
Executive Summary
September-17**

AIRPORT ACTIVITY

	Month	Variance to Prior Year	Calendar Year to Date	Variance to Prior Year
Passenger Enplanements	40,051	3.8%	342,782	13.5%
Aircraft Operations				
Commercial	1,870	15.9%	13,776	11.0%
Scheduled Flights	699	15.0%		
Flight Cancellations	15			
Seats	49,743	10.1%	427,575	16.2%
Load Factor	80.5%	(5.8%)	80.2%	(2.3%)
General Aviation	4,051	4.5%	31,507	(4.3%)
Military	564	36.6%	3,523	10.7%

FINANCIAL RESULTS

	Month	Variance to Budget	Fiscal Year to Date	Variance to Budget
Operating Revenues	\$ 1,052,878	17.0%	\$ 3,049,060	10.3%
Operating Expenses	1,008,520	15.7%	2,136,765	(1.1%)
Net Operating Revenues before Depreciation	<u>\$ 44,358</u>	58.3%	<u>\$ 912,295</u>	51.4%
Net Non-Operating Revenues	<u>\$ 279,305</u>	14.6%	<u>\$ 912,565</u>	8.3%
Grants:				
FAA AIP Grants	\$ 162,270		\$ 3,507,334	
NC Dept of Transportation Grants	-		-	
Total	<u>\$ 162,270</u>		<u>\$ 3,507,334</u>	

CASH

Restricted	\$ 13,836,992
Designated for O&M Reserve	4,517,470
Designated for Emergency Repair	650,000
Unrestricted, Undesignated	14,340,412
Total	<u>\$ 33,344,874</u>

RECEIVABLES PAST DUE

	Total	1-30 Days	31-60 Days	Over 60 Days
Advertising Customers	12,667	2,527	6,215	3,925
Allegiant	1,186	497	-	689
American	439	289	-	150
Avis	1,181	254	639	288
Budget	2,507	1,378	183	946
Delta	15,817	15,817	-	-
Enterprise	8,481	2,830	-	5,651
TSA	34,204	17,066	7,109	10,029
FAA	13,037	13,000	-	37
Paradies	3,531	2,819	-	712
Signature	2,000	-	-	2,000
Skywest	294	-	-	294
United	54,150	18,934	-	35,216
Vanguard	4,330	1,533	-	2,797
Miscellaneous	4,681	1,277	-	3,404
Total	<u>\$ 158,505</u>	<u>\$ 78,221</u>	<u>\$ 14,146</u>	<u>\$ 66,138</u>
% of Total Receivables	<u>25.06%</u>			

Note: Excludes balances paid subsequent to month-end.

REVENUE BONDS PAYABLE

	Original Amount	Current Balance
Parking Garage Revenue Bond, Series 2016A	\$ 15,750,000	\$ 15,750,000
Parking Garage Taxable Revenue Bond, Series 2016B	5,250,000	4,295,000
	<u>\$ 21,000,000</u>	<u>\$ 20,045,000</u>

CAPITAL EXPENDITURES

Annual Budget	\$ 44,615,330
Year-to-Date Spending	\$ 7,199,689

**REGULAR MEETING
GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY
October 6, 2017**

The Greater Asheville Regional Airport Authority ("Authority") met on Friday, October 6, 2017 at 9:00 a.m. in the Conference Room at the Authority's Administrative Offices, Asheville Regional Airport ("Airport"), 61 Terminal Drive, Suite 1, Asheville, NC 28732.

MEMBERS PRESENT: Robert C. Roberts, Chair; Matthew C. Burrell, Vice-Chair; Andrew T. Tate; K. Ray Bailey; William L. Moyer; Stephanie Pace Brown; and David Gantt

MEMBERS ABSENT: None

STAFF AND LEGAL COUNSEL PRESENT: Cindy Rice, Authority Legal Counsel; Lew Bleiweis, Executive Director; Michael Reisman, Deputy Executive Director of Development and Operations; Kevan Smith, Chief of Public Safety; Suzie Baker, Director of Administration; Tina Kinsey, Director of Marketing and Public Relations; Janet Burnette, Director of Finance and Accounting; Shane Stockman, IT Director; John Coon, Director of Operations; and Ellen Heywood, Clerk to the Board

ALSO PRESENT: Amira Trebinjevic, Delta Airlines; Tracy Montross, American Airlines; James Seadler, American Airlines; Eric Rysdon, RS&H; Christa Horvath, United Airlines

CALL TO ORDER: The Vice-Chair called the meeting to order at 9:00 a.m.

The Chair called for a moment of silence in remembrance of the victims of the Las Vegas shooting.

FINANCIAL REPORT: The Director reported on the airport activity for the month of August which included enplanements, aircraft operations, and general aviation activity. Janet Burnette reported on the financial activity for the month of August.

CONSENT ITEMS: It was determined that the September 8, 2017 Closed Session Minutes were not available and would be added to the agenda for the November meeting.

A. Approval of the Greater Asheville Regional Airport Authority September 8 2017 Regular Meeting Minutes:

C. Approval of Amended Human Resources Policies and Procedures and the Administration Policies and Procedures:

Mr. Bailey moved to approve Consent Items A and C. Mr. Tate seconded the motion and it carried unanimously.

CLOSED SESSION: At 9:09 a.m. Mr. Moyer moved to go into Closed Session pursuant to Subsection 143-318.11 (a)(7) of the General Statutes of North Carolina to Plan, Conduct or Hear Reports Concerning the Investigations of Alleged Criminal Misconduct. Ms. Brown seconded the motion and it carried unanimously.

Open Session resumed at 9:15 a.m.

OLD BUSINESS:

A. Airline Non-Signatory Fees: The Director stated that over the last two years staff has been trying to negotiate a new lease agreement with the airlines. At the December 2016 Authority Board meeting, rates by ordinance was approved by the Board. At that time, the Board decided to return the non-signatory fees to the airlines retroactive to October 1, 2016. Staff and the airlines negotiated a new rate model which was approved by the Board at the September 8, 2017 Board meeting. The Director advised the Board that the airlines are requesting a refund of the non-signatory fees that were paid from July 1, 2015 until October 1, 2016.

Ms. Tracy Montross of American Airlines thanked the Board for their time. Ms. Montross reviewed American's position on the issue as well as the timeline and details of the negotiations between the airlines and staff. Ms. Montross advised the Board that the total amount of non-signatory fees paid by American Airlines from July, 2015 through October, 2016 was \$212,082.24 and respectfully requested the Board refund the non-signatory fees to American Airlines.

The Chair stated that a letter was received from Mr. Daniel Meier on behalf Allegiant Airlines objecting to the refund of the non-signatory fees to American Airlines, United Airlines, and Delta Airlines, and requested that Mr. Burril read the letter to the Board.

The Director reviewed the non-signatory premiums that were collected from the airlines. The Director advised the Board that it was not uncommon to return non-signatory premiums in a situation such as when airlines come up with an agreement and an airline may be late returning the agreement because it had to go through the airline's legal channels. In discussing this issue with the consultant and others in the industry, the Director was not aware of a situation where non-signatory premiums were returned after an airport board went to an ordinance when an agreement could not be formed.

Ms. Brown inquired if the Authority would be open to legal challenge if the non-signatory fees were not returned to the airlines. Ms. Rice stated that this would be a discussion for closed session.

Mr. Moyer stated the Board has followed procedure and everything was done according to what the Board thought was right. Mr. Moyer heard in the presentation by Ms. Montross that if the Board decides to return the non-signatory premiums, it's because staff and the Board did not follow proper procedures or negotiate in good faith and Mr. Moyer was not willing to concede to that.

Mr. Bailey concurred with Mr. Moyer's comments.

Mr. Gantt stated that there were rules of engagement and that all parties were aware of the rules and the airlines chose not to sign the agreement. Mr. Gantt was in agreement with the procedures the Board took and did not feel the need to backtrack.

Mr. Tate was in agreement with the comments made by Mr. Moyer and Mr. Gantt. The Board supported the process when it started and was not sure why the terms would change at this point.

Ms. Brown stated that from the partner side of the issue, she was not sure what options were available to the airlines during this whole process. The airlines were not in concurrence with the agreement that was presented to them nor were in control of the timeline to resolve those issues. Ms. Brown stated that she does not believe the Board or the Director acted inappropriately, but when there is an outcome that is unsatisfactory to part of the partnership, the question must be asked if they had an influence over that negative outcome.

Mr. Burril stated that while he appreciated Ms. Brown's perspective, he was of the opinion that the airlines were given plenty of time to negotiate on their side of the table. It seemed to him that the timeline was fully engaged by both parties. Mr. Burril stated that it was difficult for him to say that this was a business decision that the Board has caused solely and are responsible for. Mr. Burril stated that while the Board does value and appreciate all of the airlines and are supportive of what they provide to the community, he did not see that there was fault or issue contractually that the Board had erred and created harm.

Ms. Brown remarked that there is a penalty for one party and questioned whether or not there should be a penalty in this process. Ms. Brown was not sure that it admits fault or was done incorrectly, but questioned if it was appropriate for one party to have a penalty for engaging in that process.

The Director informed the Board that the Authority is contractually obligated to pay the non-signatory fees to Allegiant since they were signatory to the agreement. If the Board is looking to repay all of the legacy carriers' non-signatory premiums, the \$240,000 paid to Allegiant could not be taken back and would need to come from the fund balance.

Ms. Brown moved to defer action on this item until a discussion with legal counsel could take place in closed session during that meeting. Mr. Gantt seconded the motion and it carried unanimously.

CLOSED SESSION: At 9:45 a.m. Mr. Gantt moved to go into Closed Session pursuant to Subsection 143-318.11 (a) (3) of the General Statutes of North Carolina to Consult with Legal Counsel in Order to Preserve the Attorney-Client Privilege and to Discuss the Legal Ramifications. Mr. Moyer seconded the motion and it carried unanimously.

Open Session resumed at 9:59 a.m.

Mr. Burril moved to finalize the non-signatory fees and go back to the date of ordinance and what was agreed to at that time. Mr. Moyer seconded the motion and it carried unanimously.

NEW BUSINESS:

A. Approval of the Greater Asheville Regional Airport Authority Board 2018

Schedule: Mr. Moyer inquired if a planning workshop would be held during the year to review or revise the strategic plan and also make plans once the terminal assessment study is complete. The Chair responded that some of those discussions would take place later during the meeting. The Chair further stated that it may be a good opportunity to have a planning session during the March 23, 2018 meeting. The Director remarked that one of the meetings denoted with an asterisk on the schedule could be set aside for a planning session once the terminal assessment study is completed, possibly the July 13, 2018 meeting.

Mr. Gantt stated that he had several conflicts with the schedule that was proposed. The Director stated that this agenda item could be held for discussion in November which would give the Board members time to notify the clerk of any conflicts.

B. Approval of Scope of Services No. 12 to Contract with Delta Airport Consultants, Inc. for Environmental Assessment Services Associated with

South Terminal Apron Expansion: Michael Reisman advised the Board that expansion of the south terminal apron was identified in the 2013 Master Plan. Continued growth of enplanements and flight operations has resulted in the need to move forward with this project. The FAA requires environmental review services take place prior to any design work. Mr. Reisman advised the Board that the cost to move forward with an environmental review would not exceed \$70,000.00 and would be paid from the \$2,026,331.00 of recent state appropriations. The following budget ordinance amendment would be necessary:

BE IT ORDAINED by the Greater Asheville Regional Airport Authority that the following amendment be made to the annual budget ordinance for the fiscal year ending June 30, 2018:

Section 1. To amend the appropriations as follows:

EXPENDITURES:

	<u>Decrease</u>	<u>Increase</u>
Capital Improvement	\$0	\$2,026,331
Totals	<u>\$0</u>	<u>\$2,026,331</u>

This will result in a net increase of \$70,000 in the appropriations. To provide the additional revenue for the above, revenues will be revised as follows:

REVENUES:

	<u>Decrease</u>	<u>Increase</u>
NC DOT Appropriation	<u>\$0</u>	<u>\$2,026,331</u>
Totals	<u>\$0</u>	<u>\$2,026,331</u>

Section 2. Copies of this budget amendment shall be furnished to the Secretary of the Greater Asheville Regional Airport Authority, who for purposes of this ordinance, is designated as the Clerk to the Greater Asheville Regional Airport Authority, and to the Budget Officer and to the Finance Officer for their direction.

Adopted this 6th day of October, 2017.

Robert C. Roberts, Chairman

Attested by:

Ellen Heywood, Clerk to the Board

Mr. Moyer moved to approve Scope of Services No. 12 with Delta Airport Consultants, Inc. for Environmental Review Services, authorize the Executive Director to execute the necessary documents, and to amend the FY17/18 budget by adopting the budget ordinance amendment as presented by staff. Mr. Bailey seconded the motion and it carried unanimously.

DIRECTOR'S REPORT: The Director advised the Board that he had a couple of additional items to include that were not on the agenda.

A. FAA Grant: The Director informed the Board that the first grant that has been executed solely by the Greater Asheville Regional Airport Authority was received at the end of September. The grant amount was for \$7.8 million.

B. TSA: The Director reported that TSA has begun enhanced security screening procedures for electronic devices larger than a cell phone. The new procedures were implemented the prior week and as a result, there have been issues with long lines at the security checkpoint. TSA has assured staff that once passengers become familiar with the new procedures, the security process will move more quickly. TSA is aware of the issues. Staff has had discussions with airline staff about the possibility of setting up a line for premium flyers that would feed into the document checking station. The frequent flyers would not have to wait in as long a line. Staff has broached the idea of the airlines hiring a contractor to operate this line. Staff has also had discussions with TSA about a third lane that could be used for a pre-check lane. TSA has criteria the airport would need to meet in terms of number of passengers to allow for a pre-check lane to be created. Staff has requested the airlines gather information on premium lanes or ideas from other airports to resolve the issue for frequent travelers. The Director reported that staff is also trying to collaborate with the airlines to go to Washington, DC to meet with TSA regarding a third lane. Staff will keep the Board apprised.

C. ACI-NA: The Director advised the Board that at the recent annual conference for Airports Council International, he was elected as Vice Chair of the organization's Board.

PRESENTATIONS:

A. Department of Public Safety: Chief Kevan Smith gave a presentation to the Board on the Department of Public Safety that included the functions covered by the department as well as the numerous training requirements involved. Chief Smith highlighted some of the additional certifications obtained by staff as well as industry and community involvement. The Board thanked Chief Smith for all that he and his staff do at the airport.

DISCUSSION OF KEY PRIORITIES:

The Chair stated that all of the topics included under this item on the agenda could not be covered during the meeting, so the Board would use future meetings to cover the remaining topics.

A. Air Service Development: Tina Kinsey provided basic air service development information that highlighted the airport's top 25 market for origin and destination, air service goals relating to retention, growth and engagement, data on how the airport benchmarks with regional peers, as well as local and industry challenges.

Mrs. Kinsey explained how staff handles retaining existing service as well as seeking new air service. Mr. Moyer inquired if the airport's existing facility hampers air service growth. The Director responded that at the present time, the answer was no, but the airport is getting close. Mr. Moyer also asked if consultants serve a role in attracting new air service. Mrs. Kinsey responded that staff has a consultant on retainer that is communicated with weekly to analyze data and discuss potential new air service as well as retention of existing service.

Mrs. Kinsey described how the airport uses data to benchmark with regional peers. Ms. Brown commented that from a Board perspective, the Board needs to stay on top of understanding the trends for other markets such as Charleston and Savannah. The Chair stated while the airport is doing well, the Board will have to figure out how to leverage that and do even better. Ms. Brown suggested that the Board may need to focus on air service development as a priority in the strategic plan.

Mr. Moyer questioned if the equestrian games in Tryon were a part of the planning. Mrs. Kinsey stated that all of the airlines are aware of this event and will add seats to the market during this time.

The Chair suggested the Board digest the information received and discuss further at the next meeting.

INFORMATION SECTION: No comments

PUBLIC AND TENANTS COMMENTS: None

CALL FOR NEXT MEETING: The next regular meeting of the Authority Board will be held on November 9, 2017.

AUTHORITY MEMBER REPORTS: The Chair announced that Mr. Tate had accepted a new position and would no longer be able to serve on the Board.

Mr. Tate left the meeting at 11:30 a.m.

CLOSED SESSION: At 11:30 a.m. Mr. Burril moved to go into Closed Session Pursuant to Subsections 143-318.11 (a) (3) and (4) of the General Statutes of North Carolina to Consult with Legal Counsel Regarding, Among Other Things, That Lawsuit Entitled Tricor Construction, Inc. vs. RS&H Architects-Engineers-Planners, Inc., Thalle Construction Co., Inc. and Liberty Mutual Insurance Company and vs. Defendant & Third-Party Plaintiff, Greater Asheville Regional Airport Authority, vs. Third-Party Defendant, Avcon, Inc. d/b/a Avcon Engineers and Planners, Inc. and that Informal Part 13 Complaint Filed with the Federal Aviation Administration on or about August 28, 2017 by the Aircraft Owners and Pilots Association, in Order to Preserve the Attorney-Client Privilege, and to Discuss Matters Relating to the Location and/or Expansion of Industries or Other Businesses in the Area Served by the Greater Asheville Regional Airport Authority, Including Agreement on a Tentative List of Economic Development Incentives that may be Offered by the Greater Asheville Regional Airport Authority in Negotiations. Mr. Moyer seconded the motion and it carried by a 6 to 0 vote.

Open Session resumed at 12:10 p.m.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY OCTOBER 6, 2017

CLOSED SESSION MINUTES: Mr. Burril moved to seal the minutes for the Closed Sessions Parts A, B, and C just completed and to withhold such Closed Session minutes from public inspection so long as public inspection would frustrate the purpose or purposes thereof. Mr. Bailey seconded the motion and it carried by a 6 to 0 vote.

ADJOURNMENT: Mr. Moyer moved to adjourn the meeting at 12:12 p.m. Ms. Brown seconded the motion and it carried by a 6 to 0.

Respectfully submitted,

Ellen Heywood
Clerk to the Board

Approved:

Robert C. Roberts
Chair



MEMORANDUM

TO: Members of the Airport Authority

FROM: Lew Bleiweis, A.A.E., Executive Director

DATE: November 9, 2017

ITEM DESCRIPTION – Consent Item D

Ratification on the Delay of New Parking Lot Rates

BACKGROUND

At the September 2017 Board Meeting, the Board approved an increase in the parking lot rates through the Supplemental Fees and Charges section of the annual budget. This rate increase was scheduled to go into effect November 1, 2017, in preparation with the opening of the new parking garage facility. The new parking lot revenue equipment will not be installed in the parking facilities, including the garage for several more weeks. LAZ Parking, the Authority's new parking operator took over the management of the facilities November 1, 2017. In the best interest of the Authority, the traveling public, and LAZ Parking, staff decided to hold off on the parking rate increase until all the new revenue equipment is installed. We anticipate the increase to be implanted by the middle of December.

ISSUES

After consulting with the Authority's attorney, the Executive Director thought it would be best to have the Board approve the decision to postpone the implementation of the parking rate increase. Since the board meeting in November is after the 1st, the Executive Director postponed the implementation of the rate increase, and is seeking ratification of that action by the Board.

ALTERNATIVES

None

Consent – Item D



FISCAL IMPACT

Since the current Fiscal Budget did not include an annual increase in revenues based on a rate increase, there is little fiscal impact by this decision.

RECOMMENDED ACTION

It is respectfully requested that the Greater Asheville Regional Airport Authority Board resolve to ratify the delay in implementing the parking rate increase until such time that all the new parking revenue equipment is installed and operational.



MEMORANDUM

TO: Members of the Airport Authority
FROM: Lew Bleiweis, A.A.E., Executive Director
DATE: November 9, 2017

ITEM DESCRIPTION – Old Business Item A

Approval of the Greater Asheville Regional Airport Authority Board 2018 Schedule

Below are the proposed dates for the 2018 Board Meetings and Budget Workshop/Budget Approval Meeting to note on your calendars:

*January 19	*July 13
February 16	August 10
March 9	*September 7
Friday, March 23	October 12
April 13	*November 9
May 18	December 14
June 8	

Friday, March 23, 2018 – Board Budget Workshop/Budget Approval

*At this point, it is not anticipated that a meeting will be required on these dates. However, please leave your schedules open until determination is made.



MEMORANDUM

TO: Members of the Airport Authority

FROM: Janet Burnette, Director of Finance and Accounting

DATE: November 9, 2017

ITEM DESCRIPTION – New Business Item A

Presentation of the Annual Audited Financial Report for Fiscal Year 2016/2017

BACKGROUND

The annual audit for the fiscal year ended June 30, 2017 was performed by the auditing firm, Gould Killian CPA Group, P.A., and the findings are hereby submitted for the Board's review and acceptance. The audited financial statements being provided to you have been submitted to the Local Government Commission ("LGC").

ISSUES

None. An unmodified opinion was issued by the auditors.

RECOMMENDED ACTION

It is respectfully requested that the Greater Asheville Regional Airport Authority Board resolve to accept the 2016/2017 Audit Report as presented.



**GOULD KILLIAN
CPA GROUP, P.A.**

CERTIFIED PUBLIC ACCOUNTANTS

**GREATER ASHEVILLE REGIONAL
AIRPORT AUTHORITY**

2017 Annual Financial Report

Audit Summary

- Thank you for allowing us to serve as your auditors
- We issued an unmodified (clean) opinion; reasonable assurance that the financial statements are free from material misstatement
- We reported no material weaknesses in internal control
- We reported no material noncompliance with laws and regulations
- Special thanks to Janet and the staff of the finance department for their hard work and cooperation in completing the audit



Recently Issued Accounting Pronouncements

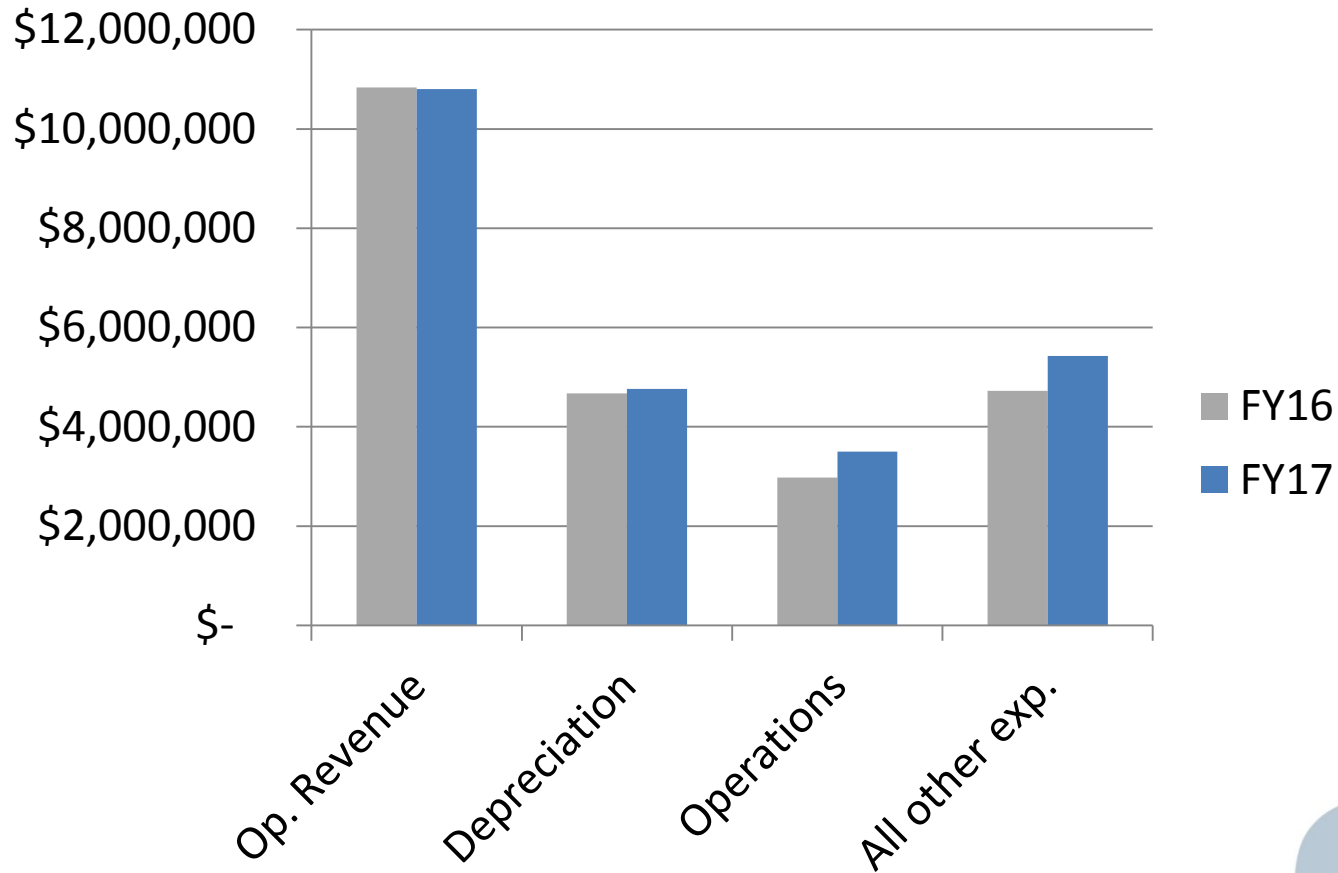
- GASB 73 – Implementation requires the Authority to record total pension liability and the effects on net position of the Law Enforcement Officers' Special Separation Allowance during the fiscal year.
 - Total pension liability at June 30, 2017 was approximately \$440,000
 - Restatement decreased net position by approximately \$400,000 at June 30, 2016
- GASB 75 - New standards require the unfunded OPEB liability to be recognized on statement of net position and OPEB expense to be recognized on statement of activities. In prior years, this information was reported in the footnotes and RSI. GASB 75 will be implemented in FY18.
 - At June 30, 2017 the net OPEB liability reported was \$1.1 million compared to the unfunded liability of \$1 million.

Key Statistics for FY17

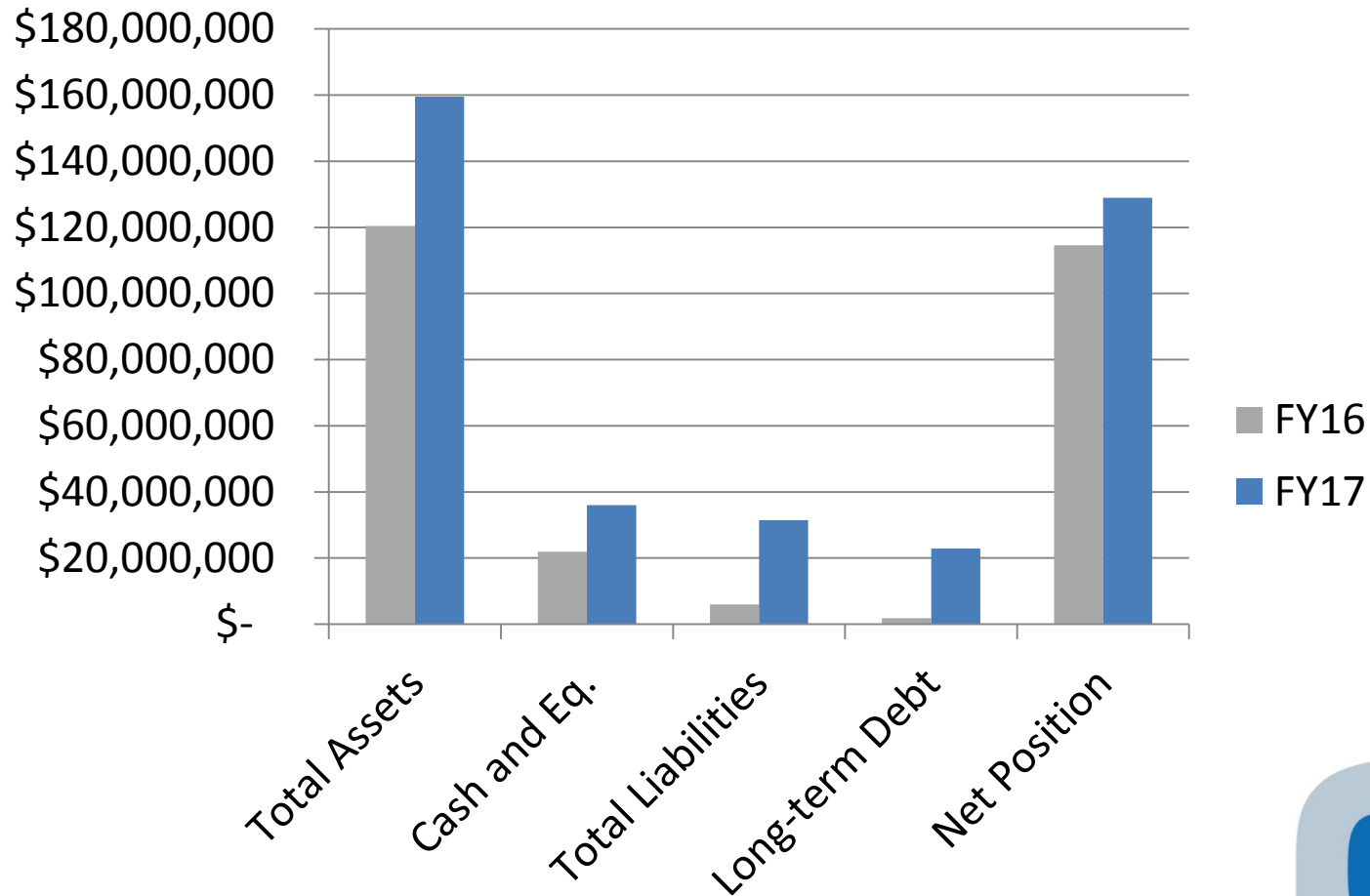
- Cash and investments totaled \$36.0 million at 06/30/2017
- Net Position for FY17 is \$128.9 million (increase of \$14.3 million)
 - Of the net position, \$14.9 million is unrestricted (decrease of \$3.0 million)
 - \$15.7 million is restricted (increase of \$10.6 million)
 - The remaining increase was attributable to investment in capital assets
- Operating loss of \$2.8 million in FY2017 (compared to \$1.5 million in FY16)
- Depreciation expense - \$4.7 million
- Debt service - \$207K
- Non-operating revenues (gross) - \$2.9 million (decrease of \$50k)



Revenues and Expenses



Balance Sheet Summary



Key Comparisons Summary

	FY16	FY17
Working capital	\$ 19,653,648	\$ 14,683,026
Working capital in months operating expenses (excl. depreciation)	30.8 months	19.7 months
Unrestricted net position	\$ 17,914,377	\$ 14,946,409
Operating loss	\$ (1,542,589)	\$ (2,882,452)
Change in net position	\$ 16,745,697	\$ 14,284,352
Cash flows from operating activities	\$ 3,359,335	\$ 1,572,053
Net increase in cash	\$ (367,669)	\$ 14,035,876



Capital Projects

- Capital Assets (gross) increased by \$32.5 million in FY2017
 - This increase is primarily due to major capital projects in progress for the Airfield Re-Development and Parking Garage
 - The Authority has remaining commitments of approximately \$39.5 million for construction/renovation of facilities
 - Funded through Federal grants, passenger facility charges, Airport funds, and Airport Revenue Bonds





**GOULD KILLIAN
CPA GROUP, P.A.**

CERTIFIED PUBLIC ACCOUNTANTS

**GREATER ASHEVILLE REGIONAL
AIRPORT AUTHORITY**

2017 Annual Financial Report

**Greater Asheville Regional
Airport Authority
Fletcher, North Carolina**

Annual Financial Report

For The Years Ended June 30, 2017 and 2016



**GOULD KILLIAN
CPA GROUP, P.A.**
CERTIFIED PUBLIC ACCOUNTANTS

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

Board of Directors

Robert C. Roberts, Chair
Matthew Burrell, Vice-Chair

Andrew T. Tate
William L. Moyer
K. Ray Bailey
Stephanie P. Brown
David Gant
Cindy Rice, Attorney

Executive Director

Lew S. Bleiweis, A.A.E.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

ANNUAL FINANCIAL REPORT

June 30, 2017 and 2016

TABLE OF CONTENTS

	<u>Page</u>
Independent Auditors' Report	1 - 3
Management's Discussion and Analysis	4 - 10
Basic Financial Statements:	<u>Exhibit</u>
Statements of Net Position	1 11 - 12
Statements of Revenues, Expenses, and Changes in Net Position	2 13
Statements of Cash Flows	3 14 - 15
Notes to the Financial Statements	16 - 37
Required Supplementary Information:	
Schedule of Changes in Total Pension Liability and Schedule of Total Pension Liability as a Percentage of Covered Payroll - Law Enforcement Officers' Special Separation Allowance	38
Schedule of Funding Progress and Schedule of Employer Contributions - Other Post-Employment Benefits - Retiree Health Plan	39
Schedule of the Proportionate Share of Net Pension Liability (Asset) - Local Government Employees' Retirement System	40
Schedule of Contributions - Local Government Employees' Retirement System	41
Supplementary Information:	
Schedule of Revenues and Expenditures - Budget and Actual (Non-GAAP)	42 - 43
Capital Improvements Supplemental Schedule - Schedule of Revenues and Expenditures - Budget and Actual (Non-GAAP)	44

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

ANNUAL FINANCIAL REPORT

June 30, 2017 and 2016

TABLE OF CONTENTS

	<u>Page</u>
Compliance:	
Report On Internal Control Over Financial Reporting And On Compliance And Other Matters Based On An Audit Of Financial Statements Performed In Accordance With Government Auditing Standards	45 - 46
Report On Compliance For Each Major Federal Program And Passenger Facility Charge (PFC) Program And On Internal Control Over Compliance; In Accordance With Uniform Guidance, The PFC Program Audit Compliance Guide, And The State Single Audit Implementation Act	47 - 48
Schedule of Findings and Responses	49
Summary Schedule of Prior Year's Audit Findings	50
Schedule of Expenditures of Federal Awards	51



**GOULD KILLIAN
CPA GROUP, P.A.**
CERTIFIED PUBLIC ACCOUNTANTS

INDEPENDENT AUDITORS' REPORT

To the Board of Directors
Greater Asheville Regional Airport Authority
Fletcher, North Carolina

Report on the Financial Statements

We have audited the accompanying financial statements of the Greater Asheville Regional Airport Authority which comprise the statements of net position as of June 30, 2017 and 2016, and the related statements of revenues, expenses, and changes in net position and cash flows for the years then ended, and the related notes to the financial statements.

Management's Responsibility for the Financial Statements

Management is responsible for the preparation and fair presentation of these financial statements in accordance with accounting principles generally accepted in the United States of America; this includes the design, implementation, and maintenance of internal control relevant to the preparation and fair presentation of financial statements that are free from material misstatement, whether due to fraud or error.

Auditor's Responsibility

Our responsibility is to express an opinion on these financial statements based on our audit. We conducted our audit in accordance with auditing standards generally accepted in the United States of America and the standards applicable to financial audits contained in *Government Auditing Standards*, issued by the Comptroller General of the United States. Those standards require that we plan and perform the audit to obtain reasonable assurance about whether the financial statements are free of material misstatement.

An audit involves performing procedures to obtain audit evidence about the amounts and disclosures in the financial statements. The procedures selected depend on the auditor's judgment, including the assessment of the risks of material misstatement of the financial statements, whether due to fraud or error. In making those risk assessments, the auditor considers internal control relevant to the entity's preparation and fair presentation of the financial statements in order to design audit procedures that are appropriate in the circumstances, but not for the purpose of expressing an opinion on the effectiveness of the entity's internal control. Accordingly, we express no such opinion. An audit also includes evaluating the appropriateness of accounting policies used and the reasonableness of significant accounting estimates made by management, as well as evaluating the overall presentation of the financial statements.

We believe that the audit evidence we have obtained is sufficient and appropriate to provide a basis for our audit opinion.

Opinion

In our opinion the financial statements referred to above present fairly, in all material respects, the financial position of the Greater Asheville Regional Airport Authority as of June 30, 2017 and 2016, and the changes in financial position and cash flows for the years then ended in accordance with accounting principles generally accepted in the United States of America.

Other Matters

Required Supplementary Information

Accounting principles generally accepted in the United States of America require that Management's Discussion and Analysis on pages 4 – 10, the Law Enforcement Officers' Special Separation Allowance Schedules of Changes in Total Pension Liability and Total Pension Liability as a Percentage of Covered Payroll on page 38, the Other Post-Employment Benefits' Schedules of Funding Progress and Employer Contributions on page 39, and the Local Government Employees' Retirement System's Schedules of the Proportionate Share of the Net Pension Liability (Asset) and Contributions on pages 40 – 41, be presented to supplement the basic financial statements. Such information, although not a part of the basic financial statements, is required by the Governmental Accounting Standards Board who considers it to be an essential part of the financial reporting for placing the basic financial statements in an appropriate operational, economic, or historical context. We have applied certain limited procedures to the required supplementary information in accordance with auditing standards generally accepted in the United States of America, which consisted of inquiries of management about the methods of preparing the information and comparing the information for consistency with management's responses to our inquiries, the basic financial statements, and other knowledge we obtained during our audit of the basic financial statements. We do not express an opinion or provide any assurance on the information because the limited procedures do not provide us with sufficient evidence to express an opinion or provide any assurance.

Supplementary and Other Information

Our audit was conducted for the purpose of forming an opinion on the financial statements that collectively comprise the Greater Asheville Regional Airport Authority's basic financial statements. The budgetary schedules, other schedules, as well as the accompanying Schedule of Expenditures of Federal Awards as required by *Title 2 U.S. Code of Federal Regulations (CFR) Part 200*, Uniform Administrative Requirements, Cost Principles and Audit Requirements for Federal Awards; the Passenger Facility Charge Audit Guide for Public Agencies; and the State Single Audit Implementation Act are presented for purposes of additional analysis and are not a required part of the basic financial statements.

The budgetary schedules and the Schedule of Expenditures of Federal Awards are the responsibility of management and were derived from, and relate directly to the underlying accounting and other records used to prepare the basic financial statements. Such information has been subjected to the auditing procedures applied in the audit of the basic financial statements and certain additional procedures; including comparing and reconciling such information directly to the underlying accounting and other records used to prepare the basic financial statements, or to the basic financial statements themselves, and other additional procedures in accordance with auditing standards generally accepted in the United States of America. In our opinion, the budgetary schedules and the Schedule of Expenditures of Federal Awards are fairly stated, in all material respects, in relation to the basic financial statements as a whole.

Other Reporting Required by *Government Auditing Standards*

In accordance with *Government Auditing Standards*, we have also issued our report dated October 31, 2017 on our consideration of the Greater Asheville Regional Airport Authority's internal control over financial reporting and on our tests of its compliance with certain provisions of laws, regulations, contracts, grant agreements, and other matters. The purpose of that report is to describe the scope of our testing of internal control over financial reporting and compliance and the results of that testing, and not to provide an opinion on internal control over financial reporting or on compliance. That report is an integral part of an audit performed in accordance with *Government Auditing Standards* in considering the Greater Asheville Regional Airport Authority's internal control over financial reporting and compliance.

Donald Killian CPA Group, P.A.

Asheville, North Carolina
October 31, 2017

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

MANAGEMENT'S DISCUSSION AND ANALYSIS

The following Management's Discussion and Analysis (the "MD&A") of the Greater Asheville Regional Airport Authority's activities and financial performance provides the reader with an overview to the financial statements of the Greater Asheville Regional Airport Authority for the fiscal year ended June 30, 2017. The information contained in this MD&A should be considered in conjunction with the financial information contained in the various sections of this audit report.

THE REPORTING ENTITY

The Asheville Regional Airport Authority was established in 1980 by the provisions of Article 20 of Chapter 160A of the General Statutes of North Carolina and by the Agreement of November 29, 1979 by and between the County of Buncombe (the "County") and the City of Asheville (the "City"). It was organized for, and has as its sole purpose, the management, operation, and maintenance of the Asheville Regional Airport (the "Airport"). On June 28, 2012, the General Assembly of North Carolina passed Session Law 2012-121 which changed the structure of the entity to an independent airport authority with more regional representation and governance. The law also changed the official name to the Greater Asheville Regional Airport Authority (the "Authority"). Pursuant to the State statute, the agreement with Buncombe County and the City of Asheville is no longer applicable.

The Authority operates as an Enterprise Fund and is governed by seven members: two registered voters of the County, appointed by the Board of Commissioners of Buncombe County; two registered voters of the City, appointed by the Asheville City Council; two registered voters of the County of Henderson, appointed by the Board of Commissioners of Henderson County; and one member appointed by the other six members of the Authority. Members of the Authority serve four-year terms. Any member may serve a total of two consecutive terms, after which said member may not be reappointed to the Authority until four years after his or her most recent appointment.

The Authority employs a managing director (the "Executive Director"), who is the chief administrator and executive officer of the Authority. The Executive Director manages the Airport under the Authority's control with a staff of 64 full-time employees. The staff is responsible for the day-to-day financial, administration, and operational matters pertaining to the Airport and for the contractual arrangements with various aeronautical and non-aeronautical businesses at the Airport.

The Airport is a small hub airport engaged in the business of facilitating commercial and general aviation passenger, cargo, and mail transportation and is the 132nd largest airport in the United States based on the number of passenger boardings during calendar year 2016.¹

As of June 30, 2017, Asheville Regional Airport had approximately twenty-four daily, non-stop commercial flights to eleven cities (Atlanta, Baltimore, Charlotte, Chicago, Fort Lauderdale, New York, Newark, Orlando, Punta Gorda, St. Petersburg/Clearwater and Vero Beach). The commercial airline carriers included Allegiant Air, American Airlines, Delta Air Lines, Elite Airlines and United Airlines.

The mission of the Greater Asheville Regional Airport Authority is to provide an exceptional airport experience with a focus on people, service, commercial and general aviation, and to contribute to the regional economy.

¹ Federal Aviation Administration, passenger boardings calendar year 2016

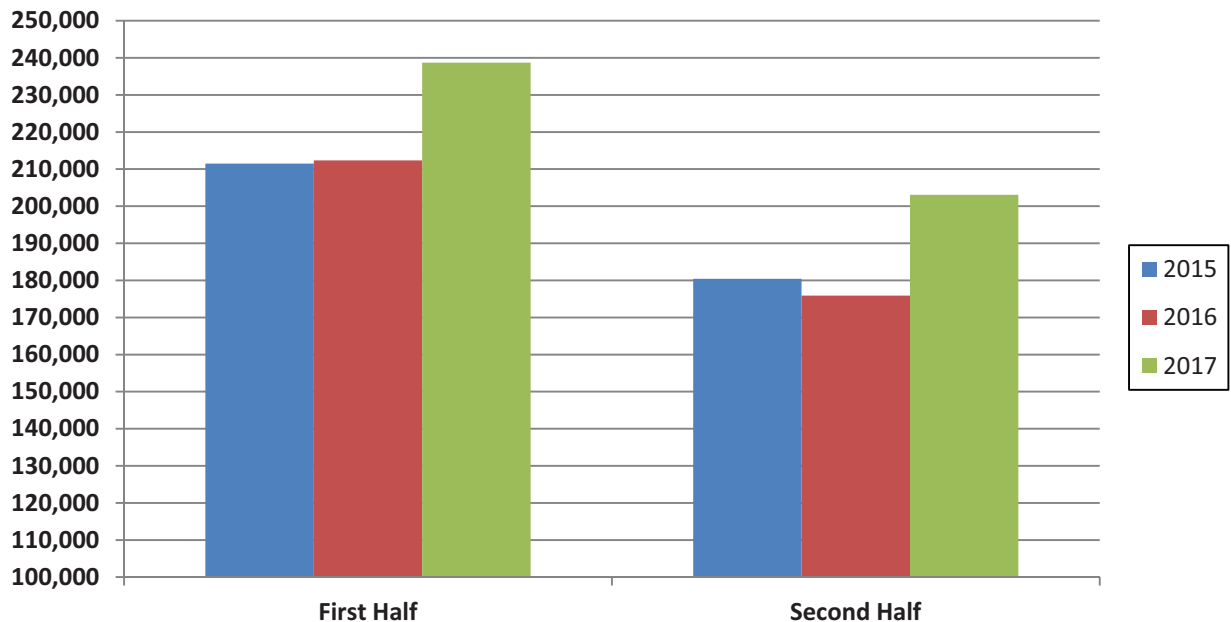
AIRPORT ACTIVITIES AND HIGHLIGHTS

<u>For Year Ended June 30</u>	<u>2017</u>	<u>2016</u>	<u>2015</u>
Enplanements	441,769	388,259	391,906
Revenues per enplanement:			
Airlines	\$ 6.24	\$ 6.44	\$ 5.88
Rental cars	5.17	5.84	5.38
Parking facility	8.04	8.49	8.07
Concessionaires	1.24	1.26	1.10

Enplanements increased by 13.78% in fiscal year 2017.

	<u>2017</u>	<u>2016</u>	<u>2015</u>
<i>Aircraft Movements (Land or Takeoff):</i>			
Airline	17,014	16,475	15,718
General aviation	41,516	44,458	42,432
Military	4,280	4,789	6,723
Total	<u>62,810</u>	<u>65,722</u>	<u>64,873</u>

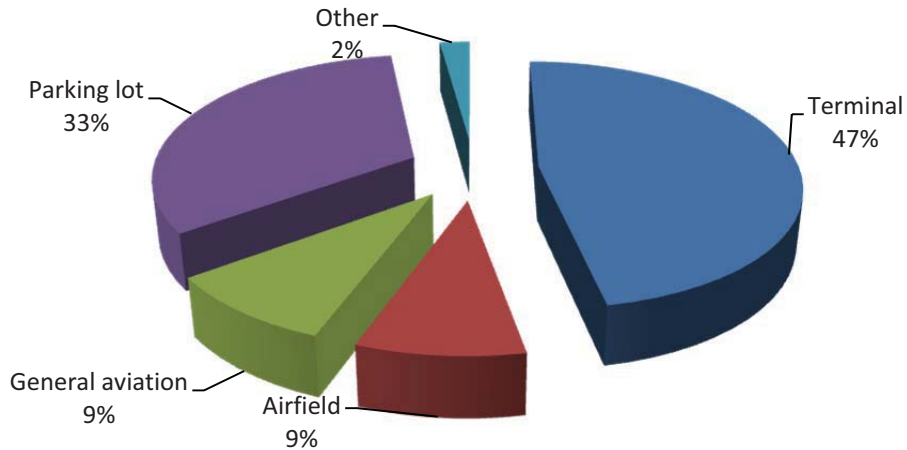
Passenger Enplanements for Fiscal Year



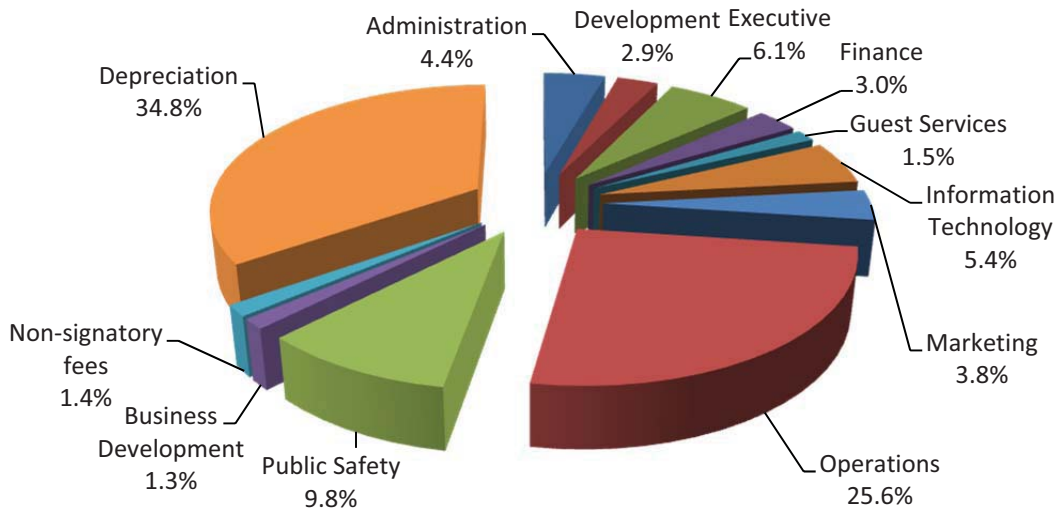
SUMMARY OF OPERATIONS AND CHANGES IN NET POSITION

For Year Ended June 30	2017	(restated) 2016	2015
Operating revenues	\$ 10,804,627	\$ 10,832,701	\$ 10,075,571
Operating expenses	13,687,079	12,375,290	11,787,981
Operating loss	(2,882,452)	(1,542,589)	(1,712,410)
Non-operating revenues and expenses, net	2,925,683	2,976,235	2,802,657
Income before capital contributions	43,231	1,433,646	1,090,247
Capital contributions	14,284,352	15,312,051	12,020,590
Increase in net position	<u>\$ 14,327,583</u>	<u>\$ 16,745,697</u>	<u>\$ 13,110,837</u>

Operating Revenues



Operating Expenses



FINANCIAL POSITION SUMMARY

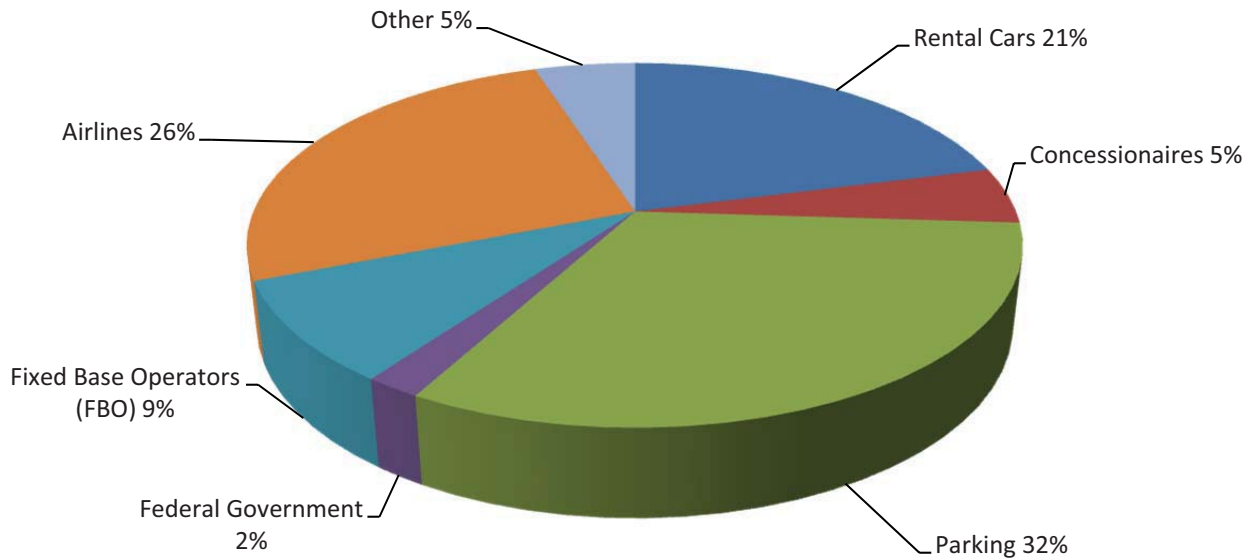
Net position may serve, over time, as a useful indicator of the Authority's financial position. The Authority's assets and deferred outflows of resources exceeded liabilities and deferred inflows of resources by \$128,928,585 at June 30, 2017, an increase of approximately \$14.3 million from June 30, 2016, and roughly a \$31.1 million increase from June 30, 2015.

	<u>2017</u>	<u>(restated) 2016</u>	<u>(restated) 2015</u>
Assets and Deferred Outflows of Resources:			
Current assets	\$ 23,318,849	\$ 23,754,888	\$ 23,902,251
Restricted assets	16,988,875	5,214,500	3,041,307
Capital assets	119,204,501	91,472,125	77,151,244
Other assets	-	-	286,204
Deferred outflows of resources	963,484	253,159	204,511
Total assets and deferred outflows of resources	<u>160,475,709</u>	<u>120,694,672</u>	<u>104,585,517</u>
Liabilities and Deferred Inflows of Resources:			
Current liabilities	8,635,823	4,101,240	3,482,341
Non-current liabilities	22,851,278	1,855,905	2,525,082
Deferred inflows of resources	60,023	136,525	722,789
Total liabilities and deferred inflows of resources	<u>31,547,124</u>	<u>6,093,670</u>	<u>6,730,212</u>
Net Position:			
Net investment in capital assets	98,204,501	91,472,125	75,561,248
Restricted	15,777,675	5,214,500	3,041,307
Unrestricted	14,946,409	17,914,377	19,252,750
	<u>\$ 128,928,585</u>	<u>\$ 114,601,002</u>	<u>\$ 97,855,305</u>

REVENUES

The daily operations of the Asheville Regional Airport are funded through the collection of user fees such as parking receipts, rental car privilege fees, landing fees, space rental, and concessions fees.

The following chart shows the major sources and the percentage of operating revenues for the year ended June 30, 2017.



SUMMARY OF CASH FLOW ACTIVITIES

The following is a summary of the major sources and uses of cash and cash equivalents. Cash equivalents are considered cash-on-hand, bank deposits, and highly liquid investments with an original maturity of three months or less.

For Year Ended June 30	2017	2016	2015
Cash flows from operating activities	\$ 1,572,053	\$ 3,359,335	\$ 2,281,659
Cash flows from capital and related financing activities	12,373,281	(3,762,764)	578,739
Cash flows from investing activities	90,542	35,760	22,959
Net increase (decrease) in cash and cash equivalents	14,035,876	(367,669)	2,883,357
Cash and Cash Equivalents			
Beginning of year - July 1	21,927,144	22,294,813	19,411,456
End of year - June 30	<u>\$ 35,963,020</u>	<u>\$ 21,927,144</u>	<u>\$ 22,294,813</u>

CAPITAL ASSETS AND DEBT ADMINISTRATION

Capital Assets

During fiscal year 2017, the Authority expended \$32.5 million on capital activities. This included the following major projects:

- \$17.2 million on the Airfield Redevelopment
- \$14.1 million on the Parking Garage

Acquisitions are funded using a variety of sources, including Federal and State grants, passenger facility charges, operating revenues, and net position appropriations.

CAPITAL ASSETS (net of accumulated depreciation)

	<u>2017</u>	<u>(restated) 2016</u>	<u>2015</u>
Land	\$ 7,593,744	\$ 7,593,744	\$ 7,217,951
Construction in progress	61,456,158	30,018,931	17,223,915
Buildings and improvements	47,732,909	51,902,967	50,931,221
Equipment	980,602	1,028,709	819,762
Furniture	114,611	97,263	93,739
Vehicles	1,326,477	830,511	864,656
Capital assets, net of accumulated depreciation	<u>\$ 119,204,501</u>	<u>\$ 91,472,125</u>	<u>\$ 77,151,244</u>

Long-Term Debt

As of June 30, 2017, the Authority issued \$21,000,000 in Airport System Revenue Bonds.

LONG-TERM DEBT

	<u>2017</u>	<u>2016</u>	<u>2015</u>
Rental Car Facilities Taxable Revenue Bonds	\$ -	\$ -	\$ 1,589,996
Greater Asheville Regional Airport Authority System Revenue Bonds	21,000,000	-	-
	<u>\$ 21,000,000</u>	<u>\$ -</u>	<u>\$ 1,589,996</u>

RECENT DEVELOPMENTS

Commercial aviation continued to have solid growth in 2016. The economy continued to expand and jet fuel prices have seen little increase, thereby, providing net profits for the airlines. U.S. commercial air carriers made a profit of \$13.5 billion in 2016. The airlines collected approximately \$7.1 billion in checked bag fees and reservation change fees. In 2016, commercial air carrier domestic enplanements

increased 3.3%, and total airline enplanements increased 5.5%. Carriers set an all-time high of 932 million system-wide scheduled service passengers.

Air carriers continue to focus on increasing shareholder return. The carriers continue to adjust capacity to seize opportunities and contracting in response to economic times. These changes have resulted in profitability for the seventh year in a row. The airlines continue to be reserved with expansion in 2017 to keep airfares stimulated.

THE FUTURE

Authority management continues to seek additional non-stop air service to the top 25 origin and destination markets at the Airport, focuses attention on airline schedule improvements, and continues to foster positive working relationships and communication with all airline partners. Allegiant Travel Company continues to grow its presence at the Airport, with its first non-stop destination to New York City through Newark Liberty International Airport. Allegiant continues to maintain an aircraft base in Asheville with 40 crew and maintenance personnel residing in the community. American Airlines added early morning departures making west coast travel more convenient and United Airlines added a mainline flight to Newark.

The Airport management is striving to increase non-aeronautical revenues to sustain long-term aviation growth at the airport. They are working diligently with both Buncombe County and Henderson County Economic Development departments to increase development opportunities at the airport.

The Authority began the 4th year of its airfield redevelopment project, which includes the rehabilitation of its runway and the construction of a new parallel taxiway. Capital funding continues to be a challenge for airports nationwide. However, the Authority was successful in obtaining an additional \$10 million Discretionary Grant for the Airfield Redevelopment project. FAA Funding Reauthorization was extended through March 2018. Airports and their trade associations continue to advocate for the next long-term funding bill. A key focus has been to increase the Passenger Facility Charge (PFC), a user fee that supports local capital development, to \$8.50 from the current \$4.50. This fee has not been increased since 2000 when Congress capped the fee at \$4.50.

Management will continue to improve the Airport and its facilities in a financially prudent manner, maximizing Federal, State, and other revenues to minimize the Authority's internal funding commitment, thereby maximizing the Authority's return on its investments.

REQUESTS FOR INFORMATION

This report is designed to provide an overview of the Authority's finances for those with an interest in this area. Questions concerning any of the information found in this report, or requests for additional information, should be directed to the Director of Finance and Accounting, Greater Asheville Regional Airport Authority, 61 Terminal Drive, Suite 1, Fletcher, NC 28732. You may also call (828) 684-2226, visit our website www.flyavl.com, or send an email to pr@flyavl.com for more information.

BASIC FINANCIAL STATEMENTS

STATEMENTS OF NET POSITION
June 30, 2017 and 2016

	2017	Restated 2016
Assets		
Current assets:		
Unrestricted assets:		
Cash and cash equivalents	\$ 18,974,145	\$ 16,712,644
Accounts receivable	1,627,600	903,798
Grants receivable	2,413,698	5,872,896
Passenger facility charges receivable	294,322	262,018
Prepaid expenses	9,084	3,532
Total current assets	<u>23,318,849</u>	<u>23,754,888</u>
Restricted assets:		
Cash and cash equivalents	16,988,875	5,214,500
Total restricted assets	<u>16,988,875</u>	<u>5,214,500</u>
Non-current assets:		
Non-depreciable capital assets	69,049,902	37,612,675
Depreciable capital assets, net	50,154,599	53,859,450
Total non-current assets	<u>119,204,501</u>	<u>91,472,125</u>
Total assets	<u>159,512,225</u>	<u>120,441,513</u>
Deferred Outflows of Resources		
Pension-related deferrals	963,484	253,159
Liabilities		
Current liabilities:		
Payable from unrestricted assets:		
Accounts payable	563,922	508,915
Construction contracts payable	3,588,119	2,293,825
Construction contract retainages	2,508,003	739,012
Compensated absences	215,104	209,798
Accrued liabilities	153,452	130,206
Prepaid fees	396,023	219,484
Payable from restricted assets:		
Accrued interest payable	256,200	-
Revenue bonds payable, current	955,000	-
Total current liabilities	<u>8,635,823</u>	<u>4,101,240</u>

The accompanying notes are an integral part of these financial statements.

STATEMENTS OF NET POSITION

June 30, 2017 and 2016

	<u>2017</u>	<u>Restated 2016</u>
Non-current liabilities:		
Other post-employment benefits	1,149,349	1,066,392
Compensated absences	134,902	117,098
Total pension liability - LEO	441,122	446,133
Net pension liability - LGERS	1,080,905	226,282
Revenue bonds payable, non-current	<u>20,045,000</u>	<u>-</u>
Total non-current liabilities	<u>22,851,278</u>	<u>1,855,905</u>
 Total liabilities	 <u>31,487,101</u>	 <u>5,957,145</u>
 Deferred Inflows of Resources		
Pension-related deferrals	<u>60,023</u>	<u>136,525</u>
 Net Position		
Net investment in capital assets	98,204,501	91,472,125
Restricted	15,777,675	5,214,500
Unrestricted	<u>14,946,409</u>	<u>17,914,377</u>
 Total net position	 <u>\$ 128,928,585</u>	 <u>\$ 114,601,002</u>

The accompanying notes are an integral part of these financial statements.

STATEMENTS OF REVENUES, EXPENSES, AND CHANGES IN NET POSITION

For the years ended June 30, 2017 and 2016

	2017	Restated 2016
	<u>2017</u>	<u>2016</u>
Operating Revenues		
Terminal	\$ 5,079,256	\$ 5,133,966
Airfield	935,323	1,009,331
General aviation	1,003,966	1,001,333
Parking lot	3,549,920	3,296,169
Other	<u>236,162</u>	<u>391,902</u>
Total operating revenues	<u>10,804,627</u>	<u>10,832,701</u>
Operating Expenses		
Administration	599,147	433,578
Development	399,036	509,271
Executive	830,832	520,962
Finance	412,904	382,127
Guest services	200,407	186,296
Information technology	741,888	676,912
Marketing	524,627	506,016
Operations	3,501,944	2,977,891
Public safety	1,341,362	1,407,510
Business development	177,255	99,274
Non-signatory fees	197,462	-
Depreciation	<u>4,760,215</u>	<u>4,675,453</u>
Total operating expenses	<u>13,687,079</u>	<u>12,375,290</u>
Operating loss	<u>(2,882,452)</u>	<u>(1,542,589)</u>
Non-Operating Revenues (Expenses)		
Passenger facility charges	1,836,201	1,615,151
Customer facility charges	1,404,051	1,354,395
Interest revenue	90,542	35,760
Gain on disposal of capital assets	58,732	44,201
Interest expense	<u>(463,843)</u>	<u>(73,272)</u>
Total non-operating revenues, net	<u>2,925,683</u>	<u>2,976,235</u>
Income before capital contributions	43,231	1,433,646
Capital contributions	<u>14,284,352</u>	<u>15,312,051</u>
Change in net position	<u>14,327,583</u>	<u>16,745,697</u>
Net position, beginning, previously reported	115,004,382	98,207,986
Restatement (Note 4)	<u>(403,380)</u>	<u>(352,681)</u>
Net position, beginning, restated	<u>114,601,002</u>	<u>97,855,305</u>
Net position, end of year	<u>\$ 128,928,585</u>	<u>\$ 114,601,002</u>

The accompanying notes are an integral part of these financial statements.

STATEMENTS OF CASH FLOWS
For the years ended June 30, 2017 and 2016

	<u>2017</u>	<u>Restated 2016</u>
Cash Flows from Operating Activities:		
Cash received for providing services	\$ 11,128,795	\$ 10,677,900
Cash paid for goods and services	(4,809,087)	(2,771,952)
Cash paid to, or on behalf of, employees for services	<u>(4,747,655)</u>	<u>(4,546,613)</u>
Net cash provided by operating activities	<u>1,572,053</u>	<u>3,359,335</u>
Cash Flows from Capital and Related Financing Activities:		
Acquisition and construction of capital assets	(29,429,306)	(17,989,184)
Cash received for the sale of capital assets	58,732	16,039
Passenger facility charges	1,803,897	1,583,133
Customer facility charges	1,404,051	1,354,395
Proceeds from revenue bonds	21,000,000	-
Principal payments of bond maturities	-	(1,589,996)
Interest paid on bond maturities	(207,643)	(73,272)
Capital contributions	<u>17,743,550</u>	<u>12,936,121</u>
Net cash provided (used) by capital and related financing activities	<u>12,373,281</u>	<u>(3,762,764)</u>
Cash Flows from Investing Activities:		
Interest income	<u>90,542</u>	<u>35,760</u>
Net cash provided by investing activities	<u>90,542</u>	<u>35,760</u>
Net increase (decrease) in cash and cash equivalents	14,035,876	(367,669)
Cash and cash equivalents, beginning of year	<u>21,927,144</u>	<u>22,294,813</u>
Cash and cash equivalents, end of year	<u>\$ 35,963,020</u>	<u>\$ 21,927,144</u>

The accompanying notes are an integral part of these financial statements.

STATEMENTS OF CASH FLOWS
For the years ended June 30, 2017 and 2016

	<u>2017</u>	<u>Restated 2016</u>
Reconciliation of Operating Loss to Net Cash Provided by Operating Activities:		
Operating loss	\$ (2,882,452)	\$ (1,542,589)
Adjustments to reconcile operating loss to net cash provided by operating activities:		
Depreciation expense	4,760,215	4,675,453
Changes in assets, deferred outflows of resources, and liabilities:		
(Increase) decrease in accounts receivable	(723,802)	17,801
(Increase) decrease in prepaid items	(5,552)	(3,352)
(Increase) decrease in net pension assets	-	298,145
(Increase) decrease in deferred outflows (pensions)	(710,325)	(17,524)
Increase (decrease) in accounts payable	55,007	233,532
Increase (decrease) in accrued liabilities	23,246	8,806
Increase (decrease) in prepaid fees	176,539	(23,875)
Increase (decrease) in other post-employment benefits	82,957	67,545
Increase (decrease) in compensated absences	23,110	(55,217)
Increase (decrease) in net pension liabilities	849,612	286,874
Increase (decrease) in deferred inflows (pensions)	(76,502)	(586,264)
Total adjustments	<u>4,454,505</u>	<u>4,901,924</u>
Net cash provided by operating activities	<u>\$ 1,572,053</u>	<u>\$ 3,359,335</u>
Non-Cash Capital and Related Financing Activities:		
Value of assets given in trade	<u>\$ -</u>	<u>\$ (44,000)</u>
Supplemental Cash Flow Information:		
Net change in construction contracts and retainage payable	<u>\$ 3,063,285</u>	<u>\$ 977,252</u>

The accompanying notes are an integral part of these financial statements.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

NOTES TO THE FINANCIAL STATEMENTS JUNE 30, 2017 AND 2016

Note 1 – Organization and Summary of Significant Accounting Policies

The accounting policies of the Greater Asheville Regional Airport Authority conform to accounting principles generally accepted in the United States of America as applicable to governments. The following is a summary of the more significant accounting policies:

A. Reporting Entity

The Asheville Regional Airport Authority (the “Authority”) was established by joint agreement between the City of Asheville and the County of Buncombe on November 29, 1979, pursuant to Article 20 of Chapter 160A of the North Carolina General Statutes. The agreement originally covered a twenty-five-year period beginning July 1, 1980. On April 19, 1988, this term was extended to April 30, 2018 by mutual agreement of the contracting parties. On January 22, 2008, the agreement was amended again with the term remaining the same, including a year-to-year extension after the term has been reached. Under the agreement, Buncombe County was responsible for funding any operating deficits of the Authority during the term of the agreement. On June 28, 2012, the General Assembly of North Carolina enacted the Greater Asheville Regional Airport Authority Act in Session Law 2012-121, making the Authority an independent airport authority and changing the official name to Greater Asheville Regional Airport Authority. Pursuant to the State statute, the agreement with Buncombe County and the City of Asheville is no longer applicable.

On June 28, 2017, the Federal Aviation Administration issued an Airport Operating Certificate in the name of the Greater Asheville Regional Airport Authority, allowing the Authority to act as the sole sponsor to operate the airport. The real property of the Authority was also deeded in the name of the Greater Asheville Regional Airport Authority. There are no outstanding liabilities, including grant reimbursements, regarding any payment to the City. Furthermore, all grant obligations incurred by the City, on behalf of the airport, were transferred in the transaction and are now the direct responsibility of the Authority. Also, the Authority is fully empowered to issue debt on behalf of the airport in support of future capital improvement needs.

B. Basis of Presentation

Fund financial statements provide information about the Authority’s funds. Statements for the proprietary fund category are presented.

Proprietary funds include the following fund type:

Enterprise Funds. Enterprise funds account for those operations (a) that are financed and operated in a manner similar to private business enterprises where the intent of the governing body is that costs (expenses, including depreciation) of providing goods or services to the general public on a continuing basis be financed or recovered primarily through user charges; or (b) where the governing body has decided that the periodic determination of revenues earned, expenses incurred, and/or net income is appropriate for capital maintenance, public policy, management control, accountability, or other purposes. The Authority has one enterprise fund for airport operation.

C. Measurement Focus and Basis of Accounting

The proprietary fund financial statements are reported using the economic resources measurement focus. These statements are reported using the accrual basis of accounting. Revenues are recorded when earned, and expenses are recorded at the time liabilities are incurred, regardless of when the related cash flows take place.

Proprietary funds distinguish operating revenues and expenses from non-operating items. Operating revenues and expenses generally result from providing services and producing and delivering goods in connection with a proprietary fund's principal ongoing operations. The principal operating revenues of the Authority's Enterprise Fund are charges to customers for services.

Operating expenses for the Enterprise Fund include the cost of sales and services, administrative expenses, and depreciation on capital assets. All revenues and expenses not meeting this definition are reported as non-operating revenues and expenses.

D. Budgetary Data

The Authority's budgets are adopted as required by the North Carolina General Statutes. An annual budget is adopted for the Enterprise Fund. All annual appropriations lapse at the fiscal year-end. All budgets are prepared using the modified accrual basis of accounting. Expenditures may not legally exceed appropriations at the functional level for all annually budgeted funds and at the object level for the multi-year funds. The Budget Officer may transfer amounts between line item expenditures, within a budget ordinance line item/cost center, without limitation and without a report being required. These changes should not result in increased recurring obligations. The Budget Officer may transfer amounts up to \$60,000 from contingency to other line items within the same fund. An official report on such transfers must be made at the next regular meeting of the Board. Any other amendments must be approved by the governing board. During the year, several amendments to the original budget were made. The budget ordinance must be adopted by May 15 for the upcoming fiscal year, or the governing board must adopt an interim budget that covers that time until the annual ordinance can be adopted.

E. Estimates

The preparation of financial statements, in conformity with U.S. generally accepted accounting principles, requires management to make estimates and assumptions that affect certain reported amounts and disclosures. Accordingly, actual results could differ from those estimates.

F. Assets, Liabilities, and Net Position

1. Deposits and Investments

All deposits of the Authority are made in Board-designated official depositories and are secured as required by State law [G.S. 159-31]. The Authority may designate, as an official depository, any bank or savings association whose principal office is located in North Carolina. Also, the Authority may establish time deposit accounts such as NOW and SuperNOW accounts, money market accounts, and certificates of deposit.

State law [G.S. 159-30(c)] authorizes the Authority to invest in obligations of the United States or obligations fully guaranteed, both as to principal and interest by the United States; obligations of the State of North Carolina; bonds and notes of any North Carolina local government or public authority; obligations of certain non-guaranteed federal agencies; certain high quality issues of commercial paper and bankers' acceptances and the North Carolina Capital Management Trust (NCCMT).

The Authority's short-term investments in U.S. treasury obligations are reported at cost. Non-participating interest earning contracts are accounted for at cost. The NCCMT Government Portfolio,

a SEC-registered (2a-7) external investment pool, is measured at amortized cost, which is the NCCMT’s share price. The NCCMT- Term Portfolio’s securities are valued at fair value.

2. Cash and Cash Equivalents

The Authority considers all highly liquid investments (including restricted assets) with a maturity of three months or less when purchased to be cash and cash equivalents.

3. Restricted Assets

Restricted assets consist of monies and other resources which are legally restricted as described below:

Cash and cash equivalents. The unexpended proceeds of revenue bonds issued by the Authority are classified as restricted assets because their use is restricted to the purpose for which the bonds were issued, the construction of the parking deck. The bond order requires a debt service fund, used to accumulate funds for principal and interest requirements of the revenue bonds.

The Authority receives from the airline carriers enplaning passengers at the Greater Asheville Regional Airport a facility charge of \$4.50 per passenger. Every air carrier servicing the Airport must collect \$4.50 from passengers on all tickets sold and remit \$4.39 of these funds to the Authority. The Authority must use these funds for Federal Aviation Administration (FAA) approved capital improvement projects. Passenger facility charge collections that have been collected, but not yet disbursed on eligible projects, are restricted.

Restricted cash and cash equivalents at June 30, 2017 and 2016 were as follows:

	<u>2017</u>	<u>2016</u>
Passenger facility charge collections, undisbursed	\$ 7,035,024	\$ 5,214,483
Unexpended bond proceeds	8,742,649	-
Debt service sinking funds - bond repayment	1,211,202	-
Other	-	17
Total restricted cash and cash equivalents	<u>\$ 16,988,875</u>	<u>\$ 5,214,500</u>

4. Allowance for Doubtful Accounts

All receivables that historically experience uncollectible accounts are shown net of an allowance for doubtful accounts. The Authority did not have any accounts that were considered uncollectible by management for the years ended June 30, 2017 and 2016.

5. Capital Assets

Purchased or constructed capital assets are reported at cost or estimated historical cost. Donated capital assets received prior to June 30, 2015 are recorded at their estimated fair value at the date of donation. Donated capital assets received after June 30, 2015 are recorded at acquisition value. All other purchased or constructed capital assets are reported at cost or estimated historical cost. Minimum capitalization cost is \$5,000. The cost of normal maintenance and repairs that do not add to the value of the asset or materially extend assets’ lives are not capitalized.

Capital assets are depreciated over their useful lives on a straight-line basis as follows:

Asset	Estimated Useful Lives
Buildings and improvements	5-40 years
Public safety and maintenance equipment	3-20 years
Vehicles	3-20 years
Furniture	5-10 years
Computer software	5 years
Computer equipment	5 years

6. Deferred Outflows and Inflows of Resources

In addition to assets, the Statement of Net Position will sometimes report a separate section for deferred outflow of resources. This separate financial statement element, *Deferred Outflows of Resources*, represents a consumption of net position that applies to a future period and so will not be recognized as an expense or expenditure until then. The Authority has one item that meets this criterion – pension-related deferred outflows.

In addition to liabilities, the Statement of Net Position can also report a separate section for deferred inflows of resources. This separate financial statement element, *Deferred Inflows of Resources*, represents an acquisition of net position that applies to a future period and so will not be recognized as revenue until then. The Authority has one item that meets this criterion – pension-related deferred inflows.

7. Compensated Absences

Airport personnel policies permit an employee to earn vacation based on years of service. Accumulated annual leave in excess of 60 days will be forfeited at December 31, unless it is determined that the employee was prevented from using such leave. Excess leave, where it is determined that the employee was prevented from taking the leave, will be paid with the first payroll of the new calendar year, upon approval of the Executive Director. Employees who resign, are laid off, or otherwise separated from the Authority shall be entitled to be paid for any unused annual leave earned by them as of the date of termination, not to exceed 60 days. Accrued vacation pay amounted to \$267,124 and \$246,067 at June 30, 2017 and 2016, respectively.

A specific number of professional leave hours are available to exempt employees. Employees must be employed for 180 days of the calendar year to be eligible for professional leave for that year. The professional leave does not accrue from year to year. Unused professional leave is not paid upon termination, thus, no accrual has been made.

The Authority’s sick leave policy provides for an unlimited accumulation of earned sick leave. Sick leave is earned by regular, full-time employees at a rate of 12 days per year. All employees who properly resign, are laid off, or otherwise separated from the Authority in good standing shall be entitled to be paid for 33% of up to 240 hours of earned, but unused sick leave. Accrued sick pay amounted to \$82,882 and \$80,829 at June 30, 2017 and 2016, respectively.

The Authority has assumed a first-in, first-out method of using accumulated compensated time. The portion of that time that is estimated to be used in the next fiscal year has been designated as a current liability in the financial statements.

8. Pensions

The Authority participates in one cost-sharing, multiple-employer, defined benefit pension plan that is administered by the State: the Local Governmental Employees' Retirement System (LGERs). For purposes of measuring the net pension asset or liability, deferred outflows of resources and deferred inflows of resources related to pensions, and pension expense, information about the fiduciary net positions of the state-administered defined benefit pension plans and additions to/deductions from the state-administered defined benefit pension plans' fiduciary net positions have been determined on the same basis as they are reported by the state-administered defined benefit pension plans. For this purpose, plan member contributions are recognized in the period in which the contributions are due. The Authority's employer contributions are recognized when due and the Authority has a legal requirement to provide the contributions. Benefits and refunds are recognized when due and payable in accordance with the terms of the state-administered defined benefit pension plans. Investments are reported at fair value.

9. Net Position

Net position in proprietary fund financial statements is classified as net investment in capital assets, restricted and unrestricted. Restricted net position represents constraints on resources that are either externally imposed by creditors, grantors, contributors, laws or regulations of other governments, or imposed by law through State statute.

Note 2 – Detailed Notes On All Funds**A. Assets****1. Deposits**

All the deposits of the Authority are either insured or collateralized by the Pooling Method as required by State law [G.S. 159-31]. Under the Pooling Method, which is a collateral pool, all uninsured deposits are collateralized with securities held by the State Treasurer's agent in the name of the State Treasurer. Since the State Treasurer is acting in a fiduciary capacity for the Authority, these deposits are considered to be held by the Authority's agents in their names. The amount of the pledged collateral is based on an approved averaging method for non-interest-bearing deposits and the actual current balance for interest-bearing deposits. Depositories using the Pooling Method report to the State Treasurer the adequacy of their pooled collateral covering uninsured deposits. The State Treasurer does not confirm this information with the Authority or the escrow agent. Because of the inability to measure the exact amounts of collateral pledged for the Authority under the Pooling Method, the potential exists for undercollateralization, and this risk may increase in periods of high cash flows; however, the State Treasurer of North Carolina enforces strict standards of financial stability for each depository that collateralizes public deposits under the Pooling Method. The Authority does not have a deposit policy for custodial credit risk for deposits, but relies on the State Treasurer to enforce standards of minimum capitalization for all Pooling Method financial institutions and to monitor them for compliance. The Authority complies with the provisions of G.S. 159-31 when designating official depositories and verifying that deposits are properly secured.

At June 30, 2017, the Authority's deposits had a carrying amount of \$27,516,227 and a bank balance of \$27,687,499. Of the bank balance, \$750,000 was covered by federal depository insurance, and \$26,937,499 was covered under the Pooling Method. The Authority maintains petty cash of \$200.

2. Investments

At June 30, 2017, the Authority's investments and maturities were as follows:

Investment Type	Valuation Measurement Method	Fair Value	Less Than 3 Months
North Carolina Capital Management Trust – Government Portfolio	Amortized Cost	\$ 17,286	N/A
North Carolina Capital Management Trust – Term Portfolio	Fair Value Level I	3,046,196	\$ 3,046,196
US Treasuries	Cost	<u>5,383,111</u>	<u>5,383,111</u>
		<u>\$ 8,446,593</u>	<u>\$ 8,429,307</u>

The North Carolina Management Trust Term Portfolio had a duration of approximately 1 month at June 30, 2017; therefore, it is presented as an investment with a maturity of less than 3 months.

All investments are measured using the market approach: using prices and other relevant information generated by market transactions involving identical or comparable assets or a group of assets.

Level of fair value hierarchy: Level 1: Debt securities valued using directly observable, quoted prices (unadjusted) in active markets for identical assets.

Interest Rate Risk. As a means of limiting its exposure to fair value losses arising from rising interest rates, the Authority's investment policy limits at least half of the Authority's investment portfolio to maturities of less than 12 months. Also, the Authority's investment policy requires purchases of securities to be laddered with staggered maturity dates and limits all securities to a final maturity of not more than three years.

Credit Risk. State law limits investments in commercial paper to the top rating issued by nationally recognized statistical rating organizations (NRSROs); however, the Authority has no formal policy on managing credit risk. As of June 30, 2017, the Authority's investment in the North Carolina Capital Management Trust Government Portfolio carried a credit rating of AAAM by Standard & Poor's. The Authority's investment in North Carolina Capital Management Trust Term Portfolio is unrated. The Term Portfolio is authorized to invest in obligations of the US government and agencies and in high grade money market instruments as permitted under North Carolina General Statutes 159-30, as amended.

3. Accounts Receivable

The balance of accounts receivable at June 30, 2017, and 2016, consisted of the following:

	2017	2016
Trade receivables	\$ 637,624	\$ 780,962
Sales tax refunds receivable	<u>989,976</u>	<u>122,836</u>
Total	<u>\$ 1,627,600</u>	<u>\$ 903,798</u>

4. Capital Assets

The capital assets of the Authority at June 30, 2017 are as follows:

	Restated July 1, 2016	Increases	Decreases	Transfers	June 30, 2017
Non-Depreciable Capital Assets:					
Land	\$ 7,593,744	\$ -	\$ -	\$ -	\$ 7,593,744
Construction in progress	<u>30,018,931</u>	<u>32,108,503</u>	<u>-</u>	<u>(671,276)</u>	<u>61,456,158</u>
Total	<u>37,612,675</u>	<u>32,108,503</u>	<u>-</u>	<u>(671,276)</u>	<u>69,049,902</u>
Depreciable Capital Assets:					
Buildings and improvements	104,906,742	121,212	-	17,578	105,045,532
Equipment	5,296,634	212,865	(359,717)	-	5,149,782
Furniture	251,038	-	-	50,725	301,763
Vehicles	<u>2,856,982</u>	<u>50,011</u>	<u>(29,347)</u>	<u>602,973</u>	<u>3,480,619</u>
Total	<u>113,311,396</u>	<u>384,088</u>	<u>(389,064)</u>	<u>671,276</u>	<u>113,977,696</u>
Less Accumulated Depreciation:					
Buildings and improvements	53,003,775	4,308,848	-	-	57,312,623
Equipment	4,267,925	260,972	(359,717)	-	4,169,180
Furniture	153,775	33,377	-	-	187,152
Vehicles	<u>2,026,471</u>	<u>157,018</u>	<u>(29,347)</u>	<u>-</u>	<u>2,154,142</u>
Total	<u>59,451,946</u>	<u>\$ 4,760,215</u>	<u>\$ (389,064)</u>	<u>\$ -</u>	<u>63,823,097</u>
Depreciable capital assets, net	<u>53,859,450</u>				<u>50,154,599</u>
Total capital assets, net	<u>\$ 91,472,125</u>				<u>\$ 119,204,501</u>

Construction Commitments

The Authority has active construction projects as of June 30, 2017. At year-end, the Authority's commitments with contractors are as follows:

Project	Spent-to-date	Remaining Commitment
Airfield re-development	\$ 28,668,894	\$ 32,305,559
Parking garage	14,941,184	7,134,663
Apron expansion	<u>71,167</u>	<u>27,833</u>
Total	<u>\$ 43,681,245</u>	<u>\$ 39,468,055</u>

B. Liabilities**1. Payables**

Payables at June 30, 2017, and 2016, were as follows:

	<u>Vendors</u>	<u>Salaries and Benefits</u>	<u>Security Deposits</u>	<u>Total</u>
June 30, 2017:				
Accounts payable	\$ 543,420	\$ 3,292	\$ 17,210	\$ 563,922
Accrued liabilities	-	153,452	-	153,452
Total	<u>\$ 543,420</u>	<u>\$ 156,744</u>	<u>\$ 17,210</u>	<u>\$ 717,374</u>
June 30, 2016:				
Accounts payable	\$ 489,692	\$ 2,013	\$ 17,210	\$ 508,915
Accrued liabilities	-	130,206	-	130,206
Total	<u>\$ 489,692</u>	<u>\$ 132,219</u>	<u>\$ 17,210</u>	<u>\$ 639,121</u>

2. Pension Plan Obligations**a. Local Governmental Employees' Retirement System**

Plan Description. The Authority is a participating employer in the statewide Local Governmental Employees' Retirement System (LGERS), a cost-sharing, multiple-employer defined benefit pension plan administered by the State of North Carolina. LGERS membership is comprised of general employees and local law enforcement officers (LEOs) of participating local governmental entities. Article 3 of G.S. Chapter 128 assigns the authority to establish and amend benefit provisions to the North Carolina General Assembly. Management of the plan is vested in the LGERS Board of Trustees, which consists of 13 members – nine appointed by the Governor, one appointed by the State Senate, one appointed by the State House of Representatives, and the State Treasurer and State Superintendent, who serve as ex-officio members. The Local Governmental Employees' Retirement System is included in the Comprehensive Annual Financial Report (CAFR) for the State of North Carolina. The State's CAFR includes financial statements and required supplementary information for LGERS. That report may be obtained by writing to the Office of the State Controller, 1410 Mail Service Center, Raleigh, North Carolina 27699-1410, by calling (919) 981-5454, or at www.osc.nc.gov.

Benefits Provided. LGERS provides retirement and survivor benefits. Retirement benefits are determined as 1.85% of the member's average final compensation times the member's years of creditable service. A member's average final compensation is calculated as the average of a member's four highest consecutive years of compensation. Plan members are eligible to retire with full retirement benefits at age 65 with five years of creditable service, at age 60 with 25 years of creditable service, or at any age with 30 years of creditable service. Plan members are eligible to retire with partial retirement benefits at age 50 with 20 years of creditable service or at age 60 with five years of creditable service. Survivor benefits are available to eligible beneficiaries of members who die while in active service or within 180 days of their last day of service and who have either completed 20 years of creditable service regardless of age or have completed five years of service and have reached age 60. Eligible beneficiaries may elect to receive a monthly Survivor's Alternate Benefit for life or a return of the member's contributions. The plan does not provide for automatic post-retirement benefit increases. Increases are contingent upon actuarial gains of the plan.

LGERS plan members who are LEOs are eligible to retire with full retirement benefits at age 55 with five years of creditable service as an officer, or at any age with 30 years of creditable service. LEO plan members are eligible to retire with partial retirement benefits at age 50 with 15 years of creditable service as an officer. Survivor benefits are available to eligible beneficiaries of LEO members who die while in active service or within 180 days of their last day of service and who also have either completed 20 years of creditable service regardless of age, or have completed 15 years of service as a LEO and have reached age 50, or have completed five years of creditable service as a LEO and have reached age 55, or have completed 15 of creditable service as a LEO if killed in the line of duty. Eligible beneficiaries may elect to a monthly Survivor's Alternate Benefit for life or a return of the member's contributions.

Contributions. Contribution provisions are established by General Statute 128-30 and may be amended only by the North Carolina General Assembly. Authority employees are required to contribute 6% of their compensation. Employer contributions are actuarially determined and set annually by the LGERS Board of Trustees. The Authority's contractually required contribution rate for the year ended June 30, 2017, was 8.00% of compensation for law enforcement officers and 7.34% for general employees, actuarially determined as an amount that, when combined with employee contributions, is expected to finance the costs of benefits earned by employees during the year. Contributions to the pension plan from the Authority were \$241,459 for the year ended June 30, 2017. For the year ended June 30, 2016, the Authority's contractually required contribution rate was 7.15% for law enforcement officers and 6.76% for general employees. Contributions to the pension plan from the Authority were \$210,066 for the year ended June 30, 2016.

Refunds of Contributions. Authority employees who have terminated service as a contributing member of LGERS, may file an application for a refund of their contributions. By State law, refunds to members with at least five years of service include 4% interest. State law requires a 60-day waiting period after service termination before the refund may be paid. The acceptance of a refund payment cancels the individual's right to employer contributions or any other benefit provided by LGERS.

Pension Liabilities, Pension Expense, and Deferred Outflows of Resources and Deferred Inflows of Resources Related to Pensions

At June 30, 2017, the Authority reported a liability of \$1,080,905 for its proportionate share of the net pension liability. The net pension liability was measured as of June 30, 2016. The total pension liability used to calculate the net pension liability was determined by an actuarial valuation as of December 31, 2015. The total pension liability was then rolled forward to the measurement date of June 30, 2016, utilizing update procedures incorporating the actuarial assumptions. The Authority's proportion of the net pension liability was based on a projection of the Authority's long-term share of future payroll covered by the pension plan, relative to the projected future payroll covered by the pension plan of all participating LGERS employers, actuarially determined. At June 30, 2016, the Authority's proportion was 0.05093%, which was an increase of 0.00051% from its proportion measured as of June 30, 2015.

For the year ended June 30, 2017, the Authority recognized pension expense of \$287,826. At June 30, 2017, the Authority reported deferred outflows of resources and deferred inflows of resources related to pensions from the following sources:

	Deferred Outflows of Resources	Deferred Inflows of Resources
Differences between expected and actual experience	\$ 20,308	\$ 37,876
Changes in assumptions	74,032	-
Net difference between projected and actual earnings on pension plan investments	597,605	-
Changes in proportion and differences between Authority contributions and proportionate share of contributions	10,827	12,589
Authority contributions subsequent to the measurement date	241,459	-
Total	<u>\$ 944,231</u>	<u>\$ 50,465</u>

\$241,459 reported as deferred outflows of resources related to pensions resulting from Authority contributions subsequent to the measurement date will be recognized as a decrease of the net pension liability in the year ended June 30, 2018. Other amounts reported as deferred inflows of resources related to pensions will be recognized in pension expense as follows:

Year Ending June 30	Amount
2018	\$ 96,619
2019	96,765
2020	286,786
2021	172,137
Total	<u>\$ 652,307</u>

Actuarial Assumptions. The total pension liability in the December 31, 2015, actuarial valuation was determined using the following actuarial assumptions, applied to all periods included in the measurement:

Inflation	3.0 percent
Salary increase	3.50 to 7.75 percent, including inflation and productivity factor
Investment rate of return	7.25 percent, net of pension plan investment expense, including inflation

The plan currently uses mortality tables that vary by age, gender, employee group (i.e. general, law enforcement officer) and health status (i.e. disabled and healthy). The current mortality rates are based on published tables and based on studies that cover significant portions of the U.S. population. The healthy mortality rates also contain a provision to reflect future mortality improvements.

The actuarial assumptions used in the December 31, 2015, valuation were based on the results of an actuarial experience study for the period ending January 1, 2010, through December 31, 2014.

Future ad hoc COLA amounts are not considered to be substantively automatic and are therefore not included in the measurement.

The projected long-term investment returns and inflation assumptions are developed through review of current and historical capital markets data, sell-side investment research, consultant whitepapers, and historical performance of investment strategies. Fixed income return projections reflect current yields across the U.S. Treasury yield curve and market expectations of forward yields projected and interpolated for multiple tenors and over multiple year horizons. Global public equity return projections are established through analysis of the equity risk premium and the fixed income return projections. Other asset categories and strategies' return projections reflect the foregoing and historical data analysis. These projections are combined to produce the long-term expected rate of return by weighting the expected future real rates of return by the target asset allocation percentage and by adding expected inflation. The target allocation and best estimates of arithmetic real rates of return for each major asset class as of June 30, 2016, are summarized in the following table:

Asset Class	Target Allocation	Expected Real Rate of Return
Fixed Income	29.0%	1.4%
Global Equity	42.0%	5.3%
Real Estate	8.0%	4.3%
Alternatives	8.0%	8.9%
Credit	7.0%	6.0%
Inflation Protected	6.0%	4.0%
Total	100.0%	

The information above is based on 30 year expectations developed with the consulting actuary for the 2015 asset, liability, and investment policy study for the North Carolina Retirement Systems, including LGERS. The long-term nominal rates of return underlying the real rates of return are arithmetic annualized figures. The real rates of return are calculated from nominal rates by multiplicatively subtracting a long-term inflation assumption of 3.00%. All rates of return and inflation are annualized.

Discount Rate. The discount rate used to measure the total pension liability was 7.25%. The projection of cash flows used to determine the discount rate assumed that contribution from plan members will be made at the current contribution rate and that contributions from employers will be made at statutorily required rates, actuarially determined. Based on these assumptions, the pension plan's fiduciary net position was projected to be available to make all projected future benefit payments of the current plan members. Therefore, the long-term expected rate of return on pension plan investments was applied to all periods of projected benefits payments to determine the total pension liability.

Sensitivity of the Authority's Proportionate Share of the Net Pension Liability to Changes in the Discount Rate. The following presents the Authority's proportionate share of the net pension liability calculated using the discount rate of 7.25 percent, as well as what the Authority's proportionate share of the net pension asset or net pension liability would be if it were calculated using a discount rate that is one percentage point lower (6.25 percent) or one percentage point higher (8.25 percent) than the current rate:

	1% Decrease <u>(6.25%)</u>	Discount Rate <u>(7.25%)</u>	1% Increase <u>(8.25%)</u>
Authority's proportionate share of the net pension liability (asset)	<u>\$ 2,565,492</u>	<u>\$ 1,080,905</u>	<u>\$ (159,132)</u>

Pension plan fiduciary net position. Detailed information about the pension plan’s fiduciary net position is available in the separately issued Comprehensive Annual Financial Report (CAFR) for the State of North Carolina.

b. Law Enforcement Officers' Special Separation Allowance

Plan Description. The Authority administers a public employee retirement system (the “Separation Allowance”), a single-employer defined benefit pension plan that provides retirement benefits to the Authority’s qualified sworn law enforcement officers under the age of 62 who have completed at least 30 years of creditable service or have attained 55 years of age and have completed five or more years of creditable service. The Separation Allowance is equal to .85 percent of the annual equivalent of the base rate of compensation most recently applicable to the officer for each year of creditable service. The retirement benefits are not subject to any increases in salary or retirement allowances that may be authorized by the General Assembly. Article 12D of G.S. Chapter 143 assigns the authority to establish and amend benefit provisions to the North Carolina General Assembly.

All full-time law enforcement officers of the Authority are covered by the Separation Allowance. At December 31, 2015, the Separation Allowance’s membership consisted of:

Retirees receiving benefits	1
Terminated plan members entitled to, but not yet receiving, benefits	-
Active plan members	<u>17</u>
Total	<u>18</u>

Summary of Significant Accounting Policies

Basis of Accounting. The Authority has chosen to fund the Separation Allowance on a pay-as-you-go basis. Pension expenditures are made from the General Fund, which is maintained on the modified accrual basis of accounting. Benefits and refunds are recognized when due and payable in accordance with the terms of the plan.

The Separation Allowance has no assets accumulated in a trust that meet the criteria which are outlined in GASB Statement 73.

Actuarial Assumptions

The entry age normal actuarial cost method was used in the December 31, 2015 valuation. The total pension liability in the December 31, 2015 actuarial valuation was determined using the following actuarial assumptions, applied to all periods included in the measurement:

Inflation	3.0 percent
Salary increase	3.50 to 7.35 percent, including inflation and productivity factor
Discount rate	3.86 percent

The discount rate used to measure the total pension liability is the weekly average of the Bond Buyer General Obligation 20-year Municipal Bond Index determined at the end of each month.

Mortality rates are based on the RP-2014 mortality tables base rates projected to the valuation date using MP-2015.

Contributions. The Authority is required by Article 12D of G.S. Chapter 143 to provide these retirement benefits and has chosen to fund the amounts necessary to cover the benefits earned on a pay-as-you-go basis through appropriations made in the operating budget. There were no contributions made by employees. The Authority’s obligation to contribute to this plan is established and may be amended by the North Carolina General Assembly. Administrative costs of the Separation Allowance are financed through investment earnings. The Authority paid \$30,949 as benefits came due for the reporting period.

Pension Liabilities, Pension Expense, and Deferred Outflows of Resources and Deferred Inflows of Resources Related to Pensions

At June 30, 2017, the Authority reported a total pension liability of \$441,122. The total pension liability was measured as of December 31, 2016 based on a December 31, 2015 actuarial valuation. The total pension liability was then rolled forward to the measurement date of December 31, 2016 utilizing update procedures incorporating the actuarial assumptions. For the year ended June 30, 2017, the Authority recognized pension expense of \$35,671.

	Deferred Outflows of Resources	Deferred Inflows of Resources
Differences between expected and actual experience	\$ -	\$ -
Changes in assumptions and other inputs	-	9,558
Benefit payments and administrative expenses subsequent to the measurement date	19,253	-
Total	<u>\$ 19,253</u>	<u>\$ 9,558</u>

\$19,253 reported as deferred outflows of resources related to pensions resulting from benefit payments made and administrative expenses incurred subsequent to the measurement date will be recognized as a decrease of the total pension liability in the year ended June 30, 2018. Other amounts reported as deferred inflows of resources related to pensions will be recognized in pension expense as follows:

Year Ending June 30	Amount
2018	\$ 1,722
2019	1,722
2020	1,722
2021	1,722
2022	1,722
Thereafter	948
Total	<u>\$ 9,558</u>

\$18,890 paid as benefits came due and \$363 of administrative expenses subsequent to the measurement date are reported as deferred outflows of resources.

Sensitivity of the Authority’s total pension liability to changes in the discount rate. The following presents the Authority’s total pension liability calculated using the discount rate of 3.86 percent, as well as what the Authority’s total pension liability would be if it were calculated using a discount rate that is 1-percentage-point lower (2.86 percent) or 1-percentage-point higher (4.86 percent) than the current rate:

	1% Decrease (2.86%)	Discount Rate (3.86%)	1% Increase (4.86%)
Total pension liability	<u>\$ 481,359</u>	<u>\$ 441,122</u>	<u>\$ 404,563</u>

Schedule of Changes in Total Pension Liability

Total pension liability as of December 31, 2015	\$ 446,133
Changes for the year:	
Service cost at end of year	21,843
Interest	15,375
Change in benefit terms	-
Difference between expected and actual experience	-
Changes of assumptions and other inputs	(11,280)
Benefit payments	(30,949)
Other	-
Net changes	<u>(5,011)</u>
Total pension liability as of December 31, 2016	<u>\$ 441,122</u>

The plan currently uses mortality tables that vary by age, and health status (i.e. disabled and healthy). The current mortality rates are based on published tables and based on studies that cover significant portions of the U.S. population. The healthy mortality rates also contain a provision to reflect future mortality improvements.

The actuarial assumptions used in the December 31, 2015 valuation were based on the results of an actuarial experience study for the period January 1, 2010 through December 31, 2014.

c. Supplemental Retirement Income Plan

Plan Description. The Authority contributes to the Supplemental Retirement Income Plan (the “Plan”), a defined contribution pension plan administered by the Department of State Treasurer and a Board of Trustees. The Plan provides retirement benefits to employees of the Authority. Article 5 of G.S. Chapter 135 assigns the authority to establish and amend benefit provisions to the North Carolina General Assembly. The Supplemental Retirement Income Plan for Law Enforcement Officers is included in the Comprehensive Annual Financial Report (the “CAFR”) for the State of North Carolina. The State’s CAFR includes the pension trust fund financial statements for the Internal Revenue Code Section 401(k) plan that includes the Supplemental Retirement Income Plan. That report may be obtained by writing to the Office of the State Controller, 1410 Mail Service Center, Raleigh, North Carolina 27699-1410, or by calling (919) 981-5454.

Funding Policy. Article 12E of G.S. Chapter 143 requires the Authority to contribute each month an amount equal to 5% of each officer’s salary. In addition to the required contribution for law enforcement officers, the Authority makes a monthly contribution equal to 5% of non-law enforcement officer's salary. Also, law enforcement officers and non-law enforcement officers of the Authority may make voluntary contributions to the Plan.

The Authority made contributions of \$41,667 and \$121,941 for law enforcement officers and non-law enforcement officers for the year ended June 30, 2017, respectively. The Authority made contributions of \$43,368 and \$113,306 for law enforcement officers and non-law enforcement officers for the year ended June 30, 2016, respectively. No amounts were forfeited in 2017 or 2016.

d. Other Employment Benefits

The Authority has elected to provide death benefits to employees through the Death Benefit Plan for members of the Local Governmental Employees’ Retirement System (Death Benefit Plan), a multiple employer, State-administered, cost-sharing plan funded on a one-year term cost basis. The beneficiaries of those employees who die in active service after one year of contributing membership in the System, or who die within 180 days after retirement or termination of service and have at least one year of contributing membership service in the System at the time of death, are eligible for death benefits. Lump-sum death benefit payments to beneficiaries are equal to the employee’s 12 highest months' salary in a row during the 24 months prior to the employee’s death, but the benefit may not exceed \$50,000, or be less than \$25,000. Because all death benefit payments are made from the Death Benefit Plan, the Authority does not determine the number of eligible participants. The Authority has no liability beyond the payment of the monthly contributions. The contributions to the Death Benefit Plan cannot be separated between the post-employment benefit amount and the other benefit amount. Contributions are determined as a percentage of monthly payroll based on rates established annually by the State. Separate rates are set for employees not engaged in law enforcement and for law enforcement officers. The Authority considers these contributions to be immaterial.

The Department of State Treasurer uses funds derived from court costs to offset employer contribution rates to the LGERS Death Benefit Plan for law enforcement officers. During the fiscal year ended June 30, 2017, these offset funds matched the total contribution rate of .014%.

The Authority provides health, dental, short-term and long-term disability, life insurance, and a 457 deferred compensation plan to its regular full-time employees. Regular part-time employees, working at least 1,000 hours annually, qualify for these benefits.

e. **Other Post-Employment Benefits (OPEB)**

Healthcare Benefits

Plan Description. Under the terms of an Authority resolution, the Authority administers a single-employer defined benefit healthcare plan (the “Retiree Health Plan”). The Plan provides post-employment healthcare benefits to retirees of the Authority up to age 65, provided they participate in the North Carolina Local Governmental Employees’ Retirement System, were hired prior to July 1, 2011, and were covered by the Authority’s group health plan for the three years immediately preceding retirement. The Authority pays the full cost of coverage for these benefits through private insurers. Also, the Authority’s retirees can purchase coverage for their dependents at the Authority’s group rates. Employees hired on or after July 1, 2011, are not eligible for the Authority’s post-employment healthcare benefits. Retirees who qualify for coverage receive the same benefits as active employees. Coverage for all retirees who are eligible for Medicare will no longer be eligible for post-employment healthcare benefits by the Authority. The Authority Board may amend the benefit provisions. A separate report was not issued for the Plan.

Membership of the Retiree Health Plan consisted of the following at December 31, 2015, the date of the latest actuarial valuation:

	General <u>Employees</u>	<u>Firefighters</u>	Law Enforcement <u>Officers</u>
Retirees receiving benefits	-	-	1
Terminated plan members entitled to, but not yet receiving, benefits	-	-	-
Active plan members	<u>26</u>	<u>3</u>	<u>8</u>
Total	<u><u>26</u></u>	<u><u>3</u></u>	<u><u>9</u></u>

Funding Policy. By Authority resolution, the Authority pays the full cost of coverage for the healthcare benefits paid for qualified retirees. The Authority’s retirees pay the full cost for any dependent coverage. The Authority has chosen to fund the healthcare benefits on a pay-as-you-go basis.

The current ARC rate is 6.50% of annual covered payroll. For the current year, the Authority contributed \$24,686, or 1.26%, of annual covered payroll. The Authority obtains healthcare coverage through private insurers. There were no contributions made by employees, except for dependent coverage in the amount of \$6,488. The Authority’s obligation to contribute to the Retiree Health Plan is established and may be amended by the Authority Board.

Summary of Significant Accounting Policies. Post-employment expenditures are made from the proprietary fund, which is maintained on the accrual basis of accounting. No funds are set aside to pay benefits and administration costs. These expenditures are paid as they come due.

Annual OPEB Cost and Net OPEB Obligation. The Authority’s annual other post-employment benefit (OPEB) cost (expense) is calculated based on the *annual required contribution of the employer* (ARC), an amount actuarially determined in accordance with the parameters of GASB Statement No. 45.

The ARC represents a level of funding that, if paid on an ongoing basis, is projected to cover normal cost each year and amortize any unfunded actuarial liabilities (or funding excess) over a period not to exceed 30 years.

The following table shows the components of the Authority's annual OPEB cost for the current year, the amount actually contributed to the Plan, and changes in the Authority's net OPEB obligation for the post-employment healthcare benefits:

Annual required contribution	\$ 127,779
Interest on net OPEB obligation	42,656
Adjustment to annual required contribution	<u>(62,792)</u>
Annual OPEB cost (expense)	107,643
Contributions made	<u>(24,686)</u>
Increase in net OPEB obligation	82,957
Net OPEB obligation, beginning of year	<u>1,066,392</u>
Net OPEB obligation, end of year	<u>\$ 1,149,349</u>

The Authority's annual OPEB cost, the percentage of annual OPEB cost contributed to the Plan, and the net OPEB obligation for 2017 and the preceding two years are as follows:

Three-Year Trend Information			
Year Ended June 30	Annual OPEB Cost	Percentage of Annual OPEB Cost Contributed	Net OPEB Obligation
2015	\$ 130,547	45.23%	\$ 998,847
2016	129,378	47.79%	1,066,392
2017	107,643	22.93%	1,149,349

Funding Status and Funding Progress. As of December 31, 2015, the most recent actuarial valuation date, the Plan was not funded. The actuarial accrued liability for benefits and, thus, the unfunded actuarial accrued liability (UAAL) was \$938,046. The covered payroll (annual payroll of active employees covered by the Plan) was \$1,965,966, and the ratio of the UAAL to the covered payroll was 47.7%. Actuarial valuations of an ongoing plan involve estimates of the value of reported amounts and assumptions about the probability of occurrence of events far into the future. Examples include assumptions about future employment, mortality, and healthcare trends. Amounts determined regarding the funded status of the Plan and the annual required contributions of the employer are subject to continual revision as actual results are compared with past expectations, and new estimates are made about the future. The Schedule of Funding Progress, presented as required supplementary information following the notes to the financial statements, presents multi-year trend information about whether the actuarial value of plan assets is increasing or decreasing, over time, relative to the actuarial accrued liabilities for benefits.

Actuarial Methods and Assumptions. Projections of benefits for financial reporting purposes are based on the substantive plan (the plan as understood by the employer and the plan members), and include the types of benefits provided at the time of each valuation and the historical pattern of sharing of benefit costs between the employer and plan members at that point. The actuarial methods and assumptions used include techniques that are designed to reduce the effects of short-term volatility in actuarial accrued liabilities and the actuarial value assets, consistent with the long-term perspective of the calculations.

In the December 31, 2015, actuarial valuation, the projected unit credit actuarial cost method was used. The actuarial assumptions included a 4.00% investment rate of return (net of administrative expenses), which is the expected long-term investment returns on the employer's own investments calculated based on the funded level of the plan at the valuation date, and an annual medical cost trend increase of 7.75% to 5.00% annually. The investment rate included a 3.00% inflation assumption.

The actuarial value of assets was calculated based on the assumption that there were no assets set aside with the Authority that are legally held exclusively for retiree health benefits. If a trust or equivalent arrangement was set up for this purpose, the investment rate of return can be increased. The UAAL is being amortized as a level dollar amount, on a closed basis. The remaining amortization period at December 31, 2015, was 27 years.

3. Long-term Debt

On September 1, 2016, the Authority issued \$21,000,000 of bonds entitled Greater Asheville Regional Airport Authority System Revenue Bonds 2016A and 2016B. These bonds provide financing for the construction of a parking deck for the airport system. The bonds require sinking fund payments beginning July 1, 2017 ranging from \$955,000 to \$1,665,000 annually, including semi-annual interest ranging from 2.39% to 2.59%, until July 1, 2031.

The revenue bonds mature as follows:

Year Ended		
June 30	Principal	Interest
2018	\$ 955,000	\$ 500,033
2019	1,215,000	471,931
2020	1,245,000	440,074
2021	1,280,000	407,376
2022	1,315,000	374,530
2023-2027	7,050,000	1,377,955
2028-2032	7,940,000	483,378
Total	<u>\$ 21,000,000</u>	<u>\$ 4,055,277</u>

The Authority is in compliance with the covenants of the bond order. The Bond Order requires the debt service coverage ratio to be no less than 125%. The debt service coverage ratio calculation for the year ended June 30, 2017, is as follows:

Airport revenues (excluding passenger facility charges)	\$ 12,299,220
Current expenses *	<u>8,758,012</u>
Net revenues	<u>\$ 3,541,208</u>
Debt service: **	
Principal	955,000
Interest	<u>411,343</u>
	<u>\$ 1,366,343</u>
Debt service coverage ratio	259.2%

* excludes capital, debt service, and depreciation

** Per Bond Indenture, debt service for a fiscal year ending June 30, includes amounts of principal and interest due on the following July 1

4. Changes in Long-Term Liabilities

Changes in long-term liabilities are as follows:

	Restated				
	July 1, 2016	Additions	Retirements	June 30, 2017	Current
Revenue bonds	\$ -	\$ 21,000,000	\$ -	\$ 21,000,000	\$ 955,000
Compensated absences	326,896	223,142	200,032	350,006	215,104
Total pension liability (LEO)	446,133	-	5,011	441,122	-
Net pension liability (LGERS)	226,282	854,623	-	1,080,905	-
Other post-employment benefits:					
Healthcare benefits	<u>1,066,392</u>	<u>107,643</u>	<u>24,686</u>	<u>1,149,349</u>	<u>-</u>
Total	<u>\$ 2,065,703</u>	<u>\$ 22,185,408</u>	<u>\$ 229,729</u>	<u>\$ 24,021,382</u>	<u>\$ 1,170,104</u>

5. Construction Contract Commitments

The Authority has commitments of approximately \$39.5 million for the construction/renovation of facilities. These projects are to be funded through Federal grants, State grants, passenger facility charges, customer facility charges and Authority funding.

The underlying contracts have termination provisions. The contract with an architectural/engineering consulting firm representing approximately \$1.2 million of the commitment amount allows either party to terminate the agreement with a seven-day written notice and the consultant is entitled to payment for work executed and costs incurred by reason of such termination.

The contracts with the construction firms representing approximately \$2.6 million of the commitment amount allows the Authority to either terminate the agreement “for cause” due to failure by the

contractor to fulfill contract obligations, or “for the convenience” of the Authority at any time. The contract termination for the convenience of the Authority will provide for an equitable adjustment in the contract price, but no amount will be allowed for anticipated profit on unperformed services.

The contract with the construction firm representing approximately \$28.8 million of the commitment amount allows the Authority to suspend or terminate the agreement upon any violation or breach of terms of the contract. Authority will provide contractor written notice of breach and corrective actions contractor must undertake to avoid termination of contract, including specific date by which the breach must be corrected. Authority reserves the right to withhold payments to contractor until such time the contractor corrects the breach or the Authority elects to terminate the contract.

The contract with the construction firm representing approximately \$6.9 million of the commitment amount allows the contract to be terminated by the Authority or by the contractor as provided in Article 14 of AIA Document A201-2007.

C. Deferred Outflows and Inflows of Resources

Deferred outflows of resources at year-end are as follows:

Source	2017	Restated 2016
Pension-related deferrals	\$ 963,484	\$ 253,159
Total	<u>\$ 963,484</u>	<u>\$ 253,159</u>

Deferred inflows of resources at year-end are as follows:

Source	2017	2016
Pension-related deferrals	\$ 60,023	\$ 136,525
Total	<u>\$ 60,023</u>	<u>\$ 136,525</u>

D. Risk Management

The Authority is exposed to various risks of loss related to torts; theft of, damage to, and destruction of assets; errors and omissions; injuries to employees; and natural disasters. The Authority has property, general liability, flood, automobile, workers’ compensation, crime, public officials, law enforcement, and employment practices coverage. There have been no significant reductions in insurance coverage from the previous year, and settled claims have not exceeded coverage in any of the past three fiscal years.

The Authority carries flood insurance through Alliant Insurance Services, Inc., in the amount of \$10,000,000.

The Authority’s crime insurance covers all employees, with a limit of \$250,000 per loss. The Finance Officer is individually bonded for \$250,000.

E. Net Position

The Authority Board has established an operations and maintenance designation equal to six months of the Airport’s subsequent year’s operations and maintenance budget. The Board has also designated \$650,000 for emergency repairs.

Unrestricted net position consists of the following:

Unrestricted net position, designated for:	
Operating and maintenance	\$ 4,290,100
Emergency repair	650,000
Undesignated, unrestricted net position	<u>10,006,309</u>
Total	<u>\$ 14,946,409</u>

F. Revenue and Expenses

1. Leases and Other Non-Cancelable Agreements

The Authority leases land and facilities to tenants under various cancelable and non-cancelable agreements. Some agreements require a fixed monthly rent and others require a fee that is the greater of a minimum annual guarantee privilege fee or a percentage of gross sales by the tenant. The future non-cancelable minimum annual rentals and privilege fees to be received under agreements in effect at June 30, 2017, are as follows:

Year Ended	Amount
<u>June 30</u>	<u>Amount</u>
2018	\$ 3,184,100
2019	1,191,187
2020	980,993
2021	775,705
2022	475,678
Thereafter	<u>4,934,024</u>
Total	<u>\$ 11,541,687</u>

2. Passenger Facility Charges

The Authority receives from the airline carriers enplaning passengers at the Greater Asheville Regional Airport a facility charge of \$4.50 per passenger. Every air carrier servicing the Airport must collect \$4.50 from passengers on all tickets sold and remit \$4.39 of these funds to the Authority. The Authority must use these funds for Federal Aviation Administration (FAA) approved capital improvement projects. Revenues from passenger facility charges totaled \$1,836,201 for the year ended June 30, 2017, and \$1,615,151 for the year ended June 30, 2016.

Note 3 – Summary Disclosure of Significant Contingencies

Federal and State Assisted Programs

The Authority has received proceeds from several Federal and State grants. Periodic audits of these grants are required and certain costs may be questioned as not being appropriate expenditures under the grant agreements. Such audits could result in the refund of grant monies to the grantor agencies. Management believes that any required refunds will be immaterial. No provision has been made in the accompanying financial statements for the refund of grant monies.

Contingent Liabilities

The Authority is presently a defendant in a lawsuit and is also subject to potential claims which may arise out of the ordinary conduct of its affairs. It is the opinion of the Authority's management that settlement of these matters, if any, will not have a material adverse effect on the Authority's financial position.

Note 4 – Change in Accounting Principles/Restatement

The Authority implemented Governmental Accounting Standards Board (GASB) No. Statement 73, *Accounting and Financial Reporting for Pensions and Related Assets That Are Not within the Scope of GASB Statement 68, and Amendments to Certain Provisions of GASB Statements 67 and 68*, in the fiscal year ending June 30, 2017. The implementation of the statement required the Authority to record beginning total pension liability and the effects on net position of benefit payments and administrative expenses paid by the City to the Law Enforcement Officers' Special Separation Allowance during the measurement period (fiscal year ending December 31, 2016). As a result, net position decreased \$405,116 at July 1, 2016 and \$352,681 at July 1, 2015. The Authority also restated net position by \$1,736 to correct a depreciation error as of June 30, 2016, for a net restatement of \$403,380 at July 1, 2016.

**REQUIRED SUPPLEMENTARY
INFORMATION**

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

**LAW ENFORCEMENT OFFICERS' SPECIAL SEPARATION ALLOWANCE
REQUIRED SUPPLEMENTARY INFORMATION**

For the year ended June 30, 2017

Schedule of Changes in Total Pension Liability

	<u>2017*</u>
Beginning balance	\$ 446,133
Changes for the year:	
Service cost at end of year	21,843
Interest	15,375
Change in benefit terms	-
Difference between expected and actual experience	-
Changes of assumptions and other inputs	(11,280)
Benefit payments	(30,949)
Other	-
Net changes	<u>(5,011)</u>
Ending balance of the total pension liability	<u>\$ 441,122</u>

Schedule of Total Pension Liability as a Percentage of Covered Payroll

Total Pension Liability	\$ 441,122
Covered Payroll	834,877
Total pension liability as a percentage of covered payroll	52.84%

Notes to the schedule:

The Authority has no assets accumulated in a trust that meet the criteria in paragraph 4 of GASB Statement 73 to pay related benefits.

* The amounts presented for each fiscal year were determined as of the prior fiscal year ending June 30.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

**OTHER POST-EMPLOYMENT BENEFITS - RETIREE HEALTH PLAN
REQUIRED SUPPLEMENTARY INFORMATION**

For the year ended June 30, 2017

Schedule of Funding Progress

Actuarial Valuation Date	Actuarial Value of Assets (A)	Actuarial Accrued Liability (AAL) (B)	Funded Ratio (A/B)	Unfunded AAL (UAAL) (B-A)	Covered Payroll (C)	UAAL as a % of Covered Payroll ((B-A)/C)
6/30/2008	\$ -	\$ 752,739	0.00%	\$ 752,739	\$ 1,834,165	41.04%
6/30/2009	-	915,011	0.00%	915,011	2,545,647	35.94%
12/31/2009	-	1,058,716	0.00%	1,058,716	2,265,417	46.73%
12/31/2012	-	1,016,647	0.00%	1,016,647	2,413,329	42.13%
12/31/2015	-	938,046	0.00%	938,046	1,965,966	47.71%

Schedule of Employer Contributions

Year Ended June 30	Annual Required Contribution (ARC)	Percentage of ARC Contributed
2012	\$ 145,738	37.67%
2013	144,929	34.97%
2014	144,929	38.91%
2015	145,955	40.45%
2016	147,062	42.05%
2017	127,779	19.32%

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

AUTHORITY'S PROPORTIONATE SHARE OF NET PENSION LIABILITY (ASSET)
 REQUIRED SUPPLEMENTARY INFORMATION
 Last Four Fiscal Years*

Local Governmental Employees' Retirement System

	<u>2017</u>	<u>2016</u>	<u>2015</u>	<u>2014</u>
Authority's proportion of the net pension liability (asset) (%)	0.05093%	0.05042%	0.04853%	0.05120%
Authority's proportion of the net pension liability (asset) (\$)	\$ 1,080,905	\$ 226,282	\$ (286,204)	\$ 617,157
Authority's covered payroll	3,067,020	2,861,908	2,699,905	2,574,058
Authority's proportionate share of the net pension liability (asset) as a percentage of its covered payroll	35.24%	7.91%	(10.60%)	23.98%
Plan fiduciary net position as a percentage of the total pension liability	91.47%	98.09%	102.64%	94.35%

* The amounts presented for each fiscal year were determined as of the prior fiscal year ending June 30.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SCHEDULE OF AUTHORITY'S CONTRIBUTIONS
 REQUIRED SUPPLEMENTARY INFORMATION
 Last Four Fiscal Years

Local Governmental Employees' Retirement System

	<u>2017</u>	<u>2016</u>	<u>2015</u>	<u>2014</u>
Contractually required contribution	\$ 241,459	\$ 210,066	\$ 204,511	\$ 192,190
Contributions in relation to the contractually required contribution	<u>241,459</u>	<u>210,066</u>	<u>204,511</u>	<u>192,190</u>
Contribution deficiency (excess)	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>
Authority's covered payroll	\$ 3,226,530	\$ 3,067,020	\$ 2,861,908	\$ 2,699,905
Contributions as a percentage of covered payroll	7.48%	6.85%	7.15%	7.12%

SUPPLEMENTARY INFORMATION

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SCHEDULE OF REVENUES AND EXPENDITURES - BUDGET AND ACTUAL (NON-GAAP)

For the year ended June 30, 2017

With comparative actual amounts for the year ended June 30, 2016

	2017			Restated
	Final		Variance	2016
	Budget	Actual	Positive (Negative)	Actual
Revenues:				
Terminal	\$ 4,588,257	\$ 5,079,256	\$ 490,999	\$ 5,133,966
Airfield	898,950	935,323	36,373	1,009,331
General aviation	1,018,890	1,003,966	(14,924)	1,001,333
Parking lot	2,869,850	3,549,920	680,070	3,296,169
Interest income	30,000	90,542	60,542	35,760
Other	214,257	234,107	19,850	336,994
Reimbursable costs	214,000	2,055	(211,945)	54,908
Customer facility charges	1,300,000	1,404,051	104,051	1,354,395
Total revenues	<u>11,134,204</u>	<u>12,299,220</u>	<u>1,165,016</u>	<u>12,222,856</u>
Expenditures:				
Administration	558,288	453,405	104,883	435,500
Development	440,424	396,981	43,443	454,363
Executive	856,693	830,832	25,861	520,962
Finance	447,471	412,904	34,567	382,127
Guest services	212,730	200,407	12,323	186,296
Information technology	830,468	741,888	88,580	676,912
Marketing	551,703	524,627	27,076	506,016
Operations	3,924,849	3,478,834	446,015	3,020,119
Public safety	1,478,390	1,341,362	137,028	1,356,811
Business development:				
Operating expenditures	300,000	177,255	122,745	99,274
Debt service - interest	155,144	207,643	(52,499)	1,663,268
Emergency repair:				
Operating expenditures	150,000	-	150,000	12,989
Reimbursable costs	214,000	2,055	211,945	54,908
Non-signatory fees	-	197,462	(197,462)	-
Contingency	51,639	-	51,639	-
Equipment and small capital outlay	198,361	59,601	138,760	280,224
Renewal and replacement capital outlay	649,187	324,487	324,700	459,581
Capital project expenditures	<u>69,314,896</u>	<u>32,108,503</u>	<u>37,206,393</u>	<u>18,226,631</u>
Total expenditures	<u>80,334,243</u>	<u>41,458,246</u>	<u>38,875,997</u>	<u>28,335,981</u>
Revenues under expenditures	<u>(69,200,039)</u>	<u>(29,159,026)</u>	<u>40,041,013</u>	<u>(16,113,125)</u>

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SCHEDULE OF REVENUES AND EXPENDITURES - BUDGET AND ACTUAL (NON-GAAP)

For the year ended June 30, 2017

With comparative actual amounts for the year ended June 30, 2016

	2017		Restated 2016
	Final Budget	Actual	Actual
		Variance Positive (Negative)	
Other Financing Sources:			
State grant reimbursements	300,000	-	(300,000)
Federal airport improvement program grants	29,527,468	14,284,352	(15,243,116)
Passenger facility charges	1,708,500	1,836,201	127,701
Proceeds from revenue bonds issued	-	21,000,000	21,000,000
Proceeds on sale of assets	-	58,732	58,732
Appropriated net position	37,664,071	-	(37,664,071)
Total other financing sources	<u>69,200,039</u>	<u>37,179,285</u>	<u>(32,020,754)</u>
Revenues and other financing sources over expenditures	<u>\$ -</u>	8,020,259	<u>\$ 8,020,259</u>
Reconciliation From Budgetary Basis (Modified Accrual) to Full Accrual:			
Capital outlay expenditures		384,088	739,805
Capital project expenditures		32,108,503	18,226,631
Proceeds from revenue bonds issued		(21,000,000)	-
Principal payments of bond maturities		-	1,589,996
Increase (decrease) in net pension asset (LGERS)		-	(286,204)
Increase (decrease) in deferred outflows of resources - pensions		710,325	17,524
(Increase) decrease in accrued interest payable		(256,200)	-
(Increase) decrease in other post-employment benefits		(82,957)	(67,545)
(Increase) decrease in accrued compensated absences		(23,110)	55,217
(Increase) decrease in net pension liability (LEO-SSA)		5,011	(72,533)
(Increase) decrease in net pension liability (LGERS)		(854,623)	(226,282)
(Increase) decrease in deferred inflows of resources - pensions		76,502	586,264
Book value of disposed assets		-	(15,839)
Trade-in value of disposed assets		-	44,000
Depreciation		<u>(4,760,215)</u>	<u>(4,675,453)</u>
Change in net position		<u>\$ 14,327,583</u>	<u>\$ 16,745,697</u>

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

CAPITAL IMPROVEMENTS SUPPLEMENTAL SCHEDULE
 SCHEDULE OF REVENUES AND EXPENDITURES - BUDGET AND ACTUAL (NON-GAAP)
 From inception and for the year ended June 30, 2017

	Project Authorization	Actual			Variance Positive (Negative)
		Prior Years	Current Year	Total to Date	
Revenues:					
Federal airport improvement program grants	\$ 59,242,312	\$ 22,459,655	\$ 14,284,352	\$ 36,744,007	\$(22,498,305)
Passenger facility charges	5,139,089	2,357,533	1,602,556	3,960,089	(1,179,000)
State grants	<u>3,700,000</u>	<u>3,700,000</u>	<u>-</u>	<u>3,700,000</u>	<u>-</u>
Total operating revenues	<u>68,081,401</u>	<u>28,517,188</u>	<u>15,886,908</u>	<u>44,404,096</u>	<u>(23,677,305)</u>
Expenditures:					
Airfield Re-Development - Bid Package 1	11,868,128	10,311,504	29,691	10,341,195	1,526,933
Airfield Re-Development - Bid Package 2	13,057,678	12,821,536	1,090,844	13,912,380	(854,702)
Airfield Re-Development - Bid Package 3	16,338,255	5,758,858	10,265,738	16,024,596	313,659
Airfield Re-Development - Bid Package 4	34,703,096	-	5,843,807	5,843,807	28,859,289
Parking Garage Design and Construction	1,627,575	1,127,000	282,443	1,409,443	218,132
Parking Garage	21,938,700	-	13,853,417	13,853,417	8,085,283
ARFF truck	644,839	33	602,971	603,004	41,835
Other	<u>224,000</u>	<u>-</u>	<u>139,592</u>	<u>139,592</u>	<u>84,408</u>
Total expenditures	<u>100,402,271</u>	<u>30,018,931</u>	<u>32,108,503</u>	<u>62,127,434</u>	<u>38,274,837</u>
Revenues under expenditures	<u>(32,320,870)</u>	<u>(1,501,743)</u>	<u>(16,221,595)</u>	<u>(17,723,338)</u>	<u>14,597,532</u>
Other Financing Sources:					
Transfers from operating cash	<u>32,320,870</u>	<u>1,501,743</u>	<u>16,221,595</u>	<u>17,723,338</u>	<u>(14,597,532)</u>
Total other financing sources	<u>32,320,870</u>	<u>1,501,743</u>	<u>16,221,595</u>	<u>17,723,338</u>	<u>(14,597,532)</u>
Revenues and other financing sources over expenditures	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>	<u>\$ -</u>

Note: Schedule represents cumulative activity for active capital improvement projects. Budget and actual activity is accounted for in the Operating Fund; not a separate fund. This schedule is for additional detail of active capital improvement projects.

COMPLIANCE SECTION



Report on Internal Control over Financial Reporting and on Compliance and Other Matters Based on an Audit of Financial Statements Performed in Accordance with *Government Auditing Standards*

Independent Auditors' Report

To the Board of Directors
Greater Asheville Regional Airport Authority
Fletcher, North Carolina

We have audited, in accordance with the auditing standards generally accepted in the United States of America and the standards applicable to the financial audits contained in *Government Auditing Standards* issued by the Comptroller General of the United States, the accompanying financial statements of the business-type activities of the Greater Asheville Regional Airport Authority, as of and for the year ended June 30, 2017, and the related notes to the financial statements, which collectively comprise the Greater Asheville Regional Airport Authority's basic financial statements, and have issued our report, thereon, dated October 31, 2017.

Internal Control Over Financial Reporting

In planning and performing our audit of the financial statements, we considered the Greater Asheville Regional Airport Authority's internal control over financial reporting (internal control) to determine the audit procedures that are appropriate in the circumstances for the purpose of expressing our opinions on the financial statements, but not for the purpose of expressing an opinion on the effectiveness of the Authority's internal control. Accordingly, we do not express an opinion on the effectiveness of the Authority's internal control.

A deficiency in internal control exists when the design or operation of a control does not allow management or employees, in the normal course of performing their assigned functions, to prevent or detect and correct misstatements on a timely basis. A material weakness is a deficiency, or a combination of deficiencies, in internal control, such that there is a reasonable possibility that a material misstatement of the entity's financial statements will not be prevented, or detected and corrected on a timely basis. A significant deficiency is a deficiency, or a combination of deficiencies, in internal control that is less severe than a material weakness, yet important enough to merit attention by those charged with governance.

Our consideration of internal control was for the limited purpose described in the first paragraph of this section and was not designed to identify all deficiencies in internal control that might be material weaknesses or significant deficiencies. Given these limitations, during our audit we did not identify any deficiencies in internal control that we consider material weaknesses. However, material weaknesses may exist that have not been identified.

Compliance and Other Matters

As part of obtaining reasonable assurance about whether the Authority's financial statements are free from material misstatement, we performed tests of its compliance with certain provisions of laws, regulations, contracts, and grant agreements, noncompliance with which could have a direct and material effect on the determination of financial statement amounts. However, providing an opinion on compliance with those provisions was not an objective of our audit, and accordingly, we do not express such an opinion. The results of our tests disclosed no instances of noncompliance or other matters that are required to be reported under *Government Auditing Standards*.

Purpose of this Report

The purpose of this report is solely to describe the scope of our testing of internal control and compliance and the results of that testing, and not to provide an opinion on the effectiveness of the entity's internal control or on compliance. This report is an integral part of an audit performed in accordance with *Government Auditing Standards* in considering the entity's internal control and compliance. Accordingly, this communication is not suitable for any other purpose.

Donald Killian CPA Group, P.A.

Asheville, North Carolina
October 31, 2017



**GOULD KILLIAN
CPA GROUP, P.A.**
CERTIFIED PUBLIC ACCOUNTANTS

Report on Compliance for Each Major Federal Program and Passenger Facility Charge (PFC) Program; Report on Internal Control Over Compliance; With Uniform Guidance and the State Single Audit Implementation Act

Independent Auditors' Report

To the Board of Directors
Greater Asheville Regional Airport Authority
Fletcher, North Carolina

Report on Compliance for Each Major Federal Program

We have audited the Greater Asheville Regional Airport Authority's compliance with the types of compliance requirements described in OMB Compliance Supplement, the Passenger Facility Charge (PFC) Audit Guide for Public Agencies, and the *Audit Manual for Governmental Auditors in North Carolina*, issued by the Local Government Commission, that could have a direct and material effect on each of the Greater Asheville Regional Airport Authority's major federal programs and PFC program for the year ended June 30, 2017. The Greater Asheville Regional Airport Authority's major federal program is identified in the summary of auditor's results section of the accompanying Schedule of Findings and Responses.

Management's Responsibility

Management is responsible for compliance with the requirements of laws, regulations, contracts, and grants applicable to its federal programs and PFC program.

Auditor's Responsibility

Our responsibility is to express an opinion on compliance for each of the Greater Asheville Regional Airport Authority's major federal programs based on our audit of the types of compliance requirements referred to above. We conducted our audit of compliance in accordance with auditing standards generally accepted in the United States of America; the standards applicable to financial audits contained in *Government Auditing Standards*, issued by the Comptroller General of the United States; the *Passenger Facility Charge Audit Guide* for Public Agencies; the audit requirements of Title 2 U.S. *Code of Federal Regulations* Part 200 *Uniform Administrative Requirements, Cost Principles, and Audit Requirements for Federal Awards* (Uniform Guidance); and the State Single Audit Implementation Act. Those standards, the Uniform Guidance, the PFC Program Audit Compliance Guide, and the State Single Audit Implementation Act require that we plan and perform the audit to obtain reasonable assurance about whether non-compliance with the types of compliance requirements referred to above that could have a direct and material effect on a major federal program, or the PFC program, occurred. An audit includes examining, on a test basis, evidence about the Greater Asheville Regional Airport Authority's compliance with those requirements and performing such other procedures, as we considered necessary in the circumstances.

We believe that our audit provides a reasonable basis for our opinion on compliance for the major federal program and the Passenger Facility Charge (PFC) Program. However, our audit does not provide a legal determination of the Greater Asheville Regional Airport Authority's compliance.

Opinion on Each Major Federal Program and PFC Program

In our opinion, the Greater Asheville Regional Airport Authority complied, in all material respects, with the types of compliance requirements referred to above that could have a direct and material effect on its major federal program and the PFC Program for the year ended June 30, 2017.

Report on Internal Control Over Compliance

Management of the Greater Asheville Regional Airport Authority is responsible for establishing and maintaining effective internal control over compliance with the types of compliance requirements referred to above. In planning and performing our audit of compliance, we considered the Authority's internal control over compliance with the types of requirements that could have a direct and material effect on a major federal program, or the PFC Program, to determine the auditing procedures that are appropriate in the circumstances for the purpose of expressing our opinion on compliance for the major federal program and to test and report on internal control over compliance in accordance with the Uniform Guidance and the Passenger Facility Charge Audit Guide for Public Agencies, but not for the purpose of expressing an opinion on the effectiveness of internal control over compliance. Accordingly, we do not express an opinion on the effectiveness of the Authority's internal control over compliance.

A deficiency in internal control over compliance exists when the design or operation of a control over compliance does not allow management or employees, in the normal course of performing their assigned functions, to prevent, or detect and correct, non-compliance with a type of compliance requirement of a federal program on a timely basis. A material weakness in internal control over compliance is a deficiency, or a combination of deficiencies, in internal control over compliance, such that there is a reasonable possibility that material non-compliance with a type of compliance requirement of a federal program will not be prevented, or detected and corrected, on a timely basis. A significant deficiency in internal control over compliance is a deficiency, or combination of deficiencies, in internal control over compliance with a type of compliance requirement of a federal program that is less severe than a material weakness in internal control over compliance, yet important enough to merit attention by those charged with governance.

Our consideration of internal control over compliance was for the limited purpose described in the first paragraph of this section and was not designed to identify all deficiencies in internal control over compliance that might be significant deficiencies or material weaknesses. We did not identify any deficiencies in internal control over compliance that we consider to be material weaknesses. However, material weaknesses may exist that have not been identified.

The purpose of this report on internal control over compliance is solely to describe the scope of our testing of internal control over compliance and the results of that testing based on the requirements of the Uniform Guidance and the Passenger Facility Charge Audit Guide for Public Agencies. Accordingly, this report is not suitable for any other purpose.

Donald Killian CPA Group, P.A.

Asheville, North Carolina
October 31, 2017

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SCHEDULE OF FINDINGS AND RESPONSES
For the year ended 30, 2017

1. Summary of Auditor's Results

Financial Statements

Type of auditor's report issued on whether the financial statements were prepared in accordance to GAAP: Unmodified

Internal control over financial reporting:

- Material weakness(es) identified? _____ Yes X No

- Significant deficiency(ies) identified that are not considered to be material weaknesses? _____ Yes X None reported

- Non-compliance material to financial statements noted? _____ Yes X No

Federal Awards

Internal control over major federal program:

- Material weakness(es) identified? _____ Yes X No

- Significant deficiency(ies) identified that are not considered to be material weaknesses? _____ Yes X None reported

Type of auditor's report issued on compliance for major federal program Unmodified

- Any findings disclosed that are required to be reported in accordance with 2 CFR 500.516(a)? _____ Yes X No

Identification of major federal program:

<u>Name of Federal Program or Cluster</u>	<u>CFDA #</u>
Airport Improvement Program	20.106
Dollar threshold used to distinguish between Type A and Type B Programs	\$750,000
Auditee qualified as low-risk auditee?	<u> X </u> Yes _____ No

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SUMMARY SCHEDULE OF PRIOR YEAR'S AUDIT FINDINGS

For the year ended 30, 2017

There were no prior year audit findings.

GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY

SCHEDULE OF EXPENDITURES OF FEDERAL AWARDS

For the year ended June 30, 2017

Grantor/Pass-Through Grantor/Program Title	Federal CFDA Number	Federal (Direct and Pass-Through) Expenditures	Local Expenditures
Federal Awards:			
<u>U.S. Department of Transportation</u>			
Federal Aviation Administration			
Direct Program:			
Airport Improvement Program	20.106	\$ 12,524,650	\$ 2,163,355
Total federal awards		<u>\$ 12,524,650</u>	<u>\$ 2,163,355</u>

Passenger Facility Charges:

Capital improvements	
Application approved number	11-05-C-00-AVL
Beginning balance, unliquidated Passenger Facility Charges	\$ 2,808,615
Passenger Facility Charges collected	1,820,541
Expenditures	<u>(1,602,556)</u>
Ending balance, unliquidated Passenger Facility Charges	<u>\$ 3,026,600</u>

Notes to the Schedule of Expenditures of Federal Awards:

The accompanying Schedule of Expenditures of Federal Awards includes the Federal grant activity of the Greater Asheville Regional Airport Authority and is presented on the modified accrual basis of accounting. The information in this schedule is presented in accordance with the requirements of Title 2 US *Code of Federal Regulations* Part 200, *Uniform Administrative Requirements, Cost Principles, and Audit Requirements for Federal Awards*; the Passenger Facility Charge Audit Guide for Public Agencies; and the State Single Audit Implementation Act. Therefore, some amounts presented in this schedule may differ from amounts presented in, or used in, the preparation of the basic financial statements.

The Greater Asheville Regional Airport Authority has elected not to use the 10-percent de minimis indirect cost rate as allowed under the Uniform Guidance.



MEMORANDUM

TO: Members of the Airport Authority

FROM: Lew Bleiweis, A.A.E., Executive Director

DATE: November 9, 2017

ITEM DESCRIPTION – New Business Item B

Approval of Lease Assignment, Assumption and Consent with Signature Flight Support

BACKGROUND

In early 2016, Signature Flight Support Corporation (Signature) acquired the sole controlling interest in Landmark Aviation. Specifically, in Asheville, Encore Asheville FBO, L.L.C., (Encore) is doing business as Landmark Aviation. In order to achieve certain operational efficiencies, Signature wants to assume all rights and interest in the lease agreement between the Authority and Encore.

ISSUES

None

ALTERNATIVES

The Board could elect to deny the request to approve the assignment and assumption.

FISCAL IMPACT

None

RECOMMENDED ACTION

It is respectfully requested that the Greater Asheville Regional Airport Authority Board (1) approve the Lease Assignment, Assumption and Consent between Encore Asheville FBO, L.L.C. d/b/a Landmark Aviation and Signature Flight Support Corporation; and (2) authorize the Executive Director to execute the necessary documents.

New Business – Item B

LEASE ASSIGNMENT, ASSUMPTION AND CONSENT

This LEASE ASSIGNMENT, ASSUMPTION AND CONSENT (the "Agreement") is made and entered into as of _____, 2017, by and between the ENCORE ASHEVILLE FBO LLC d/b/a L A N D M A R K A V I A T I O N and d/b/a PIEDMONT HAWTHORNE AVIATION LLC ("Assignor"), SIGNATURE FLIGHT SUPPORT CORPORATION, ("Assignee") and GREATER ASHEVILLE REGIONAL AIRPORT AUTHORITY (the "GARAA" or "Landlord").

WITNESSETH:

WHEREAS, the Asheville Regional Airport Authority (the "ARAA") was created, pursuant to N.C. Gen. Stat. § 160A-462, by the City of Asheville (the "City") and the County of County (the "County"), and, prior to October 12, 2012, operated and maintained the Asheville Regional Airport (the "Airport") in accordance with that Restated and Amended Airport Authority Agreement dated January 22, 2008;

WHEREAS, Assignor and the ARAA are parties to that LEASE AGREEMENT EFFECTIVE the 19th DAY OF FEBRUARY 2011 as amended (the "Lease"), a copy of which is attached hereto and incorporated herein by reference as Exhibit A;

WHEREAS, on June 28, 2012, the General Assembly of the State of North Carolina ratified House Bill 552, Session Law 2012-121 (the "Act"), which created the GARAA to operate and maintain the Airport, in accordance with the Act;

WHEREAS, the GARAA is a body corporate and politic having all of the powers, authority, and jurisdiction enumerated in the Act, as amended, and such other and additional powers and authority as shall be conferred upon it by future acts of the North Carolina General Assembly, including, but not limited to, the power and authority to enter into lease agreements;

WHEREAS, on or about October 12, 2012, in accordance with the requirements of the Act, and pursuant to that First Bill of Assignment, Conveyance, Transfer, Authorization and Grant, the ARAA authorized, granted, assigned, conveyed, transferred, and delivered unto the GARAA all of the ARAA's rights, title, interests and obligations, in and to, and control of, all of ARAA's property, rights and interests, personal and mixed, tangible and intangible, whether contingent or not, and wherever located, including, but not limited to all personal property that is part of the Airport and/or owned, used or considered to be used by the ARAA or the Airport, including all of the ARAA's rights and obligations under the Lease;

WHEREAS, in accordance with the Act, as amended, on or about June 28, 2017 the Federal Aviation Administration issued an Airport Operating Certificate ("Certificate") to the GARAA as the sole sponsor, owner and operator of the Airport;

WHEREAS, Assignee, or its affiliates, has previously acquired, via the acquisition of all the outstanding stock of Landmark Aviation, the ultimate control of Assignor;

WHEREAS, the business of Assignor is conducted in the name of Signature Flight Support, and Assignor and Assignee are now commonly controlled, owned and managed;

WHEREAS, Assignor and Assignee desire to streamline certain operational and organizational matters by contributing the assets of Assignor to Assignee, including but not limited to the Assignor's interest in and to the Lease; and

WHEREAS, concurrently with the receipt of the Certificate, the GARAA received all of the real property previously owned by the City and operated by the ARAA as the Airport, including the Premises described in the Lease by way of Special Warranty Deed recorded in Deed Book 5565, Page 1196 of the Buncombe County Registry and Deed Book 3073, Page 239 of the Henderson County Registry, each recorded as of the 30th day of June, 2017, and the GARAA is now the Landlord under the Lease and is the proper party to consent to the Assignor's request to assign the Lease and Assignee's request to assume the Lease;

WHEREAS, Landlord desires to formally document its position as Landlord under the Lease, while at the same time Landlord desires to consent to such assignment and assumption, upon the terms and conditions hereinafter set forth.

NOW, THEREFORE, in consideration of the premises and mutual covenants and agreements contained herein, the parties agree as follows:

1. Assignment. Assignor assigns, transfers and conveys unto Assignee all of Assignor's rights and interests under the Lease, and assigns, transfers and conveys unto Assignee all of Assignor's obligations and liabilities under the Lease.

2. Assumption. Assignee assumes and agrees to pay, discharge and perform in a full and timely manner all of Assignor's liabilities and obligations under the Lease.

3. Consent. Landlord hereby consents to the foregoing assignment and assumption. Landlord also hereby acknowledges, certifies and confirms to Assignee that, to the best of Landlord's knowledge, no defaults on behalf of Assignor are continuing under the Lease, nor have any other events occurred which with the giving of notice, the passage of time or both would constitute defaults under the Lease.

4. Notices. The addresses for notices to be sent to Assignee are as follows.

Assignee:
Signature Flight Support Corporation
201 South Orange Avenue, Suite 1100
Orlando, Florida 32801
Attn: CFO and General Counsel

5. Full Force and Effect. The Lease shall continue in full force and effect except as modified herein. Assignor and Assignee hereby acknowledge and agree that the GARAA is the current Landlord under the Lease, and the Lease is hereby ratified and confirmed by Landlord and Assignor. In the event of a conflict between the terms and conditions of the Lease and this Agreement, the terms of the Lease shall prevail.

6. Binding Effect. This Agreement shall be binding upon and shall inure to the benefit of Assignor, Assignee, Landlord, and their respective and permitted successors and assigns forever.

7. Counterparts. This Agreement may be executed in any number of counterparts, each of which shall be deemed an original and all of which counterparts together shall constitute one agreement with the same effect as if the parties had signed the same signature page.

[REMAINDER OF PAGE INTENTIONALLY LEFT BLANK

SIGNATURES APPEAR ON FOLLOWING PAGE]

IN WITNESS WHEREOF, the parties have hereunto set their hands and seals as of the day and year first written above.

**ENCORE ASHEVILLE FBO LLC d/b/a
LANDMARK AVIATION, and d/b/a
PIEDMONT HAWTHORNE AVIATION LLC**

By: _____

Name: _____

Title: _____

Date: _____

SIGNATURE FLIGHT SUPPORT CORPORATION

By: _____

Name: _____

Title: _____

Date: _____

The undersigned joins in the execution of this Agreement solely for the purpose of granting its consent to the Assignment in accordance with the terms of Section 3 hereof.

**GREATER ASHEVILLE REGIONAL AIRPORT
AUTHORITY**

By: _____

Name: _____

Title: _____

Date: _____



MEMORANDUM

TO: Members of the Airport Authority

FROM: Michael A. Reisman, A.A.E.
Deputy Executive Director, Development and Operations

DATE: November 9, 2017

ITEM DESCRIPTION –New Business Item C

Approve Agreement for Professional Consulting Services with CHA Consulting, Inc.

BACKGROUND

At its August 11, 2017 meeting, the Airport Board authorized airport staff to proceed with consultant selection for a Terminal Building Assessment Study. A qualifications based selection process was conducted in accordance with FAA requirements and publicly advertised the week of August 14, 2017, with a due date of September 14, 2017. Five firms submitted Statements of Qualifications. A review committee made up of five staff members reviewed the submittals, and based on qualifications, selected CHA Consulting, Inc. as the top ranked firm to perform this study for the Authority.

Due to the need to ensure timely use of new NCDOT appropriations, certain steps that would normally be complete are still in process. These include the conduct of an Independent Fee comparison and final approval of the scope of work and fee by the Federal Aviation Administration. To ensure that the availability of NCDOT appropriations are not jeopardized, staff is conducting these steps simultaneously, with the understanding that a final fee for this project should be less than the Not to Exceed fee that is included in this action.

ISSUES

None.



ALTERNATIVES

The Airport Board could elect not to award this contract at this time. This would result in the postponement of any work associated with terminal building repairs, upgrades, or expansion, and the potential loss of certain NCDOT appropriations.

FISCAL IMPACT

The not-to-exceed cost for this study is \$628,900.00, to be paid for with NCDOT appropriations.

RECOMMENDED ACTION

It is respectfully requested that the Airport Authority Board resolve to (1) approve the award of an Agreement for Professional Consulting services with CHA Consulting, Inc. in an amount not to exceed \$628,900.00 and upon IFE review and concurrence from the FAA, and (2) authorize the Executive Director to execute the necessary documents.

**ASHEVILLE REGIONAL AIRPORT
TERMINAL BUILDING ASSESSMENT STUDY
SCOPE OF SERVICES**

The Greater Asheville Regional Airport Authority (GARAA, or “the Authority”) has requested - _____ Consulting, Inc. (“Consultant Team”) to prepare a scope of services for a Terminal Building Assessment Study (Study) for the Asheville Regional Airport (AVL, or “the Airport”). The outcome of the Study will provide planning and development guidance necessary to address the current status and future needs of the terminal building systems, utilities, physical facilities, and infrastructure. In addition, the Study will include tasks focused on forecasting commercial aviation demand, developing and evaluating conceptual terminal development options, and developing a program to upgrade the terminal building exterior and façade.

TASK 1: PROJECT PROGRAMMING

1.1: Project Definition and Schedule

The scope of services for the Terminal Building Assessment Study will be reviewed with the Authority. Following this review, the program will be further detailed and the level of effort and associated costs of accomplishing each component of this planning effort will be identified. Work activities under this task will include:

- Discussions with Authority staff;
- Preparation of draft study design, budget, and schedule;
- Meeting with Authority to review draft study design, budget, and schedule; and
- Preparation of final work program, budget, and schedule.

1.2: Organizational Meetings

Prior to initiating any effort on the work program, establishing coordination procedures with the Authority will be required to define and clarify the mechanics of the scope of services. This is typically accomplished through a one-day set of “kick-off” meetings. All study team leaders will meet for a full day at the Airport. This will be followed by a meeting with the Authority to review the study organization and for a briefing on Authority requirements, identification of outside coordination points, and identification of data sources.

1.3: Development of Terminal Study Goals and Objectives

Study goals and objectives, which will be discussed at the initial kick-off meeting, that reflect the needs and desires of the Authority will be documented. This task is critical in the development of the Study, for it is these goals and objectives that will shape the conclusions of the overall work program. Specific issues to be addressed in the Study will be identified and prioritized.

TASK 2: INVESTIGATION & ASSESSMENT OF EXISTING INFRASTRUCTURE CONDITIONS AND RECOMMENDATIONS

An important element in conducting a facilities assessment is to first determine if there are any facility access considerations that might create a critical path situation. The Consulting Team, in conjunction with airport administrative, facilities, operations, and maintenance personnel, will develop a site visit schedule that will ensure access to all respective facilities and no interruptions to facility operations. Upon approval from facilities management, the schedule will be forwarded to the requisite facility personnel members indicating date, arrival time, and estimated duration for assessment activities. The assessment will be conducted in three parts as follows:

Part 1

Kick-Off Meeting at AVL

- Identify the AVL point of contact, by discipline, for record drawings and maintenance records
- Identify the AVL point of contact, by discipline, for facilities access
- Identify the members of AVL staff that should be interviewed by the assessment team

Part 2

Assessment of AVL Terminal Facilities by Discipline (Architectural, Structural, Mechanical, Electrical, Plumbing, etc.)

- Interview administrative, operations, IT, and maintenance staff prior to assessment for any items of particular note
- Conduct facility assessment
- Interview administrative, operations, and maintenance staff after assessment to answer any assessment-generated questions
- Prepare condition assessment draft report and submit to the Authority for review

Part 3

Issue Final Assessment Report

- Incorporate Authority's comments into draft assessment report
- Prepare useful service life remaining estimates of major components and their associated replacement costs
- Issue Final Assessment Report

Assumptions

During the conduct of this building assessment study, the Consulting Team will undertake its work effort based on the following assumptions.

- The Consulting Team will not be responsible for classifying hazardous materials or the extent of hazardous materials. If during the course of the assessment, we observe any materials which

may be considered hazardous, we will report their location and existence. No collection or testing of materials are Included.

- Assessment of interior finishes are not included, except as related to roof leaks.
- The CONSULTANT Team will be provided site and building plans for use in the survey.
- AutoCAD floor plans and standards, if available, will be provided by the Authority.
- The CONSULTANT Team staff providing the facility condition services will be provided escorted access to all building areas, as necessary.
- Areas requiring partial demolition for access or are not otherwise accessible will not be included in the survey.
- All surveys are visual in nature. There is no destructive testing, or sampling included.
- Where exterior visual observation of building conditions requires special access equipment (lifts), the Authority shall make arrangements to provide such equipment. Otherwise, typical equipment used in the facility condition fieldwork will include digital cameras, binoculars, flashlights, and step ladders.
- Preparation of engineering/architectural design documents to correct existing deficiencies observed is not required.
- For recommended equipment replacement or upgrades, a life cycle cost analysis with simple payback will be included. If replacement is based on efficiency (rather than age), the CONSULTANT Team will need the Authority to provide energy consumption data for the specific equipment going back for a specified time frame.
- Interviews are limited to Authority administrative, operations, and maintenance staff. No interviews are included with airlines, TSA, concessionaires, or other user groups.
- OSHA issues are not included in the assessment. However, if any safety-related issues are observed, they will be noted and reported.

2.1: Pre-Assessment Work Tasks

2.1.1: Conduct a kick-off meeting with the Authority staff and Key CONSULTANT team members to review the overall project scope schedule and plan for completing the assessment.

2.1.2: At the kick-off meeting and prior to initiating our survey work in the field, the CONSULTANT Team will review existing reports (including the 2012 RAMP Plan) and obtain through the Authority any building/site plans that are available. CONSULTANT will develop a template for each location to be assessed and down-load available information including, but not limited to, aerial photos, building floor plans, and site survey maps.

2.1.3: After the existing data is reviewed, CONSULTANT will develop a custom project GIS database (Microsoft SQL Server - ArcSDE) to complete the facility assessments. The purpose of the project GIS database will be to create a project repository for record documents and files, tabulate information regarding site characteristics and background information, and setup a template to facilitate standardized field data collection and site information. We will develop multiple GIS feature classes specific to facility assessments, as well as specific features for architectural, site, mechanical, electrical, and plumbing assessment activities. For this project, we will utilize a relational geodatabase which will support both related tables and attachments for each specific feature (such as pictures, record files, reports, etc.). Existing data will also be imported, or digitized, as background layers or attachments to the database so that these files will also be available to the team during the

assessment phases of the project. In addition to providing GIS database we will also develop custom reporting directly from the project GIS database. These reports will include information collected as part of each facility assessment, recommendations for improvements or corrective actions, as well as photos taken during the assessment. Reports will be available for the overall facility as a whole, and for each asset. CONSULTANT will provide a final copy of the GIS data base to the Authority upon completion of the assessment.

2.1.4: The CONSULTANT Team will review the database with the Authority prior to conducting the field work for the visual assessment. This review will be completed at the kick-off meeting and adjustments to the database will be made based on Authority input.

2.1.5: Conduct Pre-Assessment interviews with Authority administrative, operations, and maintenance staff. These interviews will be conducted in one site-visit by key CONSULTANT team members.

2.1.6: Based on feedback from the Authority reliable as built drawings of under slab plumbing do not exist. To document the locations of under slab plumbing, our team will locate existing under slab plumbing using ground penetrating radar techniques, to the extent possible.

2.2: Building Condition Assessment

2.2.1: During the period in which the field work for the Facility Condition Assessment is in progress, the team leader, supported by technical disciplines, will interview administrative, operations, and maintenance personnel associated with each facility regarding existing conditions. The CONSULTANT Team will use the aforementioned GIS database template with each assessment team. Each team member will use a tablet that has searchable, tabular views for data entry specific to each facility. Attributes contained within the assessment template software include:

- Building name
- Aerial photo
- Location map
- Facility identification and address
- Date and time survey undertaken
- CONSULTANT survey team members
- Facility type(s)
- Square feet (if available)
- Year built (if available)
- Year renovated (if available)
- Building and site conditions
- Interior building conditions
- Comments/recommendations
- Digital photos
- Recommendations
- Order of magnitude cost opinions

2.2.2: The classification of survey conditions that will be completed include the following major categories:

- Site Features (water and sanitary)
 - Flow test to determine water service pressure
 - Exterior sanitary collection system including television inspection of subsurface lines
 - Exterior water distribution system (from record drawings)
 - Potential to loop the existing water service
- Exterior Building Envelope
 - Roof
 - Windows
 - Doors
 - Facade
- Building Systems
 - Mechanical (HVAC)
 - Electrical
 - Plumbing
 - Fire Protection
 - Security/IT
 - Fire Alarm
 - Safety/Risk
 - ADA
 - Elevators and Generators
 - Other IT cable trays, chases, and access points

2.2.3: The template will include the following priority classifications for conditions found:

- a. **Good:** Properly functioning and materials are sound
- b. **Satisfactory:** Properly functioning minor repair associated with normal maintenance
- c. **Adequate:** Minor level of repair to correct deficiencies
- d. **Poor:** Major repair required to correct deficiency
- e. **Very Poor:** Not functioning or system is missing

2.2.4: Prior to leaving the site, our team will conduct post interviews with Authority administrative, operations, IT, and maintenance staff to solicit input or comments on observed conditions.

2.3: Assessment Report

2.3.1: The CONSULTANT Team will prepare an overall assessment report that will be provided to the Authority in both digital and pdf format. The report will include the following:

- Cover page
- Aerial photo of the facility and facility site
- Executive Summary
 - Summary of assessment
 - Property information
 - Follow-up recommendations
 - Cost estimates
 - Description of assessment methodology

- Assessment findings
- Order of magnitude cost opinions including life-cycle costing of major equipment replacements
- Appendices and/or supporting documentation
 - Data summary from data base
 - Potential recommendations for new or differing systems
 - Photos
 - Basic floor plans
 - Building systems inventory

2.3.2: The CONSULTANT Team will present a draft report and our findings to the Authority at an on-site review meeting. Once comments are received from the Authority we will issue the final building condition assessment report.

2.3.3: Data Import Compatibility – The CONSULTANT Team will provide a Microsoft Excel template (hard copy and electronic) of our findings for use by the Authority. The spreadsheet will be populated with the following information:

- Building type
- Area type within building
- System group
- System classification
- System type
- Repair/replacement cost opinions
- Condition assessment code
- System needs

TASK 3: SIZING/SPACE REQUIREMENTS AND CONCEPTUAL PLANNING

Existing reports and studies pertaining to the Airport will be compiled and reviewed by the CONSULTANT Team. Such studies will provide essential background and reference information to enhance the understanding of existing and projected on-Airport development activities, as well as provide insight into relevant planning issues and constraints. Included among these previous and on-going planning efforts are the 2012 Airport Master Plan and any other relevant planning efforts associated with the development of AVL. A brief description of all recommendations, issues, or concerns presented in the various existing or on-going relevant studies will be described.

The CONSULTANT Team will review existing data and the terminal building conditions to establish an understanding of the current space utilization and operational characteristics of the passenger terminal building including access. Circulation patterns of passenger and baggage will be documented for reference in all proposed concept design alternatives. The Consultant Team will undertake a comprehensive reconnaissance of the public and non-public areas of the passenger terminal building, with particular emphasis on some of the older portions of the building that may not have accurate information that is readily available. On-site observations of passenger activity and circulation will be observed during busy periods of the day (early morning, early afternoon, and late afternoon). These observations will be conducted of various critical components of the terminal building, including the

security screening checkpoint, passenger hold rooms, ticket lobby, and baggage claim area. It is anticipated that the Authority will provide the CONSULTANT Team with editable electronic CADD files of the AVL terminal building, which will be supplemented by the data that is compiled by the Team's on-site inspection. The intent of this exercise is to develop an accurate and up-to-date layout plan of the terminal building as it currently exists.

Below is a list of meetings required for the completion of Task 3. These meetings will occur on-site with representatives of the CONSULTANT Team and the Authority:

1. Terminal Area Forecast Development Kickoff/Stakeholder Interviews/Site Assessment (3 days)
2. TAF Review/Terminal Program Review
3. Concept Development Workshop
4. Second Concept Development Workshop
5. Third Concept Development Workshop

3.1: Forecasts of Commercial Service Activity

Current data on commercial passenger and aircraft activities (air carrier and charter) at AVL will be collected to develop a profile of airport operations, including changes in use and levels of operations in recent years. Data sources will include but not be limited to: FAA Air Traffic Statistics; FAA Terminal Area Forecasts; AVL's monthly and annual activity summaries; and surveys of major passenger and charter carriers at AVL. This material will include information to be obtained from the Authority, aircraft operators, and FAA records, and interviews.

3.1.1: Inventory of Socioeconomic Data

Data on population and other socioeconomic factors will be obtained for use in subsequent forecasting analyses. The primary source of the population data will be the Authority, supplemented by other sources and agencies, as necessary. Socioeconomic data will be secured from Woods & Poole and will be verified with local government agencies.

3.1.2: Develop Air Service Assumptions

Building upon other recently-prepared projections of passenger and commercial aircraft activity, updated projections will be developed for the 5-, 10-, and 20-year planning horizons at AVL. The following elements will be included in the forecasting exercise.

The purpose of this effort is to develop reasonable air service scenarios to serve as the basis for projecting aviation activity for future planning horizons. These scenarios will be used in the formulation of passenger terminal development conceptual plans.

Current air service investigations and marketing programs will be the primary source of information. Three forecasting scenarios will be developed and are expected to include: growth by legacy air carriers across the board; establishment and growth of low-cost airlines and ultra-low-cost airlines; growth of regional carriers; impact of airline mergers/bankruptcies, and growth by charter/international airlines.

Levels of Airport catchment area activity, including population and economic growth which affect passenger demand will be identified and accounted for in the different forecast scenarios. Demographic data that has been compiled in Task 3.1.1 will be used for this task. Estimates of passenger demand on a market-by-market basis will be developed for use in identifying the most viable markets and how those markets might best be served. This city-pair market analysis will be an important tool in developing assumptions for the Study.

Interviews with Authority staff, airline personnel, tenants/users, and the Authority's air service consultant will be conducted to provide the most current data on existing and projected passenger activity at AVL. The need for an updated passenger growth forecast will prove beneficial so that the Authority and the CONSULTANT Team has the most up-to-date data for planning purposes.

3.1.3: Forecast Passenger and Commercial Carrier Operations

The CONSULTANT Team recommends that the Study effort consider several varying growth scenarios using "trigger-point" activity levels. This approach will allow the Authority to have the greatest amount of flexibility in its overall development program and in its ability to carefully monitor what improvements may be needed and when. The baseline scenario will be derived from the most logical future growth scenario and extended through the 20-year planning horizon.

Forecasts for enplanements and commercial air carrier operations will reflect projected national and local economic conditions, airline service levels, future fleet mix requirements, federal requirements, hubbing characteristics, and service by low-cost carriers, scheduled charter, international, and regional airlines. It will also utilize the demographic data that was obtained in Task 3.1.1. Standard forecasting methodologies will be used in combination with judgmental assessments based mainly on discussions with the air carriers currently serving AVL and on air service assumptions developed in Task 3.1.2.

Direct input will be sought from passenger carrier representatives operating in the Asheville market in order to gain any relevant insight on current and planned operations, market conditions, and/or constraints that may have an impact on the forecast assumptions or eventual enplanement/operations projections.

3.1.4: Anticipated Commercial Carrier Fleet Mix

Derivative forecasts of aircraft departures, operations, seat departures, and fleet mix will be developed based on the enplanement forecasts and assumptions on aircraft size. Load factors will also be derived in this exercise. Future shifts in aircraft type and size will be based on individual air carrier current and projected fleets (aircraft type), and the requirement to meet forecasted enplanements, operations frequency, origin and destination pairs (destinations served), and target load factors.

3.1.5: Identify and Summarize Preferred Forecast Scenario

Working closely with Authority staff, the CONSULTANT Team will summarize and evaluate the different forecast scenarios for each operation type (passenger enplanements and aircraft operations) and arrive at the Preferred Forecast Scenario. This scenario will be used for the primary calculations of facility requirements, with the range of the other scenarios used to evaluate the extent of flexibility which must be built into subsequent plan development. The CONSULTANT Team will strive to accommodate

uncertainty in forecasts by developing alternative scenarios which assume a variety of critical assumptions within the base case forecasting framework.

3.1.6: Identify Peak Activity and Critical Aircraft

Annual, busy-day, and peak-hour passenger and aircraft activity levels for air carriers will be forecasted. Peak activity forecast will be derived from the Authority historical records on passenger and aircraft operational activity. These peaking assumptions will be extended through the 20-year planning horizon. Data obtained from the Authority and air carriers operating at AVL will be used to identify schedule peaks. Peak enplanement activity will be used to establish the terminal and ancillary support facility planning requirements; peak operations will be used to establish terminal ramp requirements for aircraft gates and parking positions. Using forecasted operations aligned with the anticipated fleet mix, the critical aircraft to be used for terminal design standards will be identified.

3.1.7: Forecast Review

The forecast horizon years will include 2023, 2028, 2033, and 2038. A preliminary forecast of all components covering the 20-year planning period will be prepared for Authority review. The forecasts will then be revised, if necessary, and transmitted to the FAA for their review and approval, if the Authority desires to seek FAA input.

3.2: Determine Terminal Space and Sizing Requirements

3.2.1: Perform Building Code Analysis

A high-level building code review will be conducted for the existing facility identifying major deficiencies or capital intensive upgrades that would be required to bring the facility in compliance with current codes. This could include updating of fire protection systems, earthquake retrofits, and energy compliance upgrades. The review will also identify the agencies from which coordination and approval will be required. An OSHA assessment is not anticipated.

3.2.2: Use Factors and Planning Standards to be used in Identifying Requirements

We propose to utilize IATA's Level of Service (LOS) of "Optimum" in establishing measures contributing to passenger service at AVL. This level will be used to determine demand triggers to define those facilities that require reconfiguration or expansion. The design team will compare the existing terminal components to projected passenger facility demands for the current, and proposed horizons. This will identify deficiencies of the existing facility both now and in the future as passenger traffic increases. Deficiencies will be identified with an indication of where capital investment is most needed.

3.2.3: Stakeholder Interviews

Our team will meet with stakeholders in a two-day series of meetings to review space requirements for current and estimated future operations. Questionnaires will be developed and transmitted to participants prior to the meetings for review, completion and return to facilitate

efficient review of the data. Upon airport approval, final data will be incorporated into the identification of future terminal requirements.

3.2.4: Existing Terminal Facilities Assessment

Our team will review existing plans to determine the existing space allocation of the terminal building with regard to the major space components. An on-site and condition of the primary building systems as they exist currently and with regard to potential for future expansion. The results of the existing space allocation will be compared to the future terminal space requirements to determine deficiencies requiring remediation in the development of future concepts. The following terminal building components will be addressed in this task and in Task 3.2.5.

- Passenger Holdrooms
- Ticket Lobby and Counter Area
- Passenger Flow and Circulation
- Security Screening of Passengers and Baggage
- Baggage Claim Area and Inbound Baggage Area
- Baggage Make-Up Area
- Concessions
- Airline, Rental Car, and Other Tenant-Leased Space
- Airport Authority Administrative Space
- Air Carrier Aircraft Gates and Parking Positions
- Parking for RON Aircraft

3.2.5: Identify Future Terminal Requirements

Our team will quantify the near-term requirements for increasing capacity of the various functional areas identified above within terminal, as well as the long-range requirements for the renovation and expansion of the passenger terminal. Planning standards and functional facility requirements and gross area projections will be developed for all terminal components including the ticket lobby, departure area, arrival area, concessions, airside concourses (including holdrooms and associated customer service facilities), public circulation and other passenger amenities (restrooms, etc.), baggage and passenger screening, baggage sortation, airline operations space and building systems areas. ACRP planning guidelines will be utilized in the development the programmatic areas and evaluation of alternatives and adjusted in accordance with approved stakeholder requirements.

Our team will collaborate with the Authority, to identify the advantages and disadvantages of available technologies to enhancing Airport capacity and facility use. Areas to be considered will include remote check-in, self-bag tagging, common-use check-in kiosks, enhanced passenger security screening equipment, and potential improvements to baggage processing and handling, among other factors, which may contribute to expanding the capacity of existing facilities. The costs and timing for implementation associated with the integration of the potential technologies into the airport environment will be evaluated against the benefits to the airport, airlines, and passengers.

3.2.6: Determine Future Aircraft Gate Requirements

Future aircraft gate requirements will be determined by comparing the existing number of gate positions to those required based on anticipated future aircraft schedules developed to utilize the capacity of the Airport. Future gate demand will be analyzed by building a gate model (ramp chart) based on the existing design day schedule for each potential future aircraft schedule, a similar determination will be made for RON aircraft parking positions. The benefit to maximizing gate use through incorporation of common use terminal facilities and equipment to allow the sharing of facilities will be reviewed.

3.2.7: Review and Approval of Recommended Program

The team will meet with the airport to review and confirm the acceptability of the proposed program requirements prior to initiating the Concept Development Phase.

3.3: Conceptual Design Scenarios

3.3.1: The development of alternatives will be an interactive process, developed through a series of workshops. The emphasis will be on the development of an interactive working session with a free and open exchange of ideas, information, and concepts. The workshops will facilitate the incorporation of ideas and suggestions into the proposed terminal alternatives. This process is intended to directly involve the Authority staff in the development of options and also provide a background of knowledge that would be difficult to obtain in more traditional settings. To the extent feasible, certain preliminary materials will be provided prior to the workshops to allow the Authority to obtain a general indication of the direction of potential alternatives.

3.3.2: Concept Development

The team will develop alternatives utilizing the approved program data for review at the Kickoff Workshop. The alternatives will be depicted in scaled plan diagrams, sketches and other medium to identify relationships, functionality, and passenger processing flows for inbound and outbound passengers as well as the relationships to supporting airside and landside functions. The preferred alternatives outlined in the 2012 Master Plan for AVL for the terminal area will be included as one of the alternatives.

3.3.3: Concept Development Workshop

A. Our team will conduct an initial workshop that takes the preliminary concepts developed and have an on-site interactive workshop to work with the Authority to review the initial concepts and generate alternatives for consideration.

B. Our team will discuss the merits, applicability, and preferences of the Master Plan

alternatives as it relates to this study, incorporating those that still meet the goals/objectives of the Authority moving forward. Refinements to these alternatives may be incorporated to align them with this study.

C. In addition to the previously developed alternatives of the Master Plan, initial alternative/concept discussions will focus on the highlighted areas outlined in the request for proposals, specifically:

- Terminal alternatives - Restaurant/concession locations, passenger processing, security checkpoint location, administration/operations office location, tenant areas, baggage systems and claim areas.
- Landside alternatives - Terminal curbside capacity and configuration, vehicular parking, and general ground transportation improvements.
- Airside alternatives – Apron/ GSE circulation, jet bridge alignment and capacity, aircraft configuration, deicing areas.

D. At the end of the Concept Development Workshop, the goal is to have a general direction, as to the desires of the Authority for its preferences under each of the areas described above for further refinement. The most promising alternatives (maximum of three) will be identified for further study.

3.3.4: Alternatives Development

A. Upon determination of the various near-term and long-term improvements in Workshop 1 selected by the Authority, initial alternatives will be advanced into simple plan diagrams to illustrate the full range of reasonable solutions. Documentation of the alternatives will include block floor plans, massing studies, preliminary sections delineating the functional aspects, and site/building sections showing the relationship of levels, spaces, and functional requirements. Both narrative and graphic format will be used to explain the range of facilities assessed as part of the terminal modernization analysis.

B. The sequence in which these improvements may be implemented incrementally and economically will be discussed. The capacity of the terminal and landside facilities will be balanced with the capacity of the proposed airside facilities at each step of the terminal development process.

C. Our team will use a variety of presentation formats including sketches, and/or studies showing the general architectural character of each alternative.

D. Using the discussion of the initial workshop, requirements, and potential capacity enhancements identified previously, we will develop up to three (3) alternative terminal concepts for presentation to and evaluation by the Authority and its advisory committee in

Workshop 2 (Meeting 3). Solutions will be developed and assessed to meet both near-term and long-range requirements within the existing Terminal complex. Tradeoffs between capacity, cost, and complexity will be considered and identified in the alternative evaluations.

3.3.5: Second Concept Development Workshop

A. Based on the comments received from the Authority and other stakeholders during the Kick-off Workshop, the analyses and concepts will be revised and refined. The refinement process will result in an integrated concept that can be implemented incrementally. Criteria to be reviewed include:

- Extent to which proposed alternatives provide required capacity enhancement;
- Effect of the alternatives on other Airport facilities and development potential;
- Constructability and ease of implementation phasing of the alternatives;
- Benefit from an airline operational cost perspective
- Operational considerations;
- Planning level development/construction costs;
- Conformity with LOS criteria;
- Flexibility and future expansion potential
- Other factors identified.

B. Facility infrastructure considerations identified at this stage will include:

- Narrative descriptions of alternatives for building mechanical systems, electrical distribution and communications systems for energy management
- Descriptions of options for people moving systems, escalators and moving walkways
- Baggage system approach narratives and layouts
- Aircraft parking layouts and Passenger Loading Bridge configurations
- Aircraft ramp and taxiway modifications
- Utility infrastructure narratives
- IT/Communication distribution
- Water distribution
- Identification of sustainable opportunities that may be implemented.

C. Our team will develop conceptual plans illustrating the configuration of the airport at significant developmental stages, showing major construction elements and any significant temporary facilities. Particular attention will be paid to minimizing impacts to existing and future operations while maintaining operating capacity. Furthermore, the program will prioritize acceleration of the completion of new revenue-generating facilities, to promote the economic viability of the overall program. The phased program development alternatives will be used in evaluating alternative terminal expansion concepts, as well as in the refinement and preliminary design of the preferred alternative.

- *Prepare order of magnitude cost estimates* - Budgetary capital construction costs will be prepared for each of the three alternatives.
- *Prepare draft reports* – A terminal study report compiling information from all aspects of the scope of work will be prepared and include a cover, index, executive summary, goals and objectives, condition assessment, programming documents, preferred alternative exhibits, and appendix. This report will be included in a consolidated report that documents the overall project. Both electronic and hard copy versions will be provided.
- The design team will develop a matrix to rank the solutions according to how they satisfy the identified goals, with cost as a factor. This information will be presented and discussed to identify a preferred alternative

D. Third Concept Design Workshop

Final exhibits for the selected alternative will be updated and finalized per comments from the Second Concept Development Workshop will be prepared and presented. These exhibits will include the plans, sections, elevations, and/or 3D imaging for the terminal building. Diagrams of airside and landside development will be included along with a draft of the final report.

E. FINAL REPORT

Comments from the Third Concept Design Workshop shall be incorporated and three 11" x 17" hardcopies shall be submitted along with digital copies of plans, sections and elevations in AutoCAD format (latest version).

TASK 4: EXTERIOR FAÇADE RENEWAL

4.1: Inspection and Investigation of Exterior Building Façade

The CONSULTANT Team will review existing documents and perform a field investigation to verify existing curbside and exterior façade conditions.

4.2: Consideration/Evaluation of Current Curbside Canopy

The CONSULTANT Team will review the existing curbside canopy and outside conditions as a basis of design in consideration of potential design solutions to provide an upgrade to weather protection for departing and arriving passengers.

4.3: Recommendation for Upgrade, Remodel, and Replacement

Recommendations for changes in height and/or construction or replacement in its entirety will be developed and submitted for review. The results of the investigation, evaluation and recommendations will be documented and included in the overall report.

4.4: Conceptual Design Scenarios

4.4.1: Façade Renewal Kick-Off Meeting

An on-site meeting shall be held to review and establish the goals and objectives of proposed enhancements to the façade of the existing AVL terminal. The meeting shall include a review of the existing façade and a discussion to include the potential for energy reduction, enhanced wayfinding, curbside weather protection for passengers, and general appearance. Alternative solutions (maximum of three) will be developed depicting multiple options for façade enhancements reflecting a variety of materials including metal, glass, and stone.

4.4.2: Façade Enhancement Concept Development

The design team will develop plan, building section and elevation exhibits for the identified alternative based on comments from the Façade Enhancement Kickoff Meeting depicting dimensional criteria, materials, and clearances required for incorporation.

4.4.3: Second Façade Enhancement Concept Meeting

An on-site meeting shall be held to present façade enhancement concept development exhibits for review and comment.

4.4.4: Final Façade Enhancement Concept Exhibits

Final exhibits shall be updated per comments received at the second façade enhancement concept meeting and submitted with the overall report.



MEMORANDUM

TO: Members of the Airport Authority

FROM: Michael A. Reisman, A.A.E.
Deputy Executive Director, Development and Operations

DATE: November 9, 2017

ITEM DESCRIPTION – New Business Item D

Approve Change Order Nos. 7 and 8 to Contract with American South General Contractors

BACKGROUND

The Airport Board approved the contract with American South General Contractors for construction of the parking garage project on July 22, 2016 in the amount of \$20,244,000.00. Subsequently, Change Order No. 1 was approved on October 20, 2016 in the amount of \$11,347.53, Change Order No. 2 was approved on November 30, 2016 in the amount of \$30,966.87, Change Order No. 3 was approved on January 3, 2017 in the amount of \$51,290.76, Change Order No. 4 was approved on April 21, 2017 in the amount of \$79,436.59, Change Order No. 5 was approved on June 2, 2017 in the amount of \$31,229.87, and Change Order No. 6 was approved on August 22, 2017 in the amount of \$9,058.26 (\$213,329.88 combined).

Change Order No. 7 includes expenses associated with adding a closure wall at Stair B, additional contract time and cost for AT&T utility delay, adding crawl space light fixtures, adding ramp delineators and signs, undercut for slab on grade associated with weather, asphalt patch at water meter location, bollards in the ground transportation lot, relocation/re-installation of signs and seal coat for ground transportation lot.

Change Order No. 8 includes expenses associated with adding elevator cants, aluminum caps on metal panel tubes, additional parking system electrical needs, stop signs, interior painting of two stair towers and electrical rooms, and herbicide and weed barrier at the generator site.

New Business – Item D



ISSUES

None.

ALTERNATIVES

For Change Order No. 7, there are no alternatives, as each of the identified items is required to successfully complete the project and maintain an appropriate level of safety.

For Change Order No. 8, all items are required to successfully complete the project and maintain an appropriate level of safety except for the stair tower painting and herbicide and weed barrier. These items are recommended however to provide an appropriate level of aesthetic consistency with the overall project.

FISCAL IMPACT

The total additional cost associated with Change Order No. 7 is \$176,158.25. The total additional cost associated with Change Order No. 8 is \$55,987.82. These will adjust the total contract price to \$20,689,475.95, which is within the current construction project budget of \$21,938,700.00, which includes allowances. Total change orders to date represent a 2.2% increase from the original contract price.

RECOMMENDED ACTION

It is respectfully requested that the Airport Authority Board resolve to (1) approve Change Order Nos. 7 and 8 to the contract with American South General Contractors; and (2) authorize the Executive Director to sign the necessary documents.

CHANGE ORDER NO. SEVEN (7)

PROJECT (Name and address):

Construct Parking Garage
Asheville Regional Airport
61 Terminal Drive, Suite 1
Fletcher, NC 28732

CHANGE ORDER NUMBER: 007**DATE:** 10/05/2017**DELTA PROJECT NO:** 15086 OWNER PRIME CONSULTANT CONTRACTOR FIELD OTHER**TO CONTRACTOR (Name and address):**

American South General Contractors
60 N. Merrimon Ave. #101
Asheville, NC 28804

CONTRACT DATE: September 6, 2016**CONTRACT FOR:**General Construction of Parking Garage

THE CONTRACT IS CHANGED AS FOLLOWS:

(Include, where applicable, any undisputed amount attributable to previously executed Construction Change Directives)

COR #22	Closure Wall at Stair B	\$5,017.80 ADD
COR #23	Additional Contract time Time & Cost for AT&T utility	\$80,333.54 ADD
COR #25	Crawl Space Fixtures	\$11,074.59 ADD
COR #26	Ramp Delineators & Signs	\$1,509.19 ADD
COR #27	Undercut for Slab on Grade (SOG)	\$21,847.93 ADD
COR #28	Asphalt Patch at Water Meter	\$789.59 ADD
COR #29	Install Bollards in Ground Transportation Lot	\$20,244.59 ADD
COR #30	Relocaton / Reinstallation of Signs in New Location	\$23,656.16 ADD
COR #31	Seal coat for Ground Transportation Lot	<u>\$11,684.86 ADD</u>
TOTAL:		\$176,158.25 ADD

Terms and Conditions: In consideration of this modification agreed to herein as complete equitable adjustments, the Contractor hereby releases the Owner from any and all liability under this Contract for further equitable adjustments as it directly or indirectly relates to all items enumerated in this Contract Modification. This Contract Modification represents full and final accounting for all direct and indirect costs associated with the Work described above, both in terms of cost and time.

REASON FOR CHANGE ORDER(S):

COR #22 - Additional work to create a "Closure Wall" between the stairs and the retaining wall from the ground floor up to the second-floor elevation.

COR#23 - Additional Contract Time of 49 days at \$1,639.46 / day for AT&T Utility Relocation.

COR #25 - Work for the added doors, frames, hardware, and electrical fixtures to be added to crawlspace per ASI-07.

COR #26 - Additional work for the added reflectors, delineator posts, and signs for exterior speed rap per ASI-09.

COR #27 - Subgrade undercut for the SOG because of unusually high rain falls. Price based on established unit price at bidding.

COR #28- Additional work required to move the water meter from the island into existing paved parking area. Price of asphalt based on established until price at bidding.

COR #29 - Installation of bollards necessary to reconfigure existing rental car lot as a Ground Transportation Lot.

COR #30 - Relocation of existing wayfinding signage to provide better visibility.

COR #31 - Placement of sealcoat on existing rental car lot. Approximately 4328 SY @ \$2.70/SY.



CHANGE ORDER NO. SEVEN (7)

The original Contract Sum was	\$ 20,244,000.00
The net change by previously authorized Change Orders	\$ 213,329.88
The Contract Sum prior to this Change Order was	\$ 20,457,329.88
The Contract Sum will be increased by the Change Order in the amount of	\$ 176,158.25
The new Contract Sum including this Change Order will be	\$ 20,633,488.13

The Contract Time will be increased by Forty-Nine (49) days.
The date of Substantial Completion as of the date of this Change Order therefore is November 22, 2017.

NOTE: This Change Order does not include changes in the Contract Sum, Contract Time or Guaranteed Maximum Price which have been authorized by Construction Change Directive until the cost and time have been agreed upon by both the Owner and Contractor, in which case a Change Order is executed to supersede the Construction Change Directive.

NOT VALID UNTIL SIGNED BY THE PRIME CONSULTANT, CONTRACTOR AND OWNER.

<u>Delta Airport Consultants</u> PRIME CONSULTANT <i>(Company name)</i>	<u>American South General Contractors</u> CONTRACTOR <i>(Company name)</i>	<u>Greater Asheville Regional Airport Authority</u> OWNER <i>(Company name)</i>
11111 Carmel Commons Blvd., Ste 435 Charlotte, NC 28226 ADDRESS	60 N. Merrimon Ave. #101 Asheville, NC 28804 ADDRESS	61 Terminal Drive, Suite 1 Fletcher, NC 28732 ADDRESS
 Digitally signed by Kenneth W. Moody Date: 2017.10.25 12:11:05-04'00'		
BY <i>(Signature)</i>	BY <i>(Signature)</i>	BY <i>(Signature)</i>
Kenneth W. Moody, P.E., C.M. <i>(Typed Name)</i>	Bill Morgan, Vice President <i>(Typed Name)</i>	Lew Bleiweis / Executive Director <i>(Typed Name)</i>
10/25/2017 DATE	10/25/17 DATE	DATE

Greater Asheville Regional Airport Authority

Project Title: Parking Garage. This page has been added to this correspondence to ensure compliance with State laws. This signature page is an addition to Change Order 7 associated with Project listed.

This instrument has been pre-audited in the manner required by the Local Government Budget and Fiscal Control Act.

Finance Officer

Date

Change Order Request # 22

BUILDING ON A GREAT TRADITION

24-Aug-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #22

Dear Mr. Moody

This Change Order Request is for the additional work to create the "Closure Wall" between the stairs and retaining wall from the ground floor up to the second floor elevation as shown in RFI-059 ASK Detail issued by GS&P. This includes the angle, drywall, and painting portions of this work.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Welding Unlimited	1	LS	1,381.73	1,381.73
Xtreme Drywall	1	LS	2,678.00	2,678.00
Southern Painting	1	LS	456.74	456.74
	1	LS		0.00
SUBTOTAL			4,516.47	4,516.47
OH&P			10.00%	451.65
SUBTOTAL				4,968.12
BOND			1.0%	49.68
TOTAL CHANGE ORDER PROPOSAL				5,017.80

We have included page 6 of ASI03-R1 of where this work shall take place along with the appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$5,017.80**

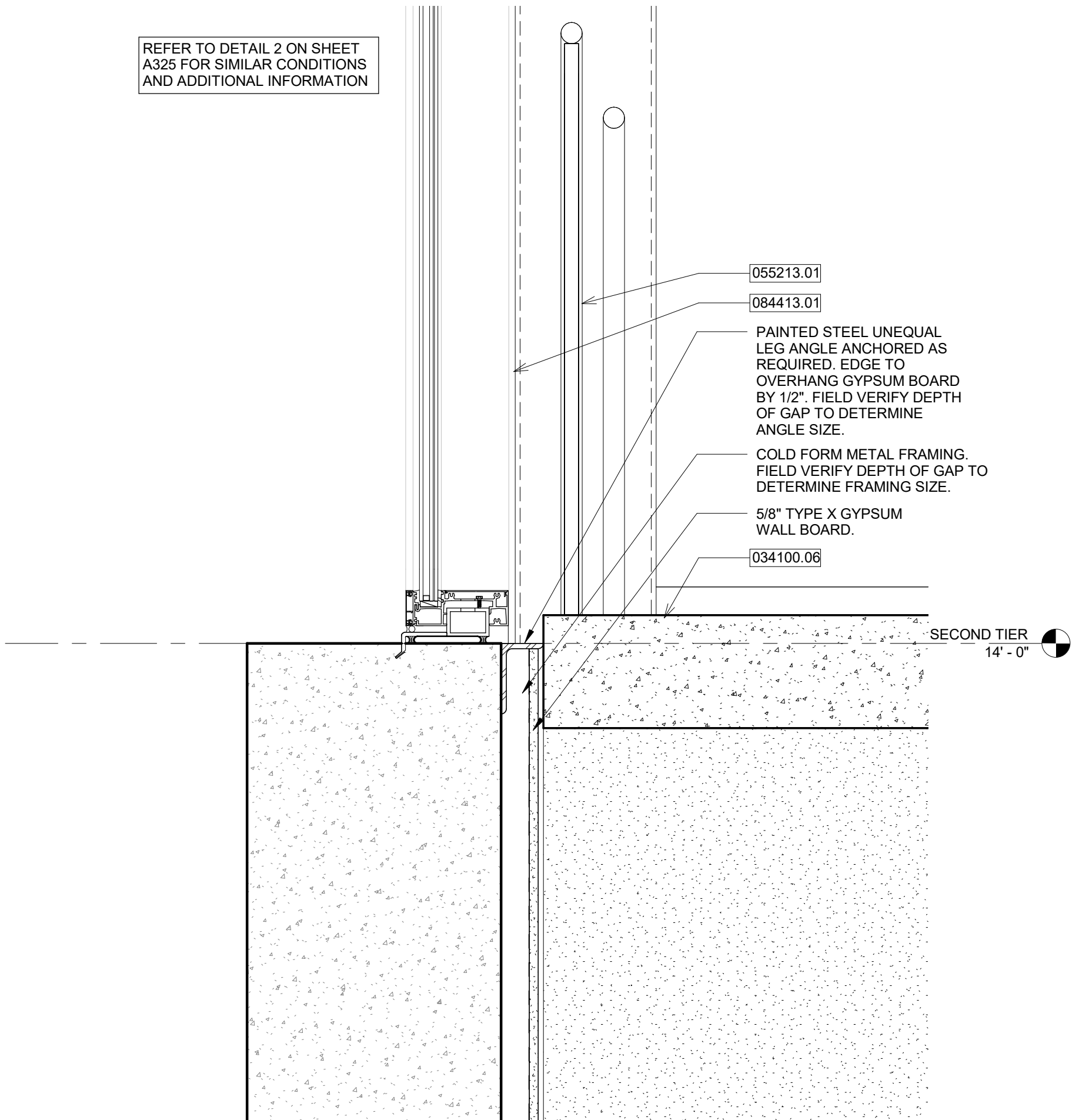
Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

REFER TO DETAIL 2 ON SHEET
A325 FOR SIMILAR CONDITIONS
AND ADDITIONAL INFORMATION



SECOND TIER
14' - 0"

1

DETAIL - RETAINING WALL @ STAIR B

1 1/2" = 1'-0"

REFER TO
ORIGINAL
DRAWING



GRESHAM
SMITH AND
PARTNERS

Asheville Regional Airport Parking Garage
Greater Asheville Regional Airport Authority
61 Terminal Drive, Suite 1
Fletcher, NC 28732

ASK-RFI 59
41152.00
07/13/17

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Structural Steel
Contractor: Welding Unlimited

Proposal #: #4
Project # 09-163
Date: 7/25/2017

Description of Change: Add 4x6x5/16 galvanized angle at stair B retaining wall. This includes plates with gussets in the area of the landing to extend the vertical leg of the angle down to where expansion anchors can be installed

Material:			Subtotals
	1. Total Cost of Materials (Attach Backup)	\$0.00	
	2. Sales Tax	\$0.00	
	3. Overhead & Profit 20%	\$0.00	\$0.00

Labor:	4. Total Man Hour Cost (Attach Backup)		
	5. Payroll Burden (Enter Percentage)		
	6. Overhead & Profit		\$0.00

Equipment:	7. Equipment / Equipment Rental (Attach Backup)		
	8. Overhead & Profit		\$0.00

Unit Prices:	9. Unit Price (If Applicable)	445	\$3.00	\$1,335.00
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Subcontractors:	10. Subcontractor (drawings)			
	11. Overhead & Profit 10%		\$0.00	\$0.00

			Subtotal of Proposal	\$1,335.00
	12. Payment & Performance Bond (Enter Bond Rate Percentage)		3.5%	\$46.73
			Total of Change Order Proposal	\$1,381.73

Time Extension Request: 1 Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: Vernon Allen Date: 7/26/2017

Change order Req. 4		
part		pounds
6x4 galvanized angle		332
Plates		96
anchors		17
	total	445

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Drywall / Framing
Contractor: Xtreme Drywall

Proposal #: XDW #1
Project # 09-163
Date: 8.10.17

Description of Change: Price to add the furring, insulation, and drywall to close the gap between the stairs and retaining wall at Stair B.

Material: **Subtotals**

1. Total Cost of Materials (Attach Backup)		\$851.02	
2. Sales Tax	7%	\$59.57	
3. Overhead & Profit	10%	\$91.06	\$1,001.65

Labor: \$30/Hour Including Burden (3 Men for 2 Days)

4. Total Man Hour Cost (Attach Backup)		\$1,111.00	
5. Payroll Burden (Enter Percentage)	35%	\$388.85	
6. Overhead & Profit	10%	\$149.99	\$1,649.84

Equipment:

7. Equipment / Equipment Rental (Attach Backup)			
8. Overhead & Profit	10%	\$0.00	\$0.00

Unit Prices:

9. Unit Price (If Applicable)			\$0.00
-------------------------------	--	--	--------

Subcontractors:

10. Subcontractor (Attach Subcontractor Backup)			
11. Overhead & Profit	10%	\$0.00	\$0.00

		Subtotal of Proposal	\$2,651.49
12. Payment & Performance Bond	1%		\$26.51
(Enter Bond Rate Percentage)			

Total of Change Order Proposal \$2,678.00

Time Extension Request: _____ Days

Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: _____ Date: _____

CK SUPPLY

120 TAVISTOCK ROAD, ARDEN, NC 28704
 ● 828-681-8812 ● 828-681-8817 (FAX)



CQ2971800347

Account No. 0082825	Customer Purchase Order No.	Pick Ticket No. 2971800347	From Order No.	B/O From Order	Order Date 8/10/17	Date Requested 9/13/17	Date Shipped 9/13/17	Invoice Number 971800347
------------------------	-----------------------------	-------------------------------	----------------	----------------	-----------------------	---------------------------	-------------------------	-----------------------------

ME DRYWALL SYSTEMS
 N LOUISIANA AVE STE T

SHIP TO:
 (Same as SOLD TO
 unless indicated)

00000
 ASHEVILLE AIRPORT
 PARKING DECK

PRICE QUOTATION PAGE 1
 08:49:14
 8/10/17

ASHEVILLE NC 28806-0000

PRICE QUOTE, DO NOT SHIP ASHEVILLE NC288060000

QUANTITY	QUANTITY		PRODUCT DESCRIPTION	EXTENDED QUANTITY	UNIT	PRICE PER UNIT	SALES AMOUNT
	ORDERED	SHIPPED					

20	0	PC	212 ST 16' 20G EQ	.320	MLF	365.0000	116.80
5	0	PC	212 TR 10' 20G EQ 114L	.050	MLF	330.0000	16.50
8	0	PC	5/8" ULT LT X D/W 4X12'	.384	MSF	295.0000	113.28
12	0	PC	2" FOAMULAR-150 4X8 SQ	.384	MSF	1,574.0625	604.44

DO NOT PAY - THIS IS NOT AN INVOICE
 DO NOT PAY - THIS IS NOT AN INVOICE

P/D=P

SUB-TOTAL	851.02
TAX N 11	59.57
TOTAL QUOTE	910.59

8 GB017

PAY LAST AMOUNT
 SHOWN ABOVE

NO SERVICE CHARGE OF 1 1/2% PER MONTH. THE SALE OF THE PRODUCTS IDENTIFIED ON THIS DOCUMENT ARE GOVERNED BY AND SUBJECT TO L&W SUPPLY CORPORATION'S TERMS & CONDITIONS IN EFFECT AS OF THE ORDER DATE SHOWN ABOVE. THESE TERMS & CONDITIONS ARE INCORPORATED HEREIN BY REFERENCE. ARE AVAILABLE ON OUR WEBSITE AT www.lwsupply.com/terms-of-use.html. IN ADDITION, YOU CAN OBTAIN A HARD COPY OF THE TERMS & CONDITIONS FROM YOUR L&W SUPPLY REPRESENTATIVE. BY SIGNING THIS DOCUMENT, YOU AGREE TO SUCH TERMS & CONDITIONS. THE TERMS & CONDITIONS, THE TERMS OF THE CREDIT AGREEMENT BETWEEN CUSTOMER AND L&W SUPPLY, IF APPLICABLE, AND THE TERMS ON THIS DOCUMENT REPRESENT OUR FINAL AND COMPLETE AGREEMENT AS TO THE SALE OF THE PRODUCTS IDENTIFIED ON THIS DOCUMENT. NO OTHER TERMS OR CONDITIONS ADDING TO OR MODIFYING THE SAME SHALL BE BINDING UPON US UNLESS MADE IN A WRITING SIGNED BY AN AUTHORIZED REPRESENTATIVE OF L&W SUPPLY. L&W SUPPLY CORPORATION IS NOT RESPONSIBLE FOR BROKEN SIDEWALKS, CURBS, LAWNS, ETC. WHEN DELIVERY IS REQUESTED ON PREMISES. Form 1

THE MATERIALS LISTED ON THIS TICKET WERE IN FACT DELIVERED TO THE JOB AND ADDRESS SHOWN ON THIS TICKET.

*DELIVERED/
 CHECKED BY

IND CHECKER

RECEIVED BY

Change Order Request # 23

BUILDING ON A GREAT TRADITION

13-Sep-17
Delta Airport Consultants
11111 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #23

Dear Mr. Moody

This Change Order Request is for the additional lost time associated with adverse weather encountered since our initial May, 2017 weather claim. Please review the appropriate backup documentation. Which includes this additional delay claim in addition to the original AT&T and weather delay claims.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
American South General Contractors	95	LS	1,639.46	155,748.70
				0.00
				0.00
				0.00
SUBTOTAL			1,639.46	155,748.70
OH&P				0.00
SUBTOTAL				155,748.70
BOND				0.00
TOTAL CHANGE ORDER PROPOSAL				155,748.70

We have included page 6 of ASI03-R1 of where this work shall take place along with the appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$155,748.70** American South General Contractors requesting an additional extension of **ninety five (95) days.**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager



October 02, 2017

Mr. Seth Chambers
Project Manager
American South General Contractors
60 North Merrimon Avenue, Unit 101
Asheville, North Carolina 28804

Subject: Change Order Request
Parking Garage
Asheville Regional Airport
Asheville, North Carolina

Dear Mr. Chambers:

The design team and Owner have reviewed your Change Order Request No. 23 requesting a ninety-five (95) calendar day increase in contract time for the above referenced project. As noted in the January 5, 2017 letter from the Greater Asheville Regional Airport Authority, an extension in the contract time would be considered upon nearing project completion when the actual impact to project completion could be determined. Based upon a project notice-to-proceed date of October 3, 2016 and a contract time of three hundred sixty-five (365) calendar days, substantial completion was expected to be October 3, 2017. At this time, American South General Contractors' schedule indicates substantial completion as November 17, 2017, resulting in a forty-five (45) calendar day impact to project completion. A forty-five (45) calendar day increase in contract time will be included in the forthcoming change order. A General Conditions Cost of \$1,639.46 per calendar day will also be included in the change order as this increase in contract time is attributable to the delay brought about by the AT&T utility relocation. No additional increase in contract time will be considered at this time. Should additional contract time be required to reach project substantial completion, the Owner will consider a no cost time extension change order at that time.

As discussed at the progress meeting on Thursday September 28, 2017 the Change Order Request No. 24 for accelerating the elevator installation will not be processed at this time. This request will only be considered upon successful completion of the elevator installation within the time period required for building occupancy prior to November 17, 2017.

A change order will be forwarded for execution under separate cover.

If you should have any questions, please do not hesitate to call.

Sincerely,

Kenneth W. Moody, P.E., C.M.
Project Manager

cc: Michael Reisman, A.A.E.

Reference: Delta Project No. 15086

11111 CARMEL COMMONS BOULEVARD, SUITE 435, CHARLOTTE, NC 28226

P. (704) 521-9101 F. (704) 521-9109 WWW.DELTAIRPORT.COM



BUILDING ON A GREAT TRADITION

Delta Airport Consultants
11121 Carmel Commons Blvd.
Suite 435
Charlotte, NC 28266
Attn: Mr. Kenneth Moody

9/13/17

RE: Asheville Regional Airport Parking Garage

Dear Mr. Moody,

American South General Contractors and our Subcontractors have experienced an unanticipated amount of delay directly associated with adverse weather over the last few months. The project has accrued an additional total of forty two (42) days of weather related days that affected the projects critical path since our initial weather claim that ran through May 6th, 2017. Based on the informational data obtained from the National Oceanic & Atmospheric Administration (NOAA), American South anticipated approximately twenty six (22) days in the original project schedule for such weather related delays between the dates of May 6th 2017 and August 11th 2017.

The total accumulation of these delays has totaled ten (20) calendar days when adjusted as actual needed versus anticipated weather days.

As discussed during the agreement conversation of Change Order Request Number Seven (7) as part of Contract Change Order Number Four (4) and stated in our November 16th, 2017 Delay Implications Letter; most of these days may not have come into account had the Project never encountered a forty nine (49) day AT&T delay. While the amount of inclement weather was highly unusual, this AT&T delay has caused the completion of the building "dry in" to slide by forty nine (49) days causing much of the weather sensitive work to be pushed into some of the historically wettest months possible. The additional weather days accumulated since May 6th has only compounded the scheduling delays. As you can see based off the provided data and original critical path schedule; without the AT&T delay most of the weather sensitive activities would have been completed prior to the months of February through August when a large portion of the adverse weather was encountered.

American South has taken all reasonably possible steps to prepare, minimize, and correct these delays. Despite all of American South's efforts, we have been unable to keep the project on track and now have no other option than to request an extension of time. We have provided the last five (5) year weather data for Fletcher, North Carolina along with our own weather data and daily logs to show anticipated versus actual weather days. As you can see from the information provided, we have drastically accrued many days beyond what could have been reasonably anticipated. There has been no additional claim or count associated with the time period prior to May 6th, 2017 so not to claim a delay upon an initial delay. We have also adjusted the partial months based off percentages used to show an accurate anticipation and reality. In addition to the weather data, we have provided our critical path reports for both scheduling periods to show impacted activities as scheduled prior to the May 6th delay claim and the adjusted schedule based off the May 6th acknowledgement of delay.



BUILDING ON A GREAT TRADITION

In accordance with Article 8.3 Delays and Extensions of Time, 8.3.2 Claims relating to time we understand that no additional claim is required after initial approval; however American South General Contractors feel obligated to keep the Owner and the Design team up to date on schedule and cost implications.

§ 8.3 DELAYS AND EXTENSIONS OF TIME

§ 8.3.1 If the Contractor is delayed at any time in the commencement or progress of the Work by an act or neglect of the Owner or Architect, or of an employee of either, or of a separate contractor employed by the Owner; or by changes ordered in the Work; or by labor disputes, fire, unusual delay in deliveries, unavoidable casualties or other causes beyond the Contractor's control; or by delay authorized by the Owner pending mediation and arbitration; or by other causes that the Architect determines may justify delay, then the Contract Time shall be extended by Change Order for such reasonable time as the Architect may determine.

§ 8.3.2 Claims relating to time shall be made in accordance with applicable provisions of Article 15.

§ 15.1.5 CLAIMS FOR ADDITIONAL TIME

§ 15.1.5.1 If the Contractor wishes to make a Claim for an increase in the Contract Time, written notice as provided herein shall be given. The Contractor's Claim shall include an estimate of cost and of probable effect of delay on progress of the Work. In the case of a continuing delay, only one Claim is necessary.

§ 15.1.5.2 If adverse weather conditions are the basis for a Claim for additional time, such Claim shall be documented by data substantiating that weather conditions were abnormal for the period of time, could not have been reasonably anticipated and had an adverse effect on the scheduled construction.

At this time American South requests a Change Order be written to increase the contract by ninety five (95) calendar days and one hundred fifty five thousand seven hundred and forty eight dollars and seventy cents (\$155,748.70). This puts the new final completion date at January 5th, 2017.

- AT&T Delay: 49 Days – Submitted on December 12th, 2016 and acknowledged by Asheville Regional Airport letter dated January 5th, 2017.
- Initial Weather Delay: 26 Days – Submitted on May 6th, 2017 and acknowledged by Asheville Regional Airport letter dated June 16th, 2017.
- Additional Weather Delay: 20 Days – Submitted on September 6th, 2017.

American South is, and will continue to diligently reschedule our subcontractors, work additional hours with additional manpower when practical, and work wherever the site allows in attempt and hope to mitigate further delays.

Sincerely,

Seth Chambers

American South General Contractors, Inc.
Seth Chambers
Project Manager

CC: Michael Reisman Leslie Jo Hurwitz Bill Morgan
Rita Yanz Kevin Hopkins
Michael Calhoun



ASHVEILLE REGIONAL AIRPORT PARKING GARAGE
Calendar Day General Conditions Cost

Project Management	\$361.01
• Seth Chambers	
Supervision	\$390.16
• Clay Wiser	
Temporary Facilities	\$258.83
• Office Trailer	
• Temporary Power	
• Temporary Water	
• Temporary Toilets	
• Telephones	
• Computers / Copiers	
• Office Supplies	
Daily Clean / Dumpsters Fees	\$204.99
• Clean Up Labor	
• Dumpster Rental	
• Dump Fees	
Temporary Protection	\$72.40
• Temporary Fencing	
• Fence Screening	
• Safety	
Jersey Barricade Rental	\$131.78
Sub Total	\$1,419.17
Payment & Performance Bond Extension	\$21.48
General Liability Insurance	\$15.02
Builders Risk Extension	\$20.00
10% Overhead & Profit	\$147.56
1% Bond	\$16.23
Total Daily General Conditions Cost	\$1,639.46

LOST TIME BETWEEN MAY 6th & AUGUST 11th, 2017

					If Due to Percipitation				
Date	Day of Week	Contractor/s	On Critical Path?	Item Number	Precipitation (In)	Total Precip. Past 2 Days	Total Precip. Past 3 Days	Reason for Rain Stoppage	
1	12-May-17	Friday	NHM Concrete	Yes	270.E	0.42	0.43	0.43	Pump out footings, no chance to continue excavation.
2	19-May-17	Friday	NHM Concrete	Yes	270.E	0.35	0.41	0.41	Pump out footings, no chance to continue excavation.
3	22-May-17	Monday	Tindall	Yes	340 & 370	0.23	1.93	1.93	Setting of precast in rain was halted.
4	23-May-17	Tuesday	Tindall, AES, Merrill, Steep Creek, Fountain, Welding	Yes	340 & 370	0.61	0.84	2.54	Setting of precast in rain was halted.
5	24-May-17	Wednesday	Tindall, AES, NHM, Steep Creek, TP Howard, Welding	Yes	340 & 370	0.52	1.13	1.36	Setting of precast in rain was halted.
6	29-May-17	Monday	Tindall	Yes	341 & 370 & 380.4	0.2	0.29	0.32	Setting and welding of precast was halted.
7	30-May-17	Tuesday	Tindall, Fountain	Yes	341 & 370 & 380.4	0.2	0.4	0.48	Setting and welding of precast was halted.
8	5-Jun-17	Monday	Tindall, Fountain, TP Howard	Yes	341, 370, 380.4. 440	0.33	0.52	0.59	Setting of precast and underground utilities halted.
9	12-Jun-17	Monday	Tindall, Fountain, TP Howard	Yes	341, 370, 380.4. 440	0.17	0.17	0.17	Setting of precast and underground utilities halted.
10	13-Jun-17	Tuesday	Tindall, Fountain, TP Howard	Yes	341, 370, 380.4. 440	0.32	0.49	0.49	Setting of precast and underground utilities halted.
11	14-Jun-17	Wednesday	Tindall, Fountain, TP Howard, NHM	Yes	341, 370, 380.4. 440	0.5	0.82	0.99	Setting of precast and underground utilities halted.
12	15-Jun-17	Thursday	Fountain, TP Howard, American South	Yes	440, 460	0.46	0.96	1.28	Interior & Exterior Underground Utilities and Slab Preperation.
13	16-Jun-17	Friday	Fountain, TP Howard, American South	Yes	440, 460, 480	1.06	1.52	2.02	Interior & Exterior Underground Utilities and Slab Preperation.
14	17-Jun-17	Saturday	Fountain, TP Howard, American South, NHM	Yes	440, 460, 480	0.39	1.45	1.91	Interior & Exterior Underground Utilities and Slab Preperation.
15	19-Jun-17	Monday	Fountain, TP Howard, American South	Yes	440, 460, 480	0.63	0.81	1.19	Interior & Exterior Underground Utilities and Slab Preperation.
16	20-Jun-17	Tuesday	Fountain, TP Howard, American South	Yes	440, 460, 480	0.23	0.86	1.04	Interior & Exterior Underground Utilities and Slab Preperation.
17	21-Jun-17	Wednesday	Fountain, TP Howard, American South	Yes	440, 460, 480. 600	0.5	0.74	1.36	Interior & Exterior Underground Utilities and Slab Preperation.
18	22-Jun-17	Thursday	Fountain, TP Howard, American South	Yes	440, 460, 480. 600	0.57	1.08	1.31	Interior & Exterior Underground Utilities and Slab Preperation.
19	23-Jun-17	Friday	Fountain, TP Howard, American South	Yes	440, 460, 480. 600, 610	0.33	0.9	1.41	Interior & Exterior Underground Utilities and Slab Preperation.
20	30-Jun-17	Friday	Fountain, American South	Yes	440, 460, 480. 600, 610	1.37	1.42	1.42	Interior & Exterior Underground Utilities and Slab Preperation.
21	1-Jul-17	Saturday	Fountain, American South	Yes	440, 460, 480. 600, 610	0.58	1.96	2	Interior & Exterior Underground Utilities and Slab Preperation.
22	3-Jul-17	Monday	Fountain, American South	Yes	440, 460, 480. 600, 610	0.9	1.33	1.92	Interior & Exterior Underground Utilities and Slab Preperation.
23	4-Jul-17	Tuesday	Fountain, American South	Yes	440, 460, 480. 600, 610	0.71	1.61	2.04	Interior & Exterior Underground Utilities and Slab Preperation.
24	5-Jul-17	Wednesday	Fountain, American South	Yes	440, 460, 480. 600, 610	0.27	0.98	1.88	Interior & Exterior Underground Utilities and Slab Preperation.
25	6-Jul-17	Thursday	Fountain, American South	Yes	440, 460, 480. 600, 610	0.63	0.9	1.61	Interior & Exterior Underground Utilities and Slab Preperation.
26	7-Jul-17	Friday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1842	0.46	1.09	1.36	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
27	8-Jul-17	Saturday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1842	0.36	0.82	1.45	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
28	10-Jul-17	Monday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1842	0.22	0.23	0.59	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
29	11-Jul-17	Tuesday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1842	0.14	0.35	0.37	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
30	12-Jul-17	Wednesday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1842	0.18	0.32	0.53	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
31	13-Jul-17	Thursday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1852	0.53	0.71	0.85	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
32	14-Jul-17	Friday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1852	1.09	1.62	1.8	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
33	17-Jul-17	Monday	Fountain, NHM Concrete, Merrill, Weld. Unl. ASGC	Yes	ER70, ER16, 610, 1852	0.41	0.41	1.26	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
34	18-Jul-17	Tuesday	Fountain, NHM Concrete, Merrill, ASGC	Yes	ER70, ER16, 610	0.44	0.85	0.85	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)
35	21-Jul-17	Friday	Fountain, NHM Concrete, Merrill, ASGC	Yes	ER70, ER16, 610	0.17	0.18	0.21	Electrical Room Footings cannot be poured & interior & exterior undground not complete (Compounded from above)

LOST TIME BETWEEN MAY 6th & AUGUST 11th, 2017 CONTINUED

					If Due to Percipitation				
Date	Day of Week	Contractor/s	On Critical Path?	Item Number	Precipitation (In)	Total Precip. Past 2 Days	Total Precip. Past 3 Days	Reason for Rain Stoppage	
36	27-Jul-17	Thursday	Fountain, NHM Concrete, Merrill, ASGC	Yes	ER70, ER16, 610	0.79	0.9	1.01	Electrical Room Footings cannot be poured & interior & exterior underground not complete (Compounded from above)
37	28-Jul-17	Friday	Fountain, Merrill, ASGC, Xtreme Drywall	Yes	ER70, ER16, 1862, 620	1.28	2.07	2.18	Electrical Room Footings cannot be poured & interior & exterior underground not complete (Compounded from above). Framing at canopy & staitowers delayed.
38	4-Aug-17	Friday	Fountain, Merrill, ASGC, Xtreme Drywall	Yes	ER70, ER16, 1862, 620	0.5	0.5	0.54	Electrical Room Footings cannot be poured & interior & exterior underground not complete (Compounded from above). Framing at canopy & staitowers delayed.
39	7-Aug-17	Monday	Fountain, Merrill, ASGC, Xtreme Drywall	Yes	ER70, ER16, 1862, 620	0.84	0.86	0.87	Electrical Room Footings cannot be poured & interior & exterior underground not complete (Compounded from above). Framing at canopy & staitowers delayed.
40	8-Aug-17	Tuesday	Fountain, Merrill, ASGC, Xtreme Drywall	Yes	ER70, ER16, 1862, 620	0.43	1.27	1.3	Electrical Room Footings cannot be poured & interior & exterior underground not complete (Compounded from above). Framing at canopy & staitowers delayed.
41	10-Aug-17	Thursday	Fountain, ASGC	Yes	ER70. ER16, 620	0.24	0.25	0.68	Slab Prep & Remediation halted due to entrance road & perm. Power exterior underground.
42	11-Aug-17	Friday	Fountain, ASGC		ER70. ER16, 620	1.1	1.34	1.35	Slab Prep & Remediation halted due to entrance road & perm. Power exterior underground.

WEEKEND RAIN DAYS FOR REFERENCE ONLY

					If Due to Percipitation			
Date	Day of Week	Contractor/s	On Critical Path?	Item Number	Precipitation (In)	Total Precip. Past 2 Days	Total Precip. Past 3 Days	Reason for Rain Stoppage
5.21.17	Sunday	None	Not Counted	Not Counted	1.7	1.7	2.05	
6.4.17	Sunday	None	Not Counted	Not Counted	0.19	0.26	0.26	
6.18.17	Sunday	None	Not Counted	Not Counted	0.18	0.56	1.63	
6.24.17	Saturday	None	Not Counted	Not Counted	0.4	0.73	1.31	
7.2.17	Sunday	None	Not Counted	Not Counted	0.43	1.02	2.39	
7.15.17	Saturday	None	Not Counted	Not Counted	0.85	1.94	2.48	
7.23.17	Sunday	None	Not Counted	Not Counted	0.22	0.22	0.39	
7.29.17	Saturday	None	Not Counted	Not Counted	0.17	1.46	2.24	

Total Rain on Lost Work Days in Inches = 21.66

Lost Days + Weekends in Inches = 25.8

Anticipated Per 5 Year Average From the US Department of Commerce - National Oceanic & Atmospheric Administration

Month	2011			2012			2013			2014			2015			Avg (Nearest Whole)		
	>=.10	>=.5	>=1.0	>=.10	>=.5	>=1.0	>=.10	>=.5	>=1.0	>=.10	>=.5	>=1.0	>=.10	>=.5	>=1.0	>=.10	>=.5	>=1.0
January	5	1	0	7	2	1	10	4	4	4	2	0	6	3	1	6	2	1
February	4	2	1	3	0	0	5	1	0	6	4	0	6	3	0	5	2	0
March	9	5	1	7	3	0	7	3	1	7	2	0	8	0	0	8	3	0
April	10	3	2	5	3	2	7	5	2	9	6	1	10	4	2	8	4	2
May	8	2	0	12	2	1	9	6	3	7	2	1	4	1	0	8	3	1
June	8	3	2	5	3	0	0	0	0	0	0	0	11	3	1	5	2	1
July	5	1	1	11	4	1	0	0	0	10	4	0	6	2	0	6	2	0
August	7	4	2	10	4	1	12	5	2	7	2	1	8	2	0	9	3	1
September	6	3	1	8	3	2	5	2	2	8	4	2	9	3	1	7	3	2
October	6	2	0	6	2	1	4	1	1	7	1	1	10	5	3	7	2	1
November	8	3	1	4	0	0	6	2	1	6	3	1	8	6	4	6	3	1
December	8	4	2	6	3	1	8	7	1	4	1	1	12	6	4	8	4	2
Totals																83	33	13

Weather Days Between May 6th and August 11th 2017		
Month	Anticipated	Actual
May 6th through May 31st (26/31=84%)	7	7
June (100%)	5	13
July	6	17
August 1st through August 11th (11/31=36%)	4	5
Totals	22	42

Additional Days Above Anticipated	20
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*No days prior to the original May 6th Claim are calculated.

Global Summary of the Month for 2011

Generated on 05/03/2017

Date		Temperature (F)										Precipitation (Inches)										
Elem ->	TAVG	TMAX	TMIN	HTDD	CLDD	EMXT		EMNT		DX90	DX32	DT32	DT00	PRCP	EMXP		SNOW	EMSD		DP01	DP05	DP10
Month	Mean	Mean Max.	Mean Min	Heating Degree Days	Cooling Degree Days	Highest	High Date	Lowest	Low Date	Number of Days				Total	Greatest Observed		Snow, Sleet			Number of Days		
										Max >= 90	Max <= 32	Min <= 32	Min <= 0		Amount	Date	Total Fall	Max Depth	Max Date	>= .10	>= .50	>= 1.0
Jan														2.15	0.60	26	9.1			5	1	0
Feb														2.37	1.32	02				4	2	1
Mar														4.76	1.47	10				9	5	1
Apr														4.76	1.61	16				10	3	2
May														3.01	0.73	04				8	2	0
Jun														4.50	1.53	16				8	3	2
Jul														3.28	1.94	09				5	1	1
Aug														4.07	1.01	14				7	4	2
Sep														4.31	2.02	06				6	3	1
Oct														2.35	0.78	12				6	2	0
Nov														5.60	2.47	29				8	3	1
Dec														5.02	1.63	23				8	4	2

Notes

(Blank) Data element not reported or missing.

+ Occurred on one or more previous dates during the month. The date in the Date field is the last day of occurrence.

A Accumulated amount.

X Monthly means or totals based on incomplete time series.

T Trace Amount.

Global Summary of the Month for 2012

Generated on 05/03/2017

Date		Temperature (F)										Precipitation (Inches)										
Elem ->	TAVG	TMAX	TMIN	HTDD	CLDD	EMXT		EMNT		DX90	DX32	DT32	DT00	PRCP	EMXP		SNOW	EMSD		DP01	DP05	DP10
Month	Mean	Mean Max.	Mean Min	Heating Degree Days	Cooling Degree Days	Highest	High Date	Lowest	Low Date	Number of Days				Total	Greatest Observed		Snow, Sleet			Number of Days		
										Max >= 90	Max <= 32	Min <= 32	Min <= 0		Amount	Date	Total Fall	Max Depth	Max Date	>=.10	>=.50	>=1.0
Jan														3.16	1.00	12				7	2	1
Feb														1.03	0.36	05				3	0	0
Mar														2.92	0.75	03				7	3	0
Apr														4.93	1.79	06				5	3	2
May														4.89	1.20	14				12	2	1
Jun														2.26	0.76	01				5	3	0
Jul														5.70	1.90	14				11	4	1
Aug														5.49	1.42	01				10	4	1
Sep														6.30	2.53	18				8	3	2
Oct														4.10	2.46	02				6	2	1
Nov														0.88	0.38	13				4	0	0
Dec														4.15	1.61	26				6	3	1

Notes

(Blank) Data element not reported or missing.

+ Occurred on one or more previous dates during the month. The date in the Date field is the last day of occurrence.

A Accumulated amount.

X Monthly means or totals based on incomplete time series.

T Trace Amount.

U.S. Department of Commerce
 National Oceanic & Atmospheric Administration
 National Environmental Satellite, Data, and Information Service
 Elev: 2116 ft. Lat: 35.4308° N Lon: -82.5242° W
 Station: FLETCHER 1.2 WSW, NC US US1NCHN0002

Global Summary of the Month for 2013

Generated on 05/03/2017

National Centers for Environmental Information
 151 Patton Avenue
 Asheville, North Carolina 28801

Date		Temperature (F)											Precipitation (Inches)									
Elem ->	TAVG	TMAX	TMIN	HTDD	CLDD	EMXT		EMNT		DX90	DX32	DT32	DT00	PRCP	EMXP		SNOW	EMSD		DP01	DP05	DP10
Month	Mean	Mean Max.	Mean Min	Heating Degree Days	Cooling Degree Days	Highest	High Date	Lowest	Low Date	Number of Days				Total	Greatest Observed		Snow, Sleet			Number of Days		
										Max >= 90	Max <= 32	Min <= 32	Min <= 0		Amount	Date	Total Fall	Max Depth	Max Date	>= .10	>= .50	>= 1.0
Jan														8.70	2.95	31				10	4	4
Feb														2.22	0.63	22				5	1	0
Mar														3.46	1.00	12				7	3	1
Apr														6.51	2.00	29				7	5	2
May														8.10	2.29	06				9	6	3
Aug														7.00	1.18	08				12	5	2
Sep														3.61	1.55	22				5	2	2
Oct														2.26	1.40	07				4	1	1
Nov														3.82	2.10	27				6	2	1
Dec														7.67	2.61	23				8	7	1

Notes

(Blank) Data element not reported or missing.

+ Occurred on one or more previous dates during the month. The date in the Date field is the last day of occurrence.

A Accumulated amount.

X Monthly means or totals based on incomplete time series.

T Trace Amount.

Global Summary of the Month for 2014

Generated on 05/03/2017

Date		Temperature (F)										Precipitation (Inches)										
Elem ->	TAVG	TMAX	TMIN	HTDD	CLDD	EMXT		EMNT		DX90	DX32	DT32	DT00	PRCP	EMXP		SNOW	EMSD		DP01	DP05	DP10
Month	Mean	Mean Max.	Mean Min	Heating Degree Days	Cooling Degree Days	Highest	High Date	Lowest	Low Date	Number of Days				Total	Greatest Observed		Snow, Sleet			Number of Days		
										Max >= 90	Max <= 32	Min <= 32	Min <= 0		Amount	Date	Total Fall	Max Depth	Max Date	>=.10	>=.50	>=1.0
Jan														2.39	0.80	12				4	2	0
Feb														3.23	0.87	13				6	4	0
Mar														2.43	0.56	07				7	2	0
Apr														5.64	1.37	07				9	6	1
May														4.11	1.47	15				7	2	1
Jul														5.06	0.88	10				10	4	0
Aug														3.54	1.56	10				7	2	1
Sep														7.24	3.07	08				8	4	2
Oct														4.46	2.48	15				7	1	1
Nov														3.94	1.97	24				6	3	1
Dec														2.61	1.63	24				4	1	1

Notes

(Blank) Data element not reported or missing.

+ Occurred on one or more previous dates during the month. The date in the Date field is the last day of occurrence.

A Accumulated amount.

X Monthly means or totals based on incomplete time series.

T Trace Amount.

Global Summary of the Month for 2015

Generated on 05/03/2017

Date		Temperature (F)										Precipitation (Inches)											
Elem ->	TAVG	TMAX	TMIN	HTDD	CLDD	EMXT		EMNT			DX90	DX32	DT32	DT00	PRCP	EMXP		SNOW	EMSD		DP01	DP05	DP10
Month	Mean	Mean Max.	Mean Min	Heating Degree Days	Cooling Degree Days	Highest	High Date	Lowest	Low Date	Number of Days				Total	Greatest Observed		Snow, Sleet			Number of Days			
										Max >= 90	Max <= 32	Min <= 32	Min <= 0		Amount	Date	Total Fall	Max Depth	Max Date	>= .10	>= .50	>= 1.0	
Jan															3.26	1.10	04				6	3	1
Feb															2.87	0.92	17	9.9			6	3	0
Mar															1.90	0.31	14				8	0	0
Apr															5.59	1.28	16				10	4	2
May															1.51	0.62	26				4	1	0
Jun															5.51	1.42	23				11	3	1
Jul															2.67	0.90	15				6	2	0
Aug															3.02	0.79	11				8	2	0
Sep															4.54	1.52	26				9	3	1
Oct															7.67	2.26	04				10	5	3
Nov															7.62	2.10	10				8	6	4
Dec															9.44	1.95	25				12	6	4

Notes

{Blank} Data element not reported or missing.

+ Occurred on one or more previous dates during the month. The date in the Date field is the last day of occurrence.

A Accumulated amount.

X Monthly means or totals based on incomplete time series.

T Trace Amount.

AVL Update 08		Critical Path Report							Page 1 of 2 AV08											
Activity ID	Activity Name	Total Float	C.Dur.	R. Dur.	% Comp	Start	Finish	2017												
								May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar		
MATERIAL PROCUREMENT		-56	128	64		02-27-17 A	09-07-17	09-07-17												
F14a	FAB & DELIVER - Elevators	-56	90	43	0%	02-27-17 A	08-08-17	08-08-17												
C120	Field Measure & Fabricate Metal Panels - Canopy	-56	25	25	0%	08-03-17	09-07-17	08-03-17 09-07-17												
FOUNDATIONS		-56	3	5		05-18-17 A	06-14-17	06-14-17												
ER01	E/R/P Footings - Main Electrical Room	-56	3	5	33%	05-18-17 A	06-14-17	06-14-17												
STRUCTURE		-56	7	7		06-15-17	06-23-17	6-15-17 06-23-17												
ER16	Lay CMU Walls (Footing to L2 Precast) - Main Electrical Room	-56	7	7	0%	06-15-17	06-23-17	6-15-17 06-23-17												
1st FLOOR		-56	32	32		06-30-17	08-15-17	06-30-17 08-15-17												
ER04	Layout Panels & Equipment - Main Electrical Room	-56	2	2	0%	06-30-17	07-03-17	6-30-17 07-03-17												
ER70	Mount Panel Cans - Main Electrical Room	-56	5	5	0%	07-05-17	07-11-17	07-05-17 07-11-17												
ER28	Complete Conduit @ Panels & Equipment - Main Electrical Rm	-56	10	10	0%	07-07-17	07-20-17	07-07-17 07-20-17												
GER06	Complete Feeder Conduits @ Generator	-56	1	1	0%	07-21-17	07-21-17	07-21-17 07-21-17												
ER30	Pull Panel Feeders - Main Electrical Rm	-56	3	3	0%	07-21-17	07-25-17	07-21-17 07-25-17												
514	Set Electrical Panels	-56	3	3	0%	07-26-17	07-28-17	07-26-17 07-28-17												
GER10	Pull Emergency Electrical Feeders	-56	5	5	0%	07-24-17	07-28-17	07-24-17 07-28-17												
ER48	Test & Terminate Feeders - Main Electrical Rm	-56	5	5	0%	07-31-17	08-04-17	07-31-17 08-04-17												
ER50	ELECTRICAL SERVICE TO ELEVATOR MACHINE ROOM	-56	5	5	0%	08-09-17	08-15-17	08-09-17 08-15-17												
PPOWER	PERMANENT POWER to BUILDING FROM ROOM	-56	0	0	0%			◆ PERMANENT POWER to BUILDING FROM ROOM												
ELEVATOR & STAIRWELL AREAS		-56	59	59		08-09-17	10-31-17	08-09-17 10-31-17												
EL102	Install Elevator Machine Eqpt. - Elev. 1	-56	5	5	0%	08-09-17	08-15-17	08-09-17 08-15-17												
SACD	Complete Dry-In - Stair A	-56	0	0	0%			◆ Complete Dry-In - Stair A												
520A	Install Remaining CW Glazing @ Canopy Level - Stair A	-56	1	1	0%	09-22-17	09-22-17	09-22-17 09-22-17												
EL103	Install Rails & Platform - Elev. 1	-56	30	30	0%	08-16-17	09-27-17	08-16-17 09-27-17												
567	Hang & Finish Drywall - Stair A	-56	7	7	0%	09-25-17	10-03-17	09-25-17 10-03-17												
EL104	Set Elevator Hoistway Entrances - Elev. 1	-56	5	5	0%	09-28-17	10-04-17	09-28-17 10-04-17												
STA82	Install Wall Mounted Handrails - Stair A	-56	2	2	0%	10-04-17	10-05-17	10-04-17 10-05-17												
EL110	CMU In-Fill & Grout @ ELEV. ENTRANCES - Elev. 1	-56	4	4	0%	10-05-17	10-10-17	10-05-17 10-10-17												
STA70	Paint Stairwell - Stair A	-56	4	4	0%	10-06-17	10-11-17	10-06-17 10-11-17												
575	Install Electrical Fixtures - Stair A	-56	4	4	0%	10-12-17	10-17-17	10-12-17 10-17-17												
EL106	Install & Trim Elevator Cab - Elev. 1	-56	10	10	0%	10-11-17	10-24-17	10-11-17 10-24-17												
EL108	Align & Test Elevator Function - Elev. 1	-56	5	5	0%	10-25-17	10-31-17	10-25-17 10-31-17												
CANOPY / CONNECTOR		-56	74	74		06-05-17 A	09-21-17	6-5-17 A 09-21-17												
CNT	Provide Notice for Entrance Closure - Terminal Side	-79	7	6	0%	06-05-17 A	06-13-17	7 A 06-13-17												
C100	Demo Sidewalk for Footing Access - Terminal Side	-56	4	4	0%	06-14-17	06-19-17	4-17 06-19-17												
1812	Excavate Canopy Footings	-56	2	2	0%	06-20-17	06-21-17	20-17 06-21-17												
1822	Tie Rebar & Dow for Canopy Footings - Terminal Side	-56	2	2	0%	06-22-17	06-23-17	22-17 06-23-17												
1832	Pour Canopy Footings - Terminal Side	-56	1	1	0%	06-26-17	06-26-17	6-26-17 06-26-17												
1847	Demo Portion Of Existing Roof & Canopy Connection	-56	2	2	0%	06-27-17	06-28-17	6-27-17 06-28-17												
C102	Install & Inspect Steel Stiffeners	-56	2	2	0%	06-29-17	06-30-17	6-29-17 06-30-17												

■ Actual Work ■ Critical Remaining Work
■ Remaining Work ◆ Milestone

Critical Path Report
AVL Update 08

TASK filter: Longest Path.
Data Date: 06-08-17 Run Date: 06-08-17

AVL Update 08		Critical Path Report						Page 2 of 2 AV08														
Activity ID	Activity Name	Total Float	C.Dur.	R. Dur.	% Comp	Start	Finish	2017														
								May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar				
1842	Set Canopy Structural Steel	-56	8	8	0%	07-03-17	07-13-17	07-03-17	■	07-13-17												
1852	Set Canopy Metal Roof Decking	-56	2	2	0%	07-14-17	07-17-17	07-14-17	■	07-17-17												
1862	Frame & Sheath Canopy Parapet Walls	-56	12	12	0%	07-18-17	08-02-17	07-18-17	■	08-02-17												
1892	Install Metal Panels On Canopy (Walls & Soffit)	-56	10	10	0%	09-08-17	09-21-17			09-08-17	■	09-21-17										
SYSTEM TESTS, STARTUP & INSPECTIONS		-56	45	45		10-18-17	12-21-17					10-18-17	▶	12-21-17								
M80	Startup & Test Mechanical Equipment	-56	3	3	0%	10-18-17	10-20-17					10-18-17	■	10-20-17								
M87	Test & Balance HVAC Systems	-56	5	5	0%	10-23-17	10-27-17					10-23-17	■	10-27-17								
M90	Final Elevator Inspection	-56	5	5	0%	11-01-17	11-07-17					11-01-17	■	11-07-17								
M86	Final Mechanical Inspection (Mechanical, Plumbing, FP)	-56	10	10	0%	10-30-17	11-10-17					10-30-17	■	11-10-17								
M92	Obtain Certificate of Occupancy	-56	10	10	0%	11-01-17	11-14-17					11-01-17	■	11-14-17								
M01p	Final Cleaning	-56	10	10	0%	12-08-17	12-21-17						12-08-17	■	12-21-17							
TAXI LOT / GROUND TRANSPORTATION LOT		-56	2	2		12-20-17	12-21-17								12-20-17	▼	12-21-17					
830	Paint Pavement Markings @ Taxi Lot/ Ground Transportation Lot	-56	2	2	0%	12-20-17	12-21-17								12-20-17	■	12-21-17					
SOUTH PARKING LOT		-56	20	20		11-22-17	12-21-17								11-22-17	▶	12-21-17					
SL764	Rough-In Site Electrical - Southern Remote Lot	-56	5	5	0%	11-22-17	11-30-17								11-22-17	■	11-30-17					
SL810	Install Underground Utilities for PARCS - Southern Remote Lot	-56	5	5	0%	12-01-17	12-07-17								12-01-17	■	12-07-17					
SL800	Install Curb & Gutter - Southern Remote Lot	-56	3	3	0%	12-08-17	12-12-17								12-08-17	■	12-12-17					
SL820	Install Swing Gates - Southern Remote Lot	-56	3	3	0%	12-13-17	12-15-17								12-13-17	■	12-15-17					
SL822	Install Fence - Southern Remote Lot	-56	2	2	0%	12-18-17	12-19-17								12-18-17	■	12-19-17					
SL830	Paint Swing Gates - Southern Remote Lot	-56	2	2	0%	12-20-17	12-21-17								12-20-17	■	12-21-17					
PROJECT CLOSEOUT		-56	36	36		11-01-17	12-22-17								11-01-17	▶	12-22-17					
M89	American South Punch List & Workoff	-56	10	10	0%	11-01-17	11-14-17								11-01-17	■	11-14-17					
M94	Substantial Completion - 09/03/17	-56	0	0	0%		11-21-17*															◆ Substantial Completion
M93	Pre-Final Inspections / Generate Punch List	-56	5	5	0%	11-15-17	11-21-17								11-15-17	■	11-21-17					
M96	Pre-Final Punch List Work-off	-56	20	20	0%	11-22-17	12-21-17								11-22-17	■	12-21-17					
M98	Final Inspection	-56	1	1	0%	12-22-17	12-22-17															
M100	Project Completion - 10/03/17	-56	0	0	0%		12-22-17*															◆ Project Completion

Change Order Request # 25

BUILDING ON A GREAT TRADITION

20-Sep-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #25

Dear Mr. Moody

This Change Order Request is for the additional work for the added doors, frames, hardware, and electrical fixtures to be added to the crawlspace as called for in ASI-07.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Fountain Electric Services (All Associated Electrical Work)	1	LS	7,046.67	7,046.67
Bustle's Hardware (Door/Frame/HW Material Only)	1	LS	2,651.46	2,651.46
American South General Contractors (Door/Frame/HW Install Only)	1	LS	270.00	270.00
	1	LS		0.00
<hr/>				
SUBTOTAL			9,968.13	9,968.13
OH&P			10.00%	996.81
<hr/>				
SUBTOTAL				10,964.94
BOND			1.0%	109.65
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				11,074.59

We have included page 6 of ASI03-R1 of where this work shall take place along with the appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$11,074.59**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Electrical
Contractor: Fountain Services

Proposal #: FE COR11
Project # 09-163
Date: _____

Description of Change: Add (8) Type F6 Fixtures in Crawl Space

Material:						Subtotals
1. Total Cost of Materials (Attach Backup)			\$3,242.79			
2. Sales Tax	7%		\$227.00			
3. Overhead & Profit	10%		\$346.98			<u>\$3,816.76</u>
Labor:						
4. Total Man Hour Cost (Attach Backup)			\$2,872.85			
5. Payroll Burden (Enter Percentage)	0%		\$0.00			
6. Overhead & Profit	10%		\$287.29			<u>\$3,160.14</u>
Equipment:						
7. Equipment / Equipment Rental (Attach Backup)						
8. Overhead & Profit	10%		\$0.00			<u>\$0.00</u>
Unit Prices:						
9. Unit Price (If Applicable)						<u>\$0.00</u>
Subcontractors:						
10. Subcontractor (Attach Subcontractor Backup)						
11. Overhead & Profit	10%		\$0.00			<u>\$0.00</u>
			Subtotal of Proposal			\$6,976.90
12. Payment & Performance Bond (Enter Bond Rate Percentage)			1%			<u>\$69.77</u>
			Total of Change Order Proposal			<u>\$7,046.67</u>

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____
 Subcontractor: _____ Date: _____

desc	rate	hours	total
mechanic	47	50.55	2375.85
foreman	58	5	290
pm	69	3	207
			2872.85

Job ID: 160706 ASHEVILLE REGIONAL PARKING GARAGE
Project: 160706 ASHEVILLE REGIONAL PARKING GARAGE



CO: CO-0008: 170905 CRAWL SPACE LIGHTING

Summary by Item Number

5 Sep 2017 15:29:55

Item #	Size	Description	Q/M	Quantity	U/M	Mat Unit	Mat Result	Labor Unit	Lab Result
10001	3/4	GRC	M	224	FT	1.4429	323.21	0.0924	20.70
10548	3/4	CONDUIT CUT/THRD/REAM	M	19	EA	0.0000	0.00	0.1430	2.72
30001	3/4	GRC/IMC COUPLING	M	19	EA	0.8060	15.31	0.0264	0.50
30052	3/4	GRC THREADLESS CONNECTOR	M	32	EA	9.1725	293.52	0.3300	10.56
70034	10	THHN/THWN CU (STR)	M	652	FT	0.1713	111.66	0.0043	2.81
70146	10.	GREEN THHN CU (GRD 60A)	M	247	FT	0.1713	42.30	0.0043	1.07
100138	#18 to 8	WIRE-NUT MED - RED	M	24	EA	0.0619	1.49	0.0462	1.11
100152	#12 SOL	8" PIGTAIL W/GRD SCREW	M	16	EA	0.3226	5.16	0.0462	0.74
120146	48"	SURFACE W/LENS 1-LAMP-FLUOR	M	8	EA	0.0000	0.00	0.3080	2.46
150006	1-1/2"D	4"SQ CMB- KO NO BRKT	M	8	EA	0.4232	3.39	0.1320	1.06
150097		4"SQ BLANK CVR	M	8	EA	0.3355	2.68	0.0330	0.26
160367	1/4 x 4"	TOGGLE BOLT	M	16	EA	0.2213	3.54	0.0550	0.88
160390	#10	PLTD FLAT WASHER	M	16	EA	0.0154	0.25	0.0011	0.02
160760	10-24 x 1"	PLTD MACHINE SCREWS	M	16	EA	0.0350	0.56	0.0616	0.99
161158	#10-12 x 1"	PLASTIC ANCHOR	M	16	EA	4.4900	71.84	0.0220	0.35
161172	3/8 x 1 1/2 - 3"	HAMMER DRILLED HOLE	M	16	EA	0.0000	0.00	0.2200	3.52
161438	3/4	CONDUIT 2-HOLE STL STRAP	M	23	EA	0.4552	10.47	0.0352	0.81
Phase/Group totals:						885.37		50.55	
Job totals:						885.37		50.55	



Local (304) 523-7491 Toll Free (800) 624-3417 Fax (304) 528-0243

Quote

CUST.#: 522108

BILL TO: FOUNTAIN SERVICES LLC
244 CALLAHAN KOON ROAD
SPINDALE, NC 28160

UPC V	DATE	NUMBER
0000	09/05/17	12717118-00
P.O. NO.		PAGE #
AIRPORT		1
Freight PPD Allow		

SHIP TO: ASHEVILLE AIRPORT PARKING
c/o FOUNTAIN SERVICES LLC
61 TERMINAL DRIVE
FLETCHER, NC 28732

INSTRUCTIONS		SALES REP	PLACED BY
		3217	J. FERGUSO
SHIP POINT	SHIP VIA	TERMS	
Asheville 828-681-9813 5	Best Way	Net 30 Days	

LINE NO.	PRODUCT AND DESCRIPTION	QUANTITY		QUANTITY U/M	UNIT PRICE	AMOUNT (NET)
1	LITTZL1NL9614000LMFST MVOLT40K80CRIWH TYPE F6 ** DIRECT ORDER **	8		DROP each	275.40	2203.20
1 Lines Total						
						Total
						Taxes
						Invoice Total
						2203.20
						154.22
						2357.42

From: [Rick Bustle](#)
To: [Seth Chambers](#)
Subject: Re: AVL Airport Parking Garage ASI-07
Date: Monday, July 3, 2017 8:00:46 AM
Attachments: [image001.png](#)

Seth,
"QUOTATION"
Consists Of Adding 1 Pair Hollow Metal Doors, Frame, And Hardware - Same As Door Tag 106A.

Add the sum of \$ 2,478.00 + tax.
Note: Hollow Metal Doors & Frame Placed On 15 Working Day Fab.

Thanks,

Rick Bustle
Bustle's Hardware & Doors
Phone (828) 645-7561
rabustle@gmail.com

Sub Total = \$2,478.00
7% Tax = \$173.46
<hr/> Total = \$2,651.46

On Wed, Jun 28, 2017 at 9:03 AM, Seth Chambers <schambers@americansouthgc.com> wrote:

All,

Please find the attached ASI-07 for review, comment, and pricing. I understand this doesn't affect many of you guys but want you to have it for your own records. I need all of you guys to review this and make sure there has been nothing added to your scope of work.

Below are the contractors I know may have pricing, all others still need to review and verify there are no changes for you.

- Bustle's Hardware
- Merrill Masonry
- Fountain Electric
- TP Howard's Plumbing
- Tindall

Thanks,

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: General Contractor
Contractor: American South General Contractors

Proposal #: 24
Project # 09-163
Date: 9.20.17

Description of Change: Cost to install the door frame, doors, and hardware into the crawlspace as called for in ASI-07. For Material Quote see Bustle's Hardware information provided.

Material:		Subtotals
	1. Total Cost of Materials (Attach Backup)	
	2. Sales Tax 7%	\$0.00
	3. Overhead & Profit 10%	\$0.00
		\$0.00
Labor:		
	4. Total Man Hour Cost (Attach Backup)	\$200.00
	5. Payroll Burden (Enter Percentage) 35%	\$70.00
	6. Overhead & Profit (In Overall COR) 0%	\$0.00
		\$270.00
Equipment:		
	7. Equipment / Equipment Rental (Attach Backup)	
	8. Overhead & Profit 10%	\$0.00
		\$0.00
Unit Prices:		
	9. Unit Price (If Applicable)	\$0.00
Subcontractors:		
	10. Subcontractor (Attach Subcontractor Backup)	
	11. Overhead & Profit 10%	\$0.00
		\$0.00
		Subtotal of Proposal \$270.00
	12. Payment & Performance Bond	\$0.00
	(Enter Bond Rate Percentage) (In Overall COR)	\$0.00
	Total of Change Order Proposal	\$270.00

Time Extension Request: 0 Days

Schedule Activity # Affected N/A

Does this effect the critical path? No

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: American South GC (Seth Chambers)

Date: 9.20.17

2 Men @ \$20/HR Each - 2 Hour Delivery & Unload, 1 Hour Set Frame, 1 Hour Set Door, 1 Hour Hardware

Drawn By: Author
 Checked By: Checker
 Approved By: Approver



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



Asheville Regional Airport Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

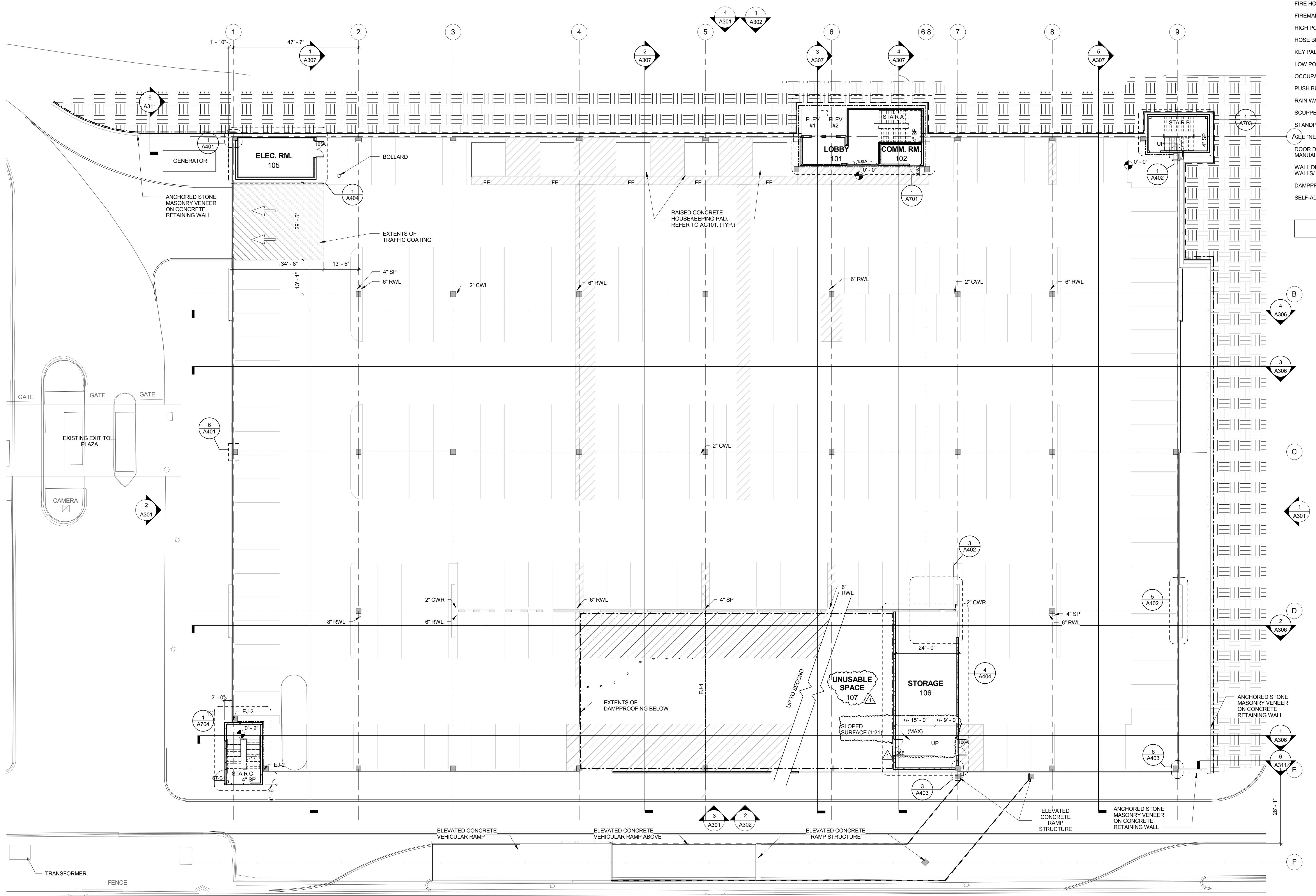


LEGEND

- TYPE A - 1 HOUR FIRE BARRIERS
- TYPE C - 2 HOUR FIRE BARRIERS
- TYPE N - NON-RATED PARTITION
- ACCESSIBLE
- AUTOMATIC DOOR (INTEGRAL HOLD-OPEN IS NOT ALLOWED)
- CARD READER
- COLD WATER RISER
- CONDUCTOR HEAD
- COURTESY TELEPHONE
- DISCONNECT PANEL
- ELECTRIC WATER COOLER
- ELECTRICAL PANEL
- EXPANSION JOINT
- FIRE DEPARTMENT VALVE CABINET
- FIRE DEPARTMENT VALVE CABINET WITH FIRE EXTINGUISHER
- FIRE EXTINGUISHER CABINET
- FIRE HOSE CABINET
- FIRE HOSE CABINET WITH FIRE EXTINGUISHER
- FIREMAN'S TELEPHONE
- HIGH POINT
- HOSE BIB
- KEY PAD
- LOW POINT
- OCCUPANCY SENSOR
- PUSH BUTTON FOR AUTOMATIC DOOR
- RAIN WATER LEADER
- SCUPPER
- STANDPIPE
- SEE "NEW CONSTRUCTION KEYNOTES" ON THIS SHEET
- DOOR DESIGNATION (SEE SHEET A91, AND PROJECT MANUAL FOR DOOR FRAME AND HARDWARE SCHEDULE)
- WALL DESIGNATION (SEE SHEET X13 FOR TYPES OF WALLS/PARTITIONS/BARRIERS)
- DAMP-PROOFING
- SELF-ADHERING SHEET WATERPROOFING

- ACC
- AUTO
- CR
- CWR
- CH
- C TEL
- DP
- EWC
- EP
- EJ
- FDVC
- FDVC W/ FE
- FEC
- FHC
- FHC W/ FE
- FT
- HP
- HB
- KP
- LP
- OC
- PB
- RWL
- SCP
- SP
- 101
- A00
- 101

KEYNOTES



Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	06/27/17	ASI #07 (PR #02)

NEW CONSTRUCTION PLAN - GROUND TIER

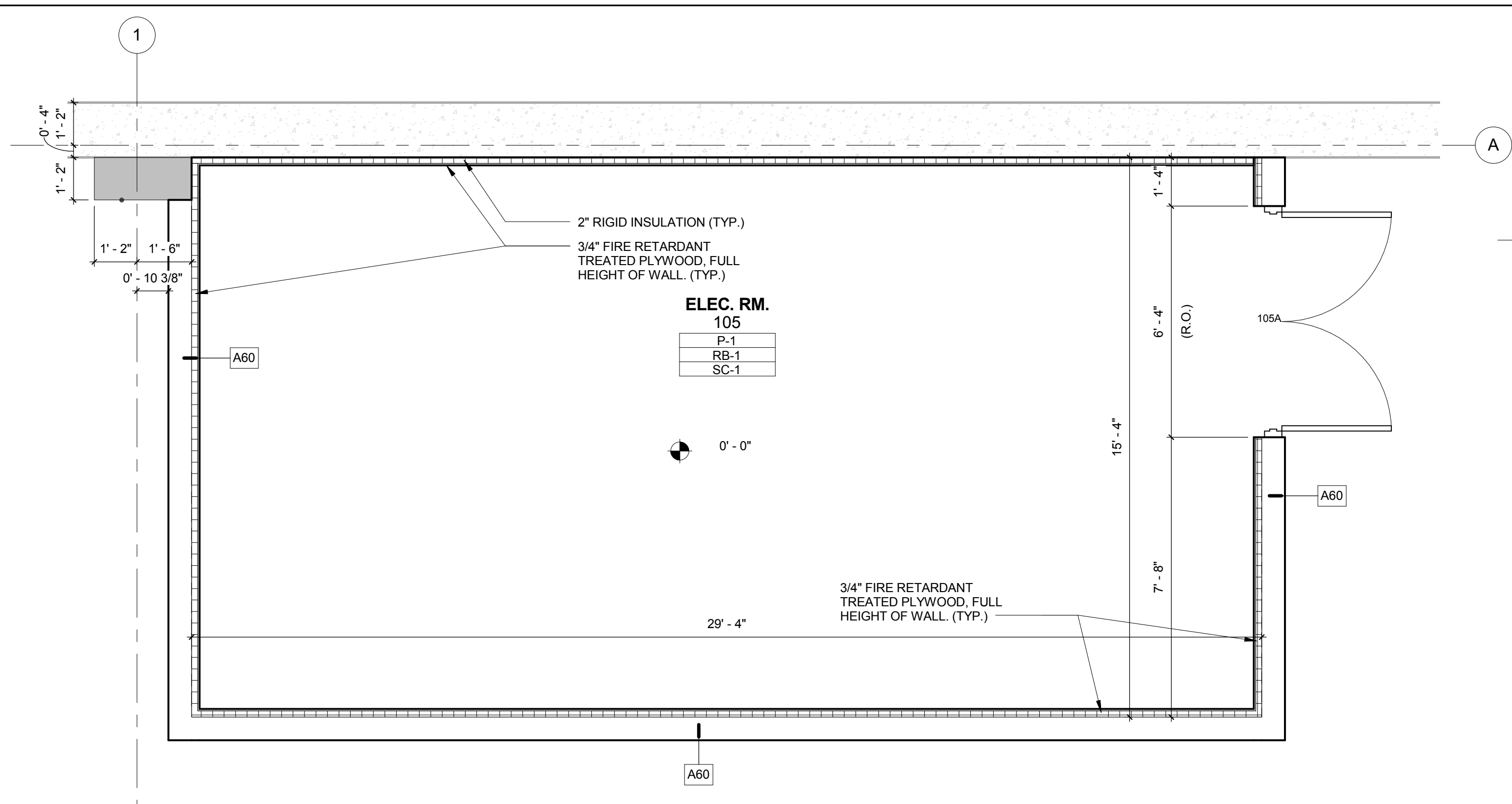
A211

PROJECT: 41152.00
 DATE: 21 SEPTEMBER 2016

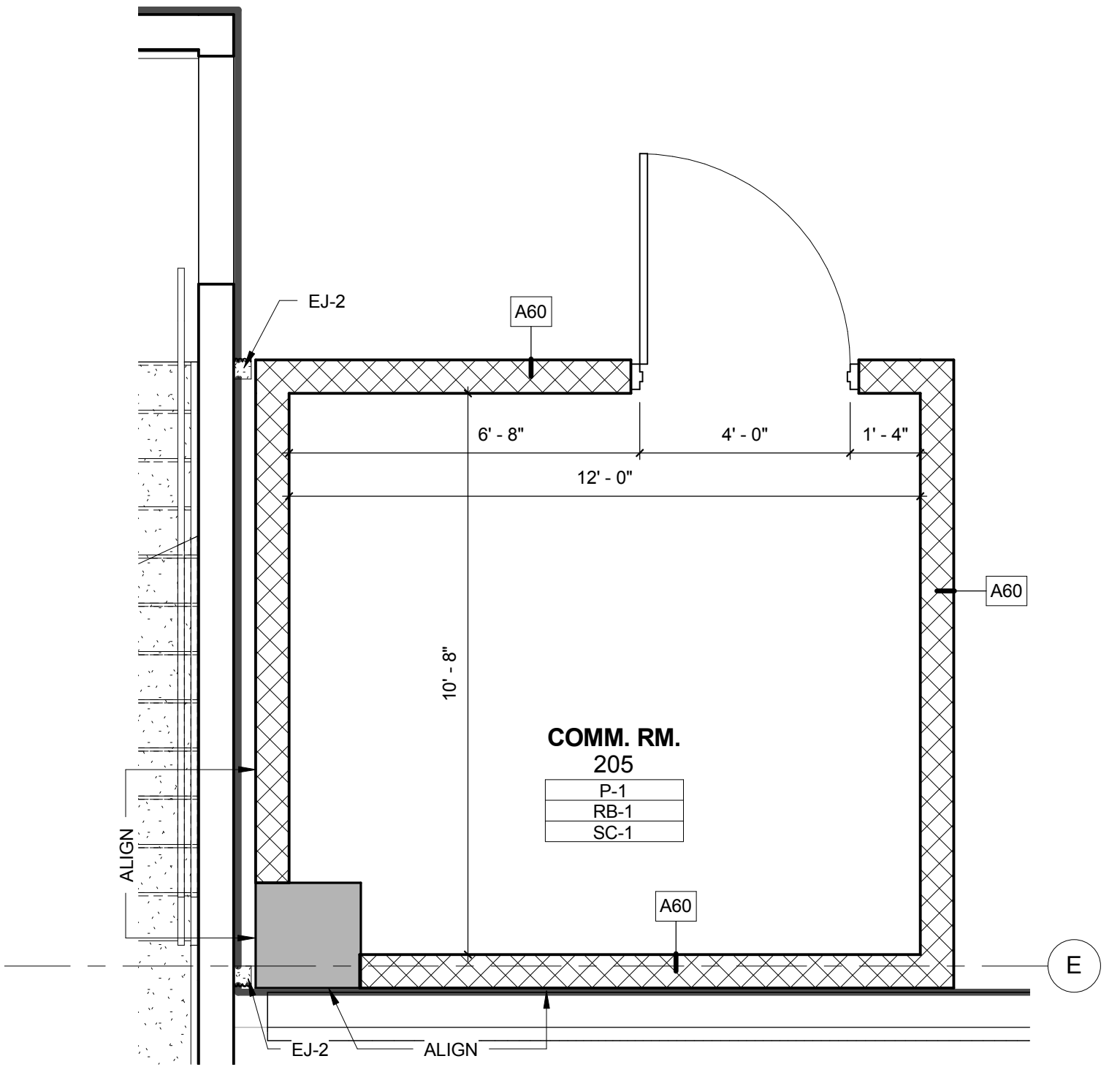
1 NEW CONSTRUCTION PLAN - GROUND TIER
 1/16" = 1'-0"
 PLAN NORTH

6/27/2017 9:23:09 PM

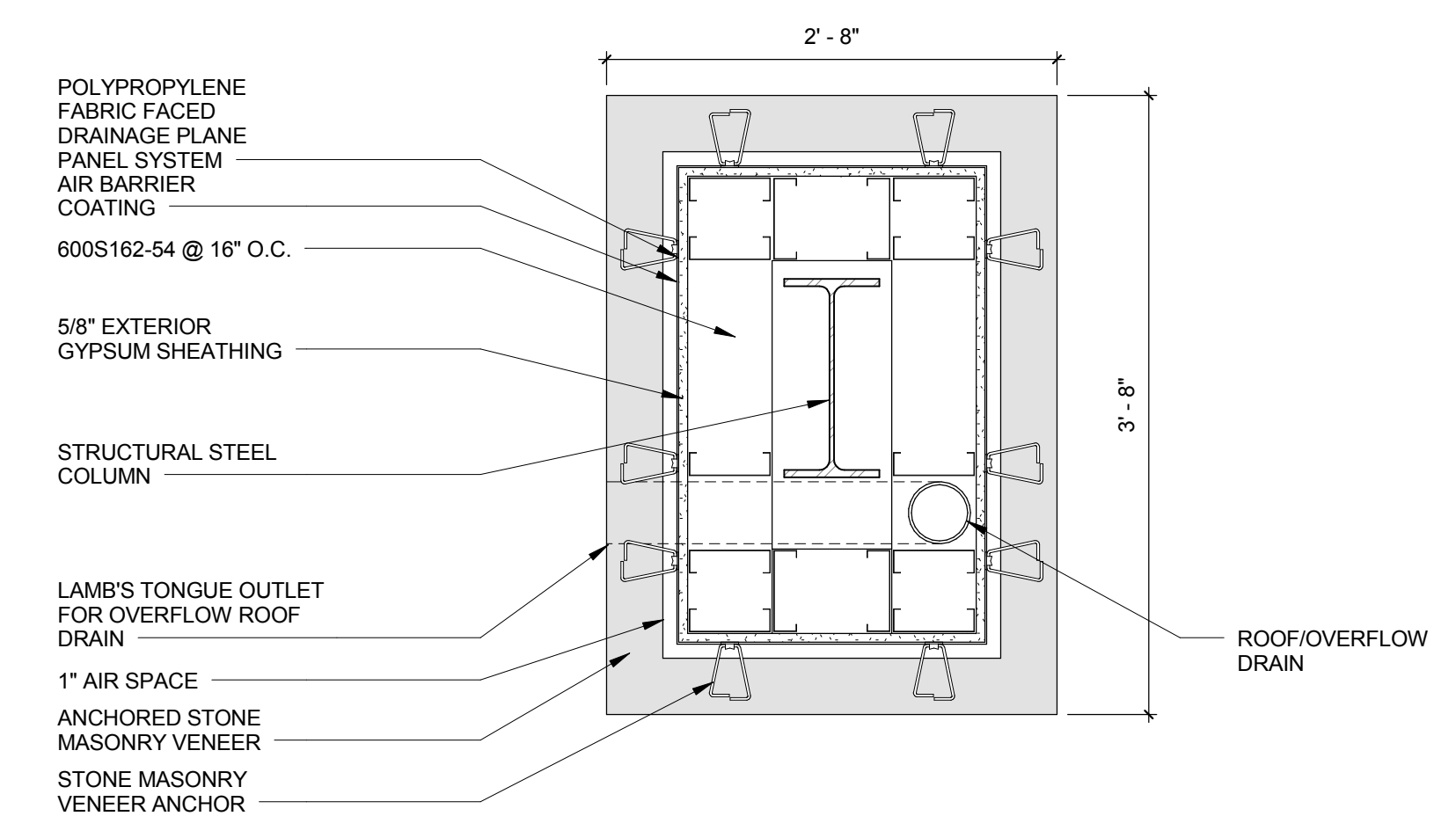
Drawn By: Author
 Checked By: Checker
 Approved By: Approver



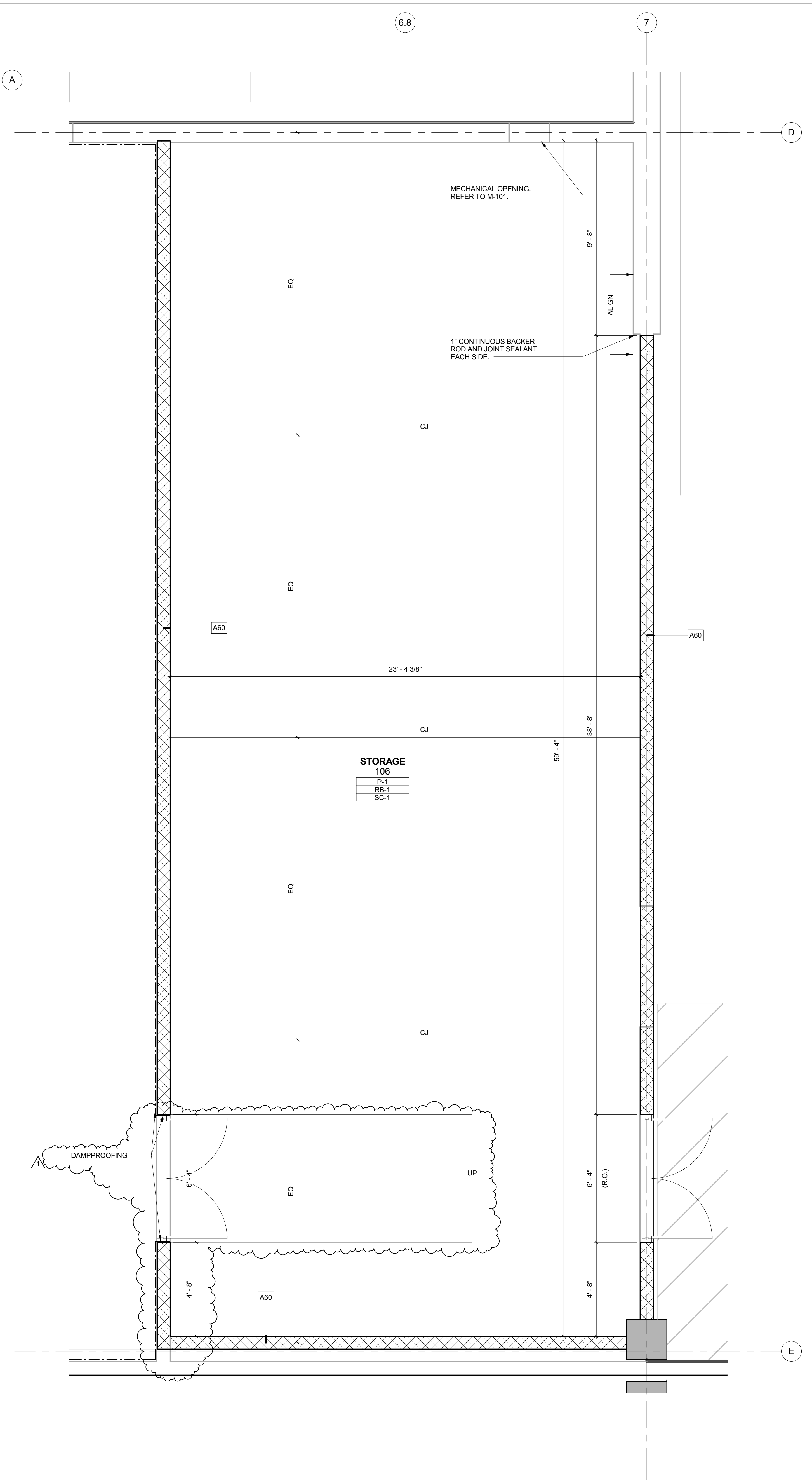
1 ENLARGED PLAN - ELECTRICAL ROOM - GROUND TIER
 3/8" = 1'-0"



2 ENLARGED PLAN - COMM. RM. (TYP)
 3/8" = 1'-0"



3 PLAN DETAIL - STONE COLUMN WRAP
 1" = 1'-0"



4 ENLARGED PLAN
 3/8" = 1'-0"

KEYNOTES		



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

- Atlanta GA
- Birmingham AL
- Charlotte NC
- Cincinnati OH
- Columbus OH
- Dallas TX
- Ft Lauderdale FL
- Houston TX
- Jackson MS
- Jacksonville FL
- Knoxville TN
- Louisville KY
- Memphis TN
- Miami FL
- Nashville TN
- Richmond VA
- Tallahassee FL
- Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



Asheville Regional Airport Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732



Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	06/27/17	ASI #07 (PR #02)

ENLARGED PLANS AND INTERIOR ELEVATIONS

A404

PROJECT: 41152.00
 DATE: 21 SEPTEMBER 2016

6/27/2017 9:23:13 PM

Drawn By: Author
Checked By: Checker
Approved By: Approver

INTERIOR DESIGN GENERAL NOTES

A. GENERAL INFORMATION

1. THE CONTRACTOR SHALL NOT SCALE DRAWINGS. IF DIMENSIONS ARE IN QUESTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING CLARIFICATION FROM THE ARCHITECT BEFORE CONTINUING WITH WORK.
2. THE CONTRACTOR SHALL NOTIFY THE ARCHITECT OF ITEMS REQUIRING FINISH SELECTIONS THAT ARE NOT CLARIFIED IN THE CONTRACT DOCUMENTS OR IN THE PROJECT MANUAL.
3. THE CONTRACTOR SHALL NOTIFY THE ARCHITECT IF ANY WORK INDICATED IN THE CONTRACT DOCUMENTS CAN NOT BE PERFORMED DUE TO EXISTING FIELD CONDITIONS.
4. THE CONTRACTOR SHALL ARRANGE FOR A PRE-INSTALLATION CONFERENCE CONCERNING RESILIENT FLOORING, RESILIENT BASE, ETC. THIS PRODUCT SHALL BE INSTALLED IN PRIMARY ELEVATOR & STAIR CORE. PARTIES TO ATTEND SHALL BE CONTRACTOR'S JOB SUPERINTENDENT, FLOORING SUB-CONTRACTOR, AND ARCHITECT.

C. FLOORING TRANSITIONS

1. ALL FLOORING TRANSITIONS SHALL OCCUR AT THE CENTERLINE OF ALL DOORS, UNLESS OTHERWISE DESIGNATED BY FLOOR TRANSITION SYMBOLS ON THE FINISH PLANS.
2. SEE SPECIFICATION SECTION 09 6513 FOR CLARIFICATION OF FLOORING TRANSITIONS.

D. TRAFFIC COATINGS, SEALED CONCRETE AND STAINED CONCRETE

1. THE "TRAFFIC COATING" SHALL BE EXTENDED UP THE WALL TO A HEIGHT OF 16 INCHES MINIMUM AFF.
2. PROVIDE SEALER (PER SPECIFICATION SECTION 09 9113) ONLY IN SPACES THAT ARE NOT SUBJECT TO BEING WET OR HOSED DOWN INCLUDING STAIRS, ELECTRICAL ROOMS, COMMUNICATIONS ROOMS, ETC.

G. PAINTING

1. PAINT FINISHES ON FLOORS SHALL MEET ALL ADA, ICC/ANSI AND OTHER APPLICABLE ACCESSIBILITY CODE REQUIREMENTS FOR SKID RESISTANCE. REFER TO SPECIFICATION SECTION 09 9113.
2. IF ADJACENT WALL IS SCHEDULED TO HAVE PAINT FINISH, PAINT UNDERSIDE SURFACE OF BULKHEADS AND SOFFITS TO MATCH, UNLESS OTHERWISE NOTED. IF ADJACENT WALL IS SCHEDULED TO HAVE A PAINT FINISH, PAINT VERTICAL AND HORIZONTAL SURFACES OF BULKHEADS AND SOFFITS TO MATCH WALL PAINT FINISH, UNLESS OTHERWISE NOTED.
3. WALLS IN ALL SPACES WITH EXPOSED CEILING SHALL BE PAINTED UP TO THE UNDERSIDE OF FLOOR/ROOF SLAB ABOVE.

H. WINDOWS, DOORS, DOOR FRAMES, ACCESS DOORS, ETC.

1. CONTRACTOR SHALL PAINT ALL METAL ACCESS DOORS, ELECTRICAL PANELS, RECESSED CABINET FACES, HVAC REGISTERS, GRILLES, ETC. TO MATCH ADJACENT WALL COLOR, UNLESS LOCAL CODES REQUIRE OTHERWISE. ALL FIRE EXTINGUISHER CABINETS AND FIRE DEPARTMENT VALVE CABINETS SHALL BE PAINTED TO MATCH ADJACENT WALL COLOR, UNLESS OTHERWISE NOTED.
2. HOLLOW METAL DOORS, DOOR FRAMES, WINDOW FRAMES AND VIEW WINDOW FRAMES SHALL BE PAINTED AS FOLLOWS:
3. IN THE CASE WHERE A DOOR FRAME RECEIVES A GIVEN PAINT COLOR ON ONE SIDE OF THE DOOR, AND ANOTHER PAINT COLOR IN THE OPPOSITE SIDE OF THE DOOR, THE TRANSITION SHALL OCCUR ON THE INSIDE CORNER OF THE JAMB STOP, WHICH IS NOT VISIBLE WHEN THE DOOR IS IN THE CLOSED POSITION.

INTERIOR DESIGN FINISH LEGEND

SEALED CONCRETE

SC-1 MFR: SHERWIN WILLIAMS
COLOR: H&C SILICONE ACRYLIC CONCRETE SEALER

RESILIENT TILE FLOORING

VCT-1 MFR: ARMSTRONG
PATT: STANDARD EXCELOP IMPERIAL TEXTURE
COLOR: 51915 CHARCOAL
SIZE: 12"x12"

VCT-2 MFR: ARMSTRONG
PATT: STANDARD EXCELOP IMPERIAL TEXTURE
COLOR: 51946 GENTIAN BLUE
SIZE: 12"x12"

VCT-3 MFR: ARMSTRONG
PATT: STANDARD EXCELOP IMPERIAL TEXTURE
COLOR: 62517 JESTER RED
SIZE: 12"x12"

RESILIENT BASE

RB-1 MFR: ARMSTRONG
PATT: COVE ROLL BASE
COLOR: V6871 MID GRAY
HEIGHT: 6"

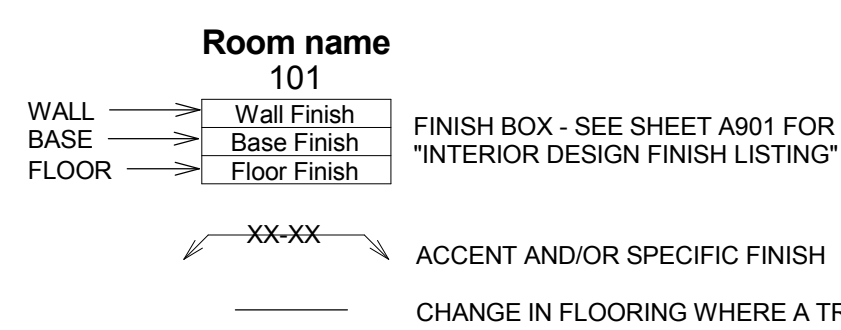
PAINTING (WALL)

P-1 GENERAL WALL PAINT
MFR: SHERWIN WILLIAMS
COLOR: SW 7004 SNOWBOUND
FINISH: LOW SHEEN

P-2 GENERAL WALL PAINT (ELEVATOR SHAFT)
MFR: SHERWIN WILLIAMS
COLOR: SW 6258 TRICORN BLACK
FINISH: LOW SHEEN

FP-1 FRAME PAINT
MFR: SHERWIN WILLIAMS
COLOR: SW 7689 SUMMIT GRAY
FINISH: SEMI-GLOSS

FINISH & ACCENTS LEGEND

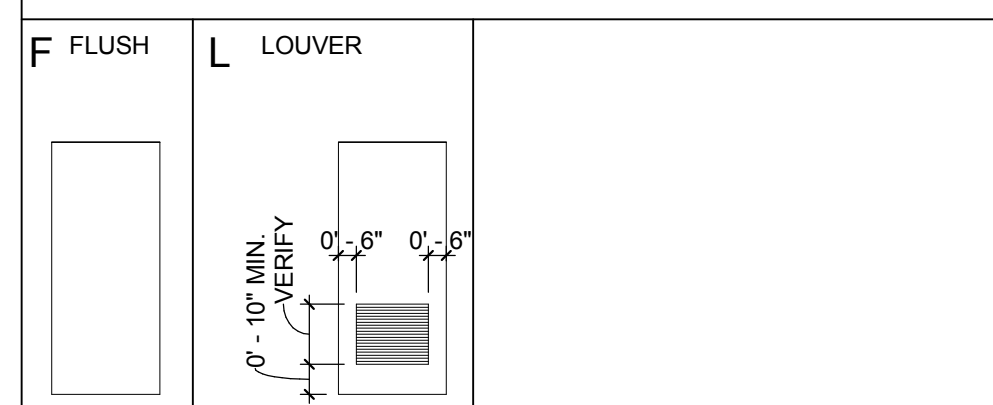


- NOTES:
- A. THIS FINISH & ACCENT FINISH LEGEND IS ONLY A SYMBOLIC REPRESENTATION OF DIFFERENT MATERIAL TYPES USED FOR THIS PROJECT.
 - B. THE CONTRACTOR SHALL REFER TO THE INTERIOR DESIGN FINISH LISTING ON SHEET A901 THE INTERIOR DESIGN DRAWINGS, AND THE PROJECT MANUAL FOR ADDITIONAL PRODUCT AND COLOR SPECIFICATIONS.
 - C. SEE SHEET A901 FOR "INTERIOR DESIGN GENERAL NOTES".

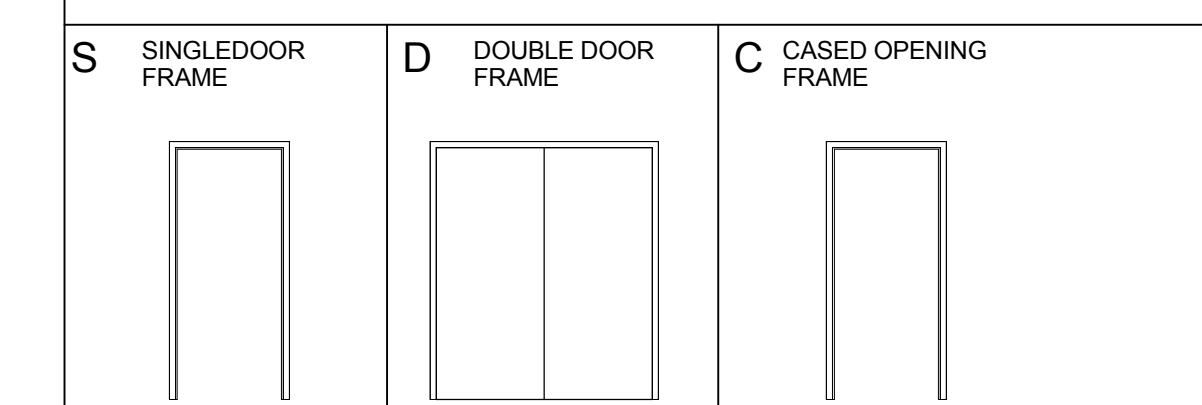
DOOR, FRAME AND HARDWARE SCHEDULE

REVISION NUMBER	SPACE NO. / DOOR DESIGNATION	FROM ROOM	TO ROOM	DOOR				FRAME			FIRE RATING	Hardware Set	REMARKS	
				WIDTH	HEIGHT	THICK	MATL	TYPE	GLASS	MATL				TYPE
0	101A		LOBBY	12'-0"	8'-9"	0'-2"	ALUM.	MB	ALUM.	N/A		2 HR	05	
0	102A	COMM. RM.		4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	01	
0	105A	ELEC. RM.		6'-0"	7'-0"	0'-1 3/4"	HM	F	HM	D		1 HR	02	
0	106A	STORAGE		6'-0"	7'-0"	0'-1 3/4"	HM	F	HM	D		2 HR	02	
2	106B	UNUSABLE SPACE	STORAGE	6'-0"	7'-0"	0'-1 3/4"	HM	F	HM	D		1 HR	02	
0	201A	LOBBY	LOBBY	12'-0"	8'-9"	0'-2"	ALUM.	MB	ALUM.	N/A		2 HR	05	
0	201B	LOBBY		11'-11 1/2"	7'-6"	0'-2"	ALUM.	MB	ALUM.	N/A				
0	202A	COMM. RM.		4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	01	
0	205A	ELEC. RM.	COMM. RM.	4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	04	180 DEGREE SWING
0	301A	LOBBY		12'-0"	8'-9"	0'-2"	ALUM.	MB	ALUM.	N/A		2 HR	05	
0	302A	STORAGE		4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	01	
0	401A	LOBBY		12'-0"	8'-9"	0'-2"	ALUM.	MB	ALUM.	N/A		2 HR	05	
0	402A	COMM. RM.		4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	01	
0	405A	ELEC. RM.	COMM. RM.	4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		1 HR	04	180 DEGREE SWING
0	501A	LOBBY		12'-0"	8'-9"	0'-2"	ALUM.	MB	ALUM.	N/A		2 HR	05	
0	502A	ELEV. MR.		6'-0"	7'-0"	0'-1 3/4"	HM	F	HM	D		1 HR	03	180 DEGREE SWING
0	ST-B2			4'-0"	8'-2"							06		
0	ST-B5			4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		2 HR	07	
0	ST-C1			4'-11"	7'-8"							08		
0	ST-C5			4'-0"	7'-0"	0'-1 3/4"	HM	F	HM	S		2 HR	07	

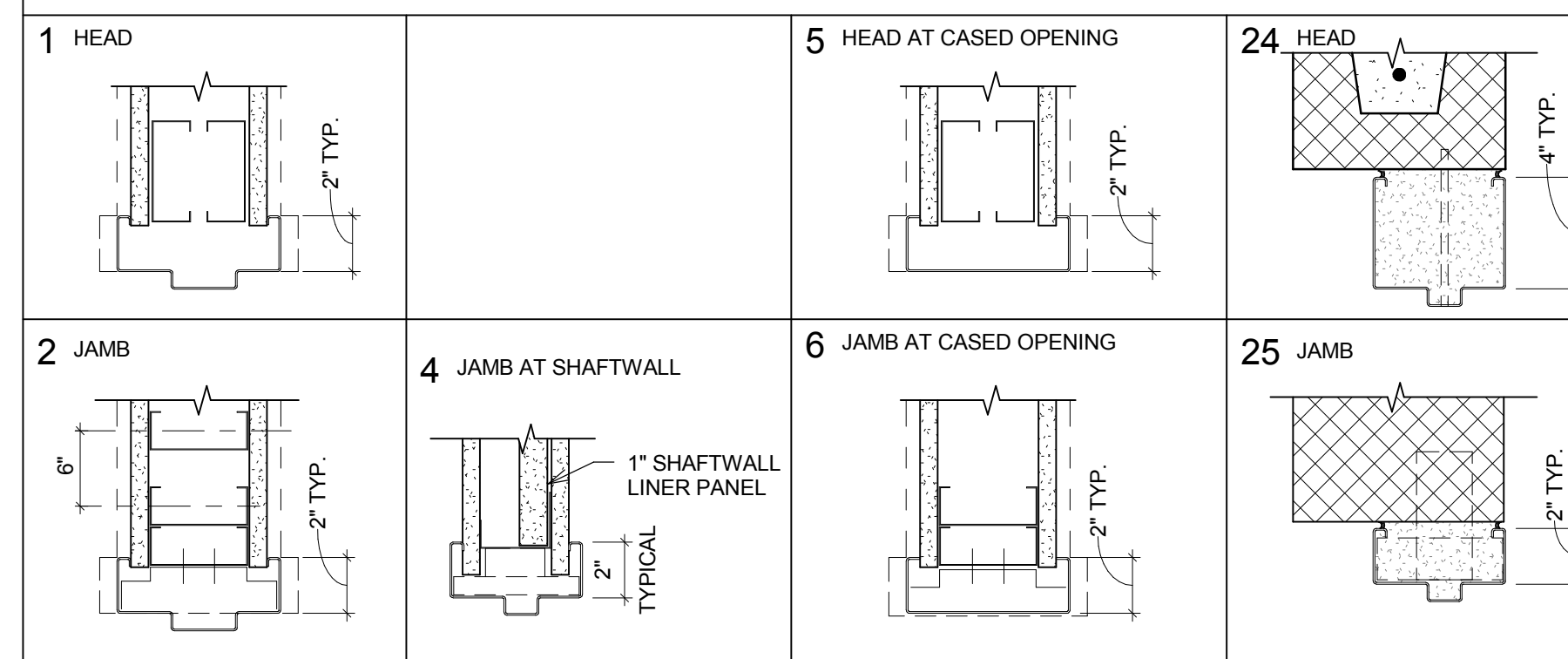
LEGEND FOR DOOR TYPES



LEGEND FOR DOOR FRAME TYPES



DOOR AND INTERIOR WINDOW FRAME DETAILS



DOOR, FRAME & HARDWARE NOTES & ABBREVIATIONS

- NOTES:
801. NOT USED
 802. STUDY AND COMPARE THE: DRAWINGS; LEGENDS; NOTES; SPECIFICATIONS; AND DOOR, FRAME & HARDWARE SCHEDULE. REPORT ANY ERROR, INCONSISTENCY OR OMISSION TO ARCHITECT.
 803. SEE DESCRIPTION OF THE HARDWARE SETS IN SPECIFICATION SECTION 08 7111 "DOOR HARDWARE".
 804. DOORS IN RATED FIRE BARRIERS, RATED SMOKE BARRIERS, NON-RATED SMOKE PARTITIONS AND SOUND PARTITIONS MAY BE UNDERCUT A MAXIMUM OF 3/4 INCH ABOVE THE CONCRETE SLAB. THIS 3/4 INCH MAXIMUM GAP ABOVE THE CONCRETE SLAB APPLIES FOR THE FULL WIDTH OF THE DOOR IN ITS CLOSED POSITION. (NOTE: DOORS IN THESE BARRIERS AND PARTITIONS WITH AN UNDERCUT GREATER THAN THIS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE).
 805. SEAL ALL FRAMES CONTINUOUSLY TO WALL. WHERE THERE IS A HORIZONTAL GAP OF 1/8 INCH OR LESS BETWEEN THE DOOR FRAME AND THE ADJACENT FINISH BASE AND/OR FLOOR MATERIAL, INSTALL A "PENCIL LINE" OF SEALANT BETWEEN THE FRAME AND THE FINISH BASE AND/OR FLOOR MATERIAL. HOWEVER, IF THIS HORIZONTAL GAP IS GREATER THAN 1/8 INCH, THE FINISH BASE AND/OR FLOOR MATERIAL SHALL BE REMOVED AND REINSTALLED TO ELIMINATE THE GAP.
 806. IF THERE IS A VERTICAL GAP OF 1/8 INCH OR GREATER BETWEEN THE BOTTOM OF THE DOOR FRAME AND THE CONCRETE SLAB, THIS GAP SHALL BE REPAIRED WITH "BONDO" OR "GROUTED SOLID" TO PROVIDE "TESTED FIRESTOP ASSEMBLY". THIS "REPAIR" SHALL COMPLY WITH THE REQUIREMENTS OF NFPA 80.
 807. DOORS SCHEDULED TO HAVE A FIRE RATING LABEL SHALL HAVE A METAL LABEL PERMANENTLY AFFIXED TO BOTH THE DOOR AND THE FRAME. THE LABELS SHALL NOT BE PAINTED. (NOTE: A VINYL LABEL IS NOT ACCEPTABLE, AND A LABEL THAT IS "EMBOSSED INTO THE FRAME" IS NOT ACCEPTABLE.)
 808. ALL DOORS THAT PENETRATE RATED ENCLOSURES SURROUNDING STAIRS, STAIR VESTIBULES AND EXIT PASSAGEWAYS SHALL HAVE A TEMPERATURE RISE RATING (TRR) METAL LABEL PERMANENTLY AFFIXED TO THE DOOR.
 809. ALL THRESHOLDS SHALL CONFORM TO THE MOST STRINGENT REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) AND ALL OTHER ACCESSIBILITY CODES ENFORCED BY THE AUTHORITIES HAVING JURISDICTION.
 810. VERIFY THAT DOOR CLOSERS AND OTHER HARDWARE WILL ALLOW DOOR TO SWING OPEN 180 DEGREES WHEN A "180 DEGREE SWING" IS EITHER SHOWN ON THE FLOOR PLAN AND/OR INDICATED IN THE DOOR SCHEDULE.
 811. PROVIDE TESTED FIRE RATED GLASS ASSEMBLY IN DOOR LITES OF INTERIOR RATED DOORS, WHERE INDICATED FOR USE IN NON-RATED DOORS. PROVIDE NON-RATED INTERIOR DOOR LITES WITH INTEGRAL VISUAL OBSCURING ABILITY (VISTAMATIC OR APPROVED EQUAL) UNLESS OTHERWISE NOTED OR REQUIRED BY CODE. PROVIDE 1/4 INCH MINIMUM THICKNESS OF TEMPERED SAFETY GLASS IN ALL OTHER INTERIOR DOOR LITES. COMPLY WITH THE FOLLOWING FOR ALL EXTERIOR DOORS: REQUIREMENTS OF THE AUTHORITIES HAVING JURISDICTION, CODE REQUIREMENTS RELATED TO HIGH VELOCITY WIND ZONES, SPECIFICATIONS, AND NFPA 80.
 812. VERIFY THE REQUIRED "THROAT WIDTH" FOR EACH DOOR FRAME BASED ON: SIZE OF STUDS; THICKNESS AND NUMBER OF LAYERS OF GYPSUM BOARD, GYPSUM SHEATHING, ETC.; CMU SIZE; CONCRETE WALL THICKNESS, ETC. AFTER TOTALING THE WIDTH OF THE COMPONENTS TO BE INSERTED INTO THE THROAT OF EACH FRAME, ADD AN ADDITIONAL 1/4 INCH TO THE CLEAR "THROAT WIDTH" DIMENSION TO ALLOW FOR CONSTRUCTION TOLERANCES. COMPLY WITH NOTES #813 AND #814.
 813. VERIFY THAT THE THROAT OF EACH FIRE RATED DOOR FRAME WRAPS ONLY AROUND THE RATED PORTION OF THE WALL/BARRIER. (NOTE: NON-RATED PORTIONS OF FURRED OUT WALLS/PARTITIONS, FURRING STRIPS, WOOD TRIM, WALL PROTECTION, ETC. SHALL NOT BE INSERTED INTO THE THROAT OF A RATED DOOR FRAME. SEE NOTES #812 AND #814.
 814. VERIFY THAT ALL REQUIRED COMPONENTS OF THE RATED WALL/BARRIER ARE INSERTED A MINIMUM OF 1/2 INCH INTO THROAT OF RATED DOOR FRAME TO COMPLY WITH NFPA 80 AND THE LATEST EDITION OF GYPSUM ASSOCIATION GA-600 FIRE RESISTANCE MANUAL. SEE NOTES #82 AND #813.
 815. VERIFY THAT ALL RATED DOORS AND FRAMES ARE FIRESTOPPED (TO MAINTAIN THE INTEGRITY OF THE RATED ASSEMBLY) AFTER DOOR HARDWARE AND SECURITY ITEMS (SUCH AS DOOR CONTACTS, ETC.) ARE INSTALLED.
 816. INSTALLATION OF DOORS, HARDWARE AND FRAMES SHALL COMPLY WITH ALL REQUIREMENTS OF NFPA 80 AND WITH OTHER CODES ENFORCED BY THE AUTHORITIES HAVING JURISDICTION.
 - *817. THE NORTH CAROLINA ACCESSIBILITY CODE SECTION 7.5.1 STATES THAT "IF VISION PANELS ARE PROVIDED IN CORRIDOR DOORS OR OPENINGS INTO CORRIDORS, THEN THE LOWER EDGE OF THE VISION PANEL SHALL BE 3'-4" MAXIMUM ABOVE THE FINISH FLOOR."

ABBREVIATIONS:		FIRE LABEL DESIGNATIONS:	
AUTO	AUTOMATIC DOOR (INTEGRAL HOLD OPEN NOT ALLOWED)	A-180	"A" LABEL - 180 MINUTE FIRE RATED
FR GL	FIRE RATED GLASS	8-60	"B" LABEL - 60 MINUTE FIRE RATED
GALV STL	GALVANIZED STEEL	C-60	"C" LABEL - 60 MINUTE FIRE RATED
HM	HOLLOW METAL	C-45	"C" LABEL - 45 MINUTE FIRE RATED
LAM GL	LAMINATED SAFETY GLASS	20 MIN.	"D" LABEL - 20 MINUTE FIRE RATED
OPP HD, OH	OPPOSITE HAND		
PR	PAIR OF DOORS		
SIM	SIMILAR		
STL	STEEL		
TEMP; TEMP GL	TEMPERED SAFETY GLASS (NON RATED USE ONLY)		
TRR	TEMPERATURE RISE RATING		



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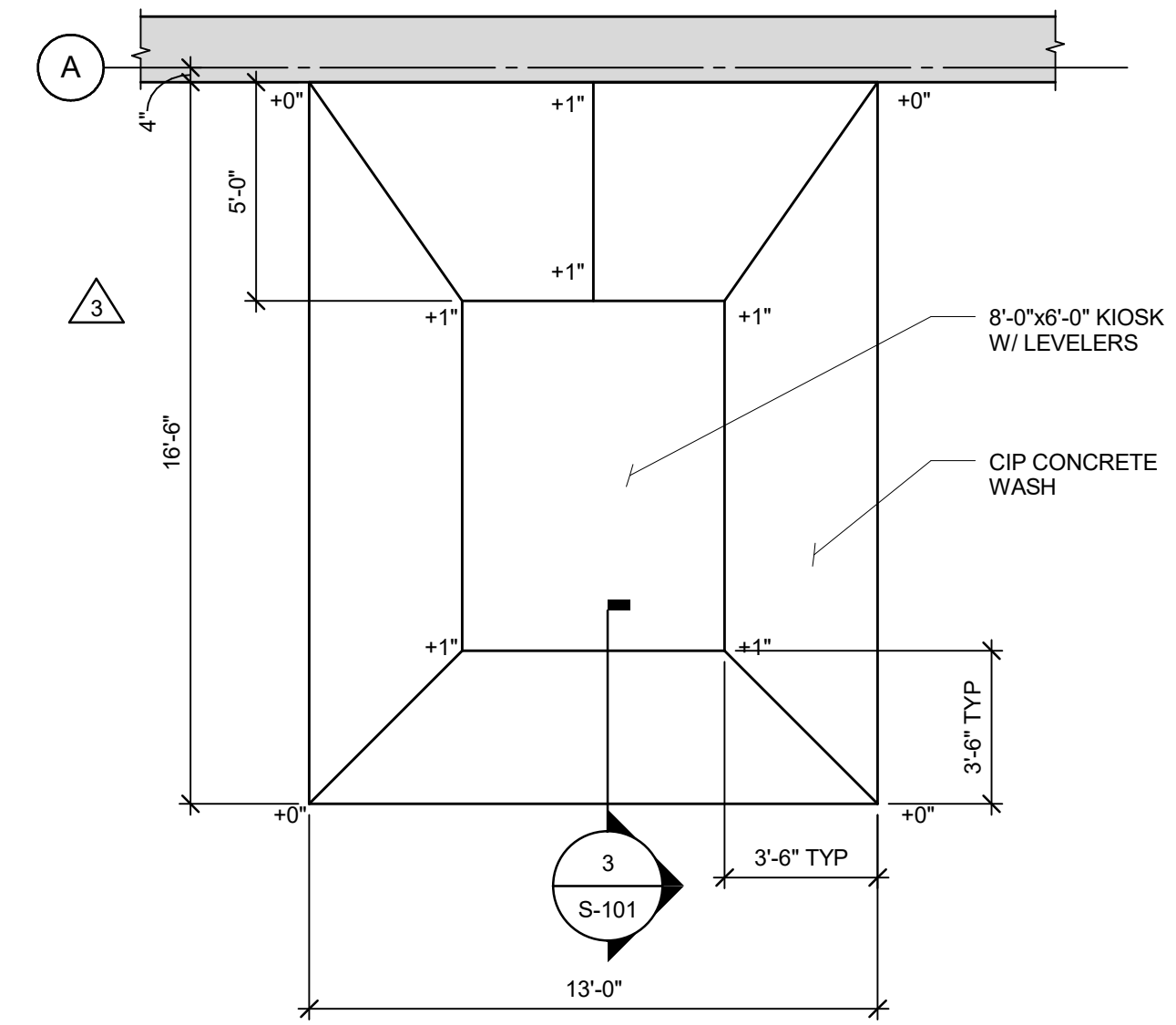
Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	12/22/16	ASI #04
2	06/27/17	ASI #07 (PR #02)

DOOR, FRAME & HARDWARE SCHEDULE

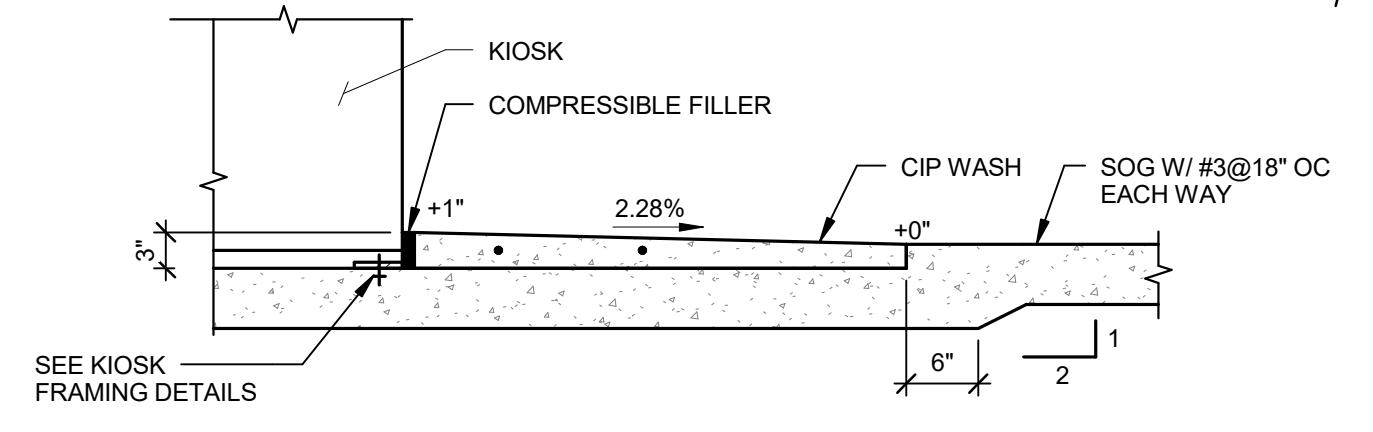
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PROJECT: 41152.00
DATE: 21 SEPTEMBER 2016

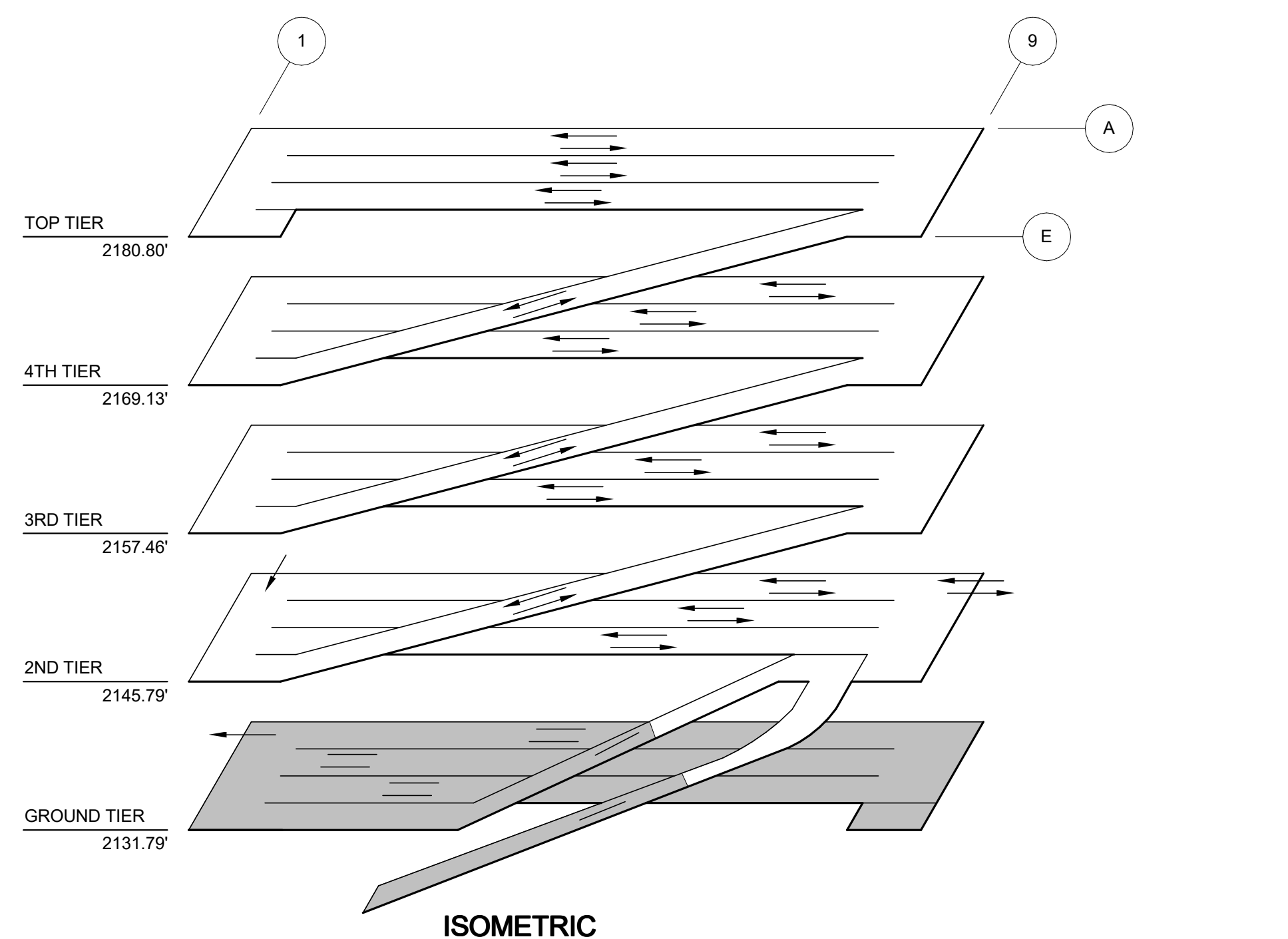
Drawn By: Author
 Checked By: Checker
 Approved By: Approver



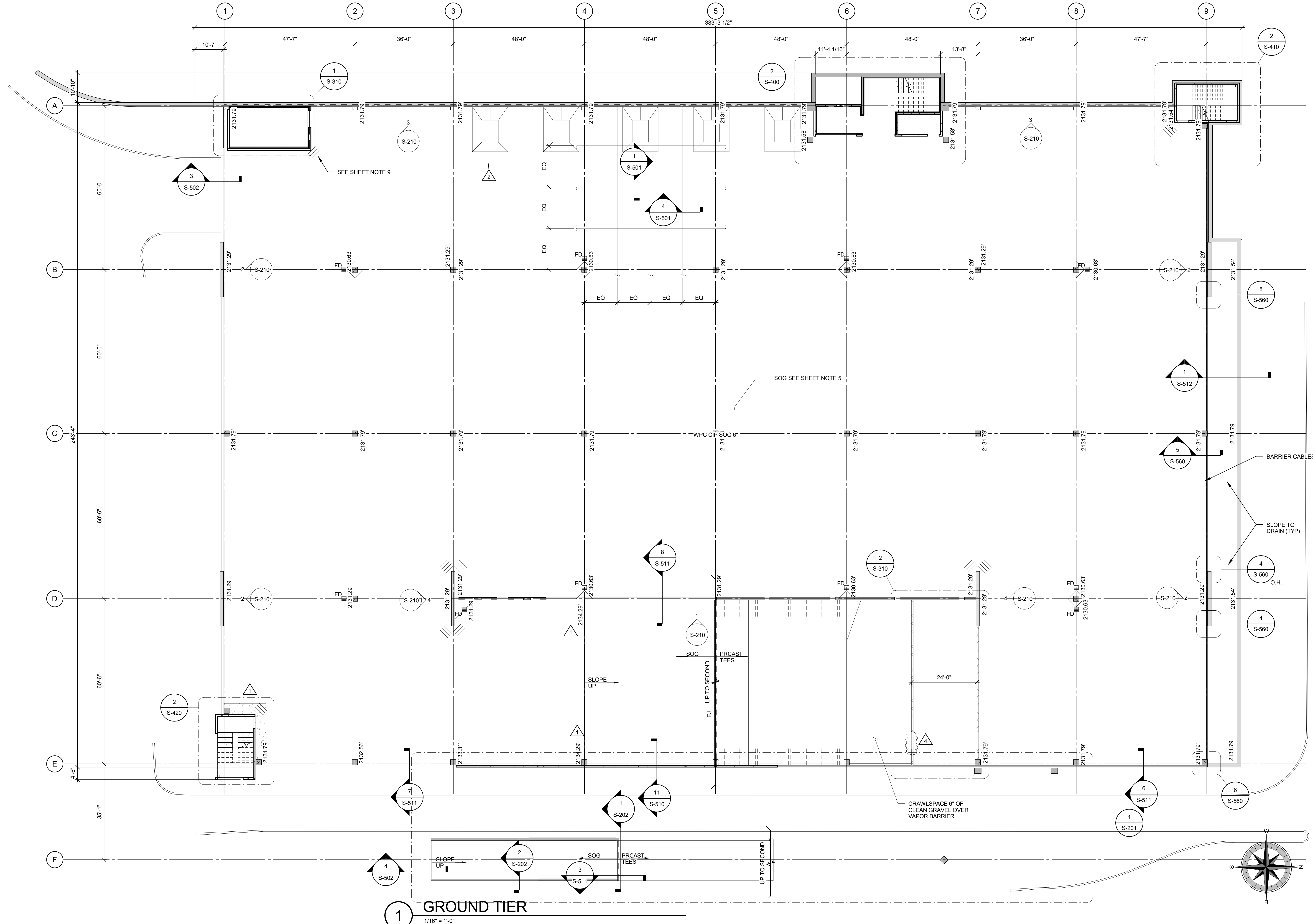
2 KIOSK ENLARGED PLAN
 1/4" = 1'-0"



3 SECTION
 3/4" = 1'-0"



ISOMETRIC



1 GROUND TIER
 1/16" = 1'-0"

GENERAL SHEET NOTES

- SHEET NOTES:**
- REFERENCES
 - GENERAL S-001 & S-002
 - PLAN ELEVATION KEY 6/S-502
 - EXTERIOR GRADE ELEVATION CIVIL DWGS
 - ENTRY/EXIT PLANS AG DWGS
 - STAIR & STAIR/ELEVATOR PLANS & DETAILS S-400 SERIES DWGS
 - BUILDING ELEVATIONS & SECTIONS SEE ARCH DWGS
 - LIGHT WALL & SHEAR WALL ELEVATIONS S-210
 - FRAMING SECTION & DETAILS S-560
 - WATERPROOFING DETAILS S-570
 - INDICATES PRECAST CONCRETE WALLS OR COLUMNS.
 - FD INDICATES FLOOR DRAIN.
 - XXXXX INDICATES TOP OF CONCRETE ELEVATION FROM DATUM AND DOES NOT INCLUDE WASHES.
 - CONCRETE SLAB ON GRADE SHALL BE 5" THICK AND REINFORCED W/ #3 @ 18" OC EACH WAY PLACED 2" CLR FROM TOP OF SLAB UNO. PROVIDE 1/2" JOINT FILLER AT GRADE SLAB AROUND COLUMNS. TYPICAL CAST SLAB TIGHT AGAINST CIP WALL W/ BOND BREAKER
 - INDICATES PLAN MATCHLINE.
 - SEE SHEETS S-101 FOR CJ REQUIREMENTS AT ALL SLABS UNO 15'-0" MAX SPACING.
 - PROVIDE CONSTRUCTION OR CONTROL JOINTS IN CAST IN PLACE WALLS PER S-501 TO MATCH JOINTS IN SLAB-ON-GRADE. 15'-0" MAX SPACING.
 - INDICATES (4)#5 (MIN) x 4'-0" (EPOXY COATED) @ MID-DEPTH PROVIDE AT ALL RE-ENTRANT CORNERS
 - PROVIDE VAPOR BARRIER BELOW SLAB-ON-GRADE AT ALL ENCLOSED ROOMS. SEE SPECS FOR REQUIREMENTS.
 - PROVIDE CONTROL JOINTS IN MASONRY WALLS PER S/S-560
 - PROVIDE AN 8'-0"x8'-0" x 2" CRICKET AROUND COLUMNS W/ DRAINS. SEE S/S-502.
 - CONTRACTOR TO PROVIDE RENTAL CAR AGENCIES ACCESS TO GROUND TIER FOR INSTALLATION OF EQUIPMENT PRIOR TO SUBSTANTIAL COMPLETION. SEE SPECIFICATION FOR ADDITIONAL INFORMATION.
 - SEE AG SHEETS FOR BOLLARD LOCATIONS AND DETAILS.
 - SEE AG SHEETS FOR FLEXIBLE DELINEATOR POSTS LOCATIONS AND DETAILS.

SHEET KEYNOTES

Asheville Regional Airport Parking Garage

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DELTA AIRPORT CONSULTANTS, INC.

WALKER
 PARKING CONSULTANTS
 NC CORPORATE LICENSE NO. F-0518

Revision		
No.	Date	Description
	9-21-16	CONFORMED CONTRACT DOCUMENTS
1	12-29-16	ASI #04
2	3-21-17	ASI #05
3	5-3-17	ASI #05 REV
4	6-21-17	ASI #07

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THERE IS A MINIMUM OF 10'-8" HEADROOM BETWEEN ALL DRIVING SURFACES AND OVERHEAD BEAMS PRIOR TO PLACING CONCRETE.

G S & P

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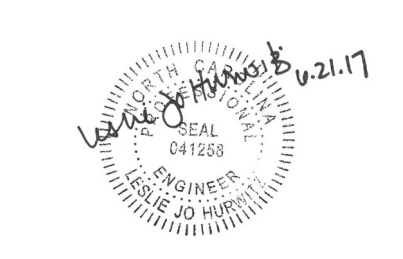
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4	6-21-17	ASI #07

GROUND TIER PLAN

S-101

PROJECT: 41152.00
 DATE: April 25, 2016



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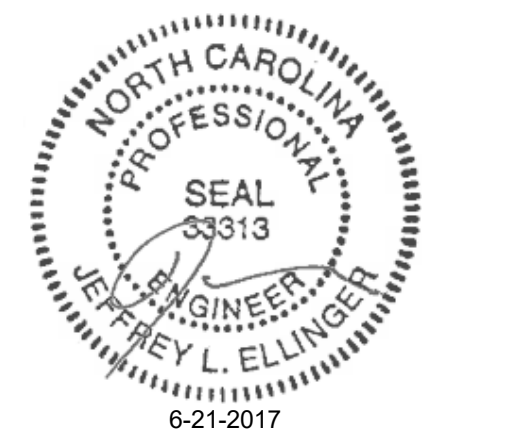
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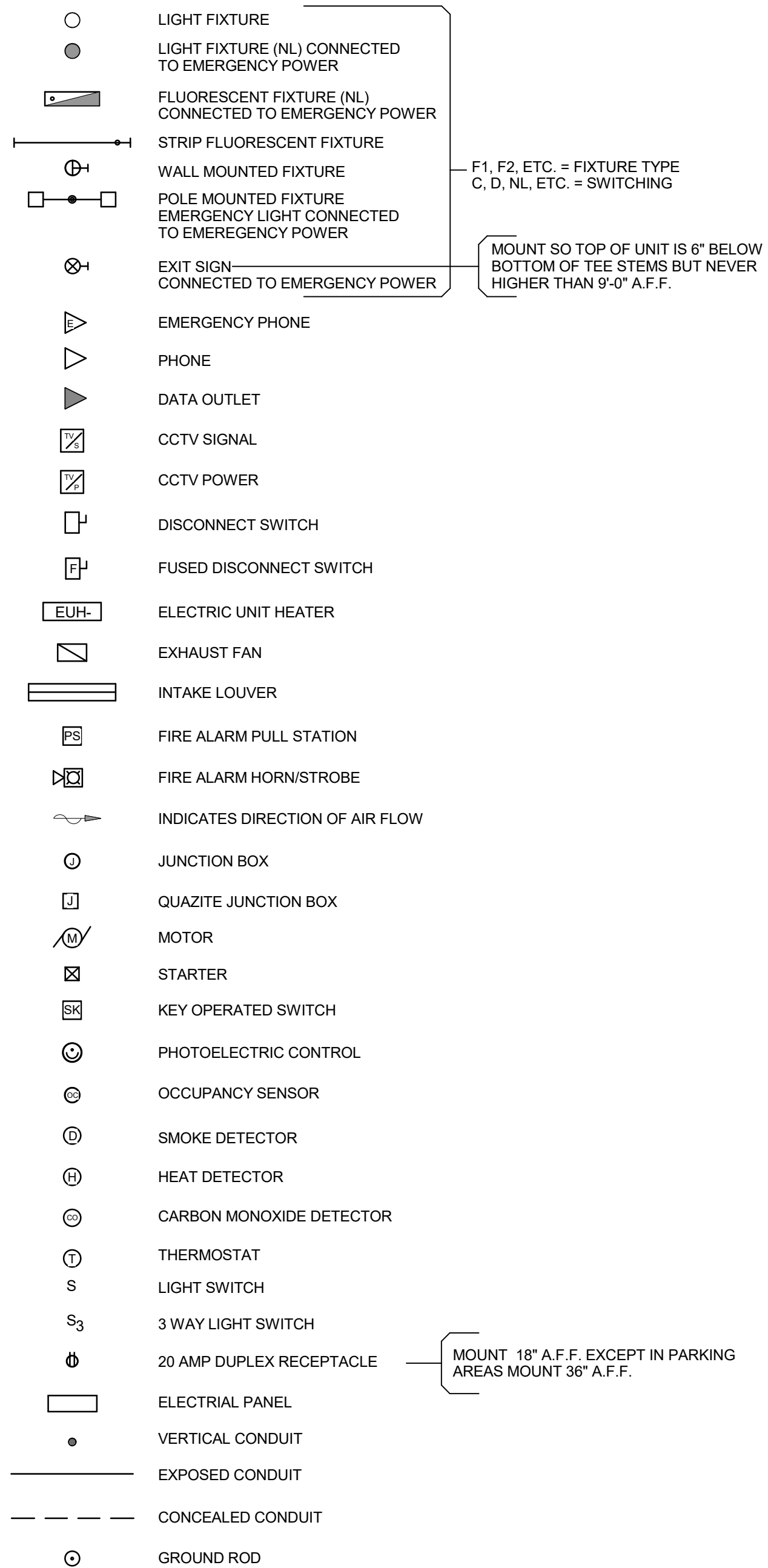
GENERAL SHEET NOTES

- 1. ALL FIXTURES ARE TYPE F1 UNLESS NOTED OTHERWISE. SEE FIXTURE MOUNTING DETAIL 1E-501. SEE LIGHT FIXTURE SCHEDULE ON THIS SHEET.
2. LIGHT FIXTURES AND CONDUIT ARE SHOWN IN REFLECTED CEILING ORIENTATION.
3. ALL CONDUITS SHALL CONTAIN A GREEN COLORED EQUIPMENT GROUNDING CONDUCTOR PROVIDING CONTINUITY TO ALL PANEL SCHEDULES AND FIXTURES. LIGHT FIXTURE CONDUCTORS SHALL BE NO. 10 THWN UNLESS NOTED OTHERWISE. ALL HOMERUN CONDUCTOR SIZES SHALL BE AS NOTED ON THE PANEL SCHEDULES UNLESS BASED ON USE OF THWN INSULATION AND NEC REQUIREMENTS.
4. ALL ABOVE GROUND CONDUIT SHALL BE SURFACE MOUNTED RIGID HOT DIPPED GALVANIZED STEEL WITH THREADED FITTINGS UTILIZING HOT DIPPED GALVANIZED, CAST, OR MALLEABLE IRON THREADED JUNCTION BOXES AND COVERS. ALL UNDERGROUND CONDUIT SHALL BE PVC. MINIMUM CONDUIT SIZE IS 3/4" UNLESS NOTED OTHERWISE. USE SEAL TIGHT FLEXIBLE CONDUIT IN LENGTHS NO GREATER THAN 2' TO CONNECT MOTORS TRANSFORMERS AND FOR WHIPS CONNECTING TRUNNION MOUNTED FIXTURES TO JUNCTION BOXES. DO NOT INSTALL FLEXIBLE CONDUIT AT OTHER LOCATIONS WITHOUT WRITTEN APPROVAL OF ENGINEER.
5. ALL HORIZONTAL CONDUIT RUNS ARE TO BE ROUTED EXPOSED ALONG THE CEILING THROUGH BLOCKOUTS IN TEE STEMS AND BEAMS. COORDINATE REQUIRED BLOCK-OUTS IN TEE STEMS AND BEAMS WITH STRUCTURAL ENGINEER. ALL VERTICAL CONDUIT RUNS SHALL BE ROUTED EXPOSED ALONG THE COLUMNS/WALLS IN PARKING AREAS. HOMERUNS TO ELECTRICAL ROOM ARE TO BE ROUTED DOWN ADJACENT COLUMNS THEN UNDERGROUND TO THE ELECTRICAL ROOM. ALL EXPOSED CONDUIT MUST BE ROUTED UP TIGHT AGAINST THE CEILING. COLUMNS AND BEAMS SHALL NOT BE PERMITTED. (USING BLOCKOUTS OR OTHER STRUCTURAL MEMBERS AS A SOURCE OF SUPPORT IS PROHIBITED. ROUTING CONDUIT UNDER BEAMS WILL NOT BE PERMITTED.) CONDUIT SHALL NOT BE ROUTED BEHIND SIGNS, OR LOCATED SUCH A WAY THAT IT OBSTRUCTS THE OPERATION OF A DEVICE OR RESTRICTS OPENING AN ACCESS POINT, ENCLOSURE DOOR OR ANY OTHER PIECE OF EQUIPMENT.
6. AT STAIRS AND ELEVATOR CLOSERS RUN CONDUITS BELOW SLAB AND FROM BOTTOM UP AND DO NOT CROSS EXPANSION JOINTS AT SUPPORTED TIERS UNLESS NOTED OTHERWISE. DO NOT ROUTE VERTICAL CONDUIT RISERS THROUGH EXPANSION JOINTS.
7. PROVIDE CONDUIT EXPANSION FITTINGS FOR ALL CONDUIT THAT CROSS EXPANSION JOINT AT LOCATIONS REQUIRED.
8. DO NOT ENCASE CONDUIT IN SLAB ON GRADE. INSTALL BELOW SLAB ON GRADE AND GRADE BEAMS. AVOID BELOW GRADE CONDUIT.
9. THE USE OF POWDER PROPELLED FASTENERS FOR MOUNTING CONDUIT, SUPPORTS, JUNCTION BOXES, FIXTURES OR OTHER EQUIPMENT IS PROHIBITED.
10. PROVIDE APPROVED, HOT DIPPED GALVANIZED STEEL GUARDS AROUND JUNCTION BOXES, CONDUITS AND OTHER EQUIPMENT WHICH MAY BE EXPOSED TO POSSIBLE BUMPER DAMAGE. SEE CONDUIT RISER GUARD DETAIL 2E-501.
11. LIGHTING DESIGN TO BE AS SHOWN ON DRAWINGS BUT IN NO CASE LESS THAN IESNA RP-20-14, AND IESNA-01-03, "LIGHTING FOR PARKING FACILITIES" AND ALL OTHER LOCAL CODE REQUIREMENTS.
12. ALL RECEPTACLES SHALL BE GFCI TYPE. ALL RECEPTACLES EXCEPT THOSE IN ENCLOSED ROOMS SHALL HAVE WEATHERPROOF COVERS.
13. CONDUITS ARE TO BE LOCATED IN THE GARAGE INTERIOR TO MINIMIZE VISUAL IMPACT. DO NOT ROUTE CONDUITS ON THE EXTERIOR OF ANY PART OF THE STRUCTURE. FIXTURES LOCATED ON THE EXTERIOR OF THE STRUCTURE ARE TO BE FED FROM BEHIND WITH NO CONDUIT VISIBLE FROM THE OUTSIDE. FIXTURES LOCATED ON BRICK (INSIDE OR OUTSIDE THE STRUCTURE) ARE TO BE FED FROM BEHIND WITH NO CONDUIT VISIBLE.
14. IN LOCATIONS WHERE LIGHT FIXTURES, EXIT SIGNS, FIRE ALARM STROBES OR OTHER PIECES OF EQUIPMENT NEED TO BE MOUNTED TO A COLUMN OR WALL OVER PIPING OR OTHER OBSTACLES, PROVIDE EXTENSION BRACKETS MADE OUT OF 1/4" HOT DIPPED GALVANIZED STEEL PLATES AS REQUIRED.
15. THE LIGHTING LAYOUT SHOWN IS BASED ON A UNIFORM STRUCTURAL LAYOUT USING 12'-0" PRECAST DOUBLE TEES. IF A DIFFERENT STRUCTURAL ARRANGEMENT IS SELECTED, THE FIXTURE LOCATIONS CAN BE MOVED A MAXIMUM OF 2'-0" PARALLEL TO THE CENTERLINE OF THE BAY THE MINIMUM ALLOWABLE DISTANCE BETWEEN THE SIDE OF A FIXTURE AND THE SIDE OF A TEE STEM IS 1'-0". THE NUMBER OF FIXTURES IN AN AREA MUST REMAIN THE SAME. FIXTURES MUST BE LOCATED TO PROVIDE MAXIMUM UNIFORMITY AND MAXIMUM BENEFIT OF UPLIGHT MOUNT FIXTURES TO ONE SIDE IF LOCATED AT PRECAST JOINT.
16. THE F1, F3, F3A FIXTURES SHALL BE EQUIPPED WITH INDIVIDUAL OCCUPANCY/PHOTO CONTROL SENSORS CONTROLLING DIMMING (100% HIGH/50% LOW) POWER SUPPLY. THE FIXTURES CONNECTED TO NL CIRCUITS SHALL BE PROGRAMMED TO SWITCH FROM HIGH MODE TO LOW MODE 15 MINUTES AFTER MOVEMENT IS NO LONGER DETECTED. THE FIXTURES CONNECTED TO NORMAL POWER CIRCUITS SHALL BE PROGRAMMED TO SWITCH FROM HIGH TO LOW MODE 5 MINUTES AFTER MOVEMENT IS NO LONGER DETECTED. UPON DETECTING MOVEMENT, FIXTURES ARE TO IMMEDIATELY SWITCH TO HIGH MODE. THE FIXTURES SHALL TURN OFF COMPLETELY OVERRIDING ALL OCCUPANCY SENSOR CONTROL WHEN THE LIGHT LEVEL INCREASES TO 20 FOOT CANDLES AND BACK ON (OCCUPANCY SENSOR CONTROLLING FIXTURES) WHEN THE LIGHT LEVEL IS LESS THAN 10 FOOT CANDLES.
17. THE F2 FIFTH TIER LIGHT FIXTURES SHALL BE EQUIPPED WITH DUAL LEVEL (100% HIGH - 50% LOW) POWER SUPPLIES PROVIDING HIGH AND LOW LIGHT LEVELS. THESE POWER SUPPLIES SHALL BE CONTROLLED BY A CENTRALIZED LIGHTING CONTROL SYSTEM THAT RECEIVES CONTROL SIGNALS FROM THE OCCUPANCY SENSORS LOCATED AT FIFTH TIER AS DETAILED ON SHEET E-602. ALL FIFTH TIER F2 FIXTURES SHALL BE CONTROLLED AS A GROUP AND GO FROM LOW TO HIGH AND BACK TO LOW AT THE SAME TIME.
18. THE F4 FIXTURES SHALL BE EQUIPPED WITH INDIVIDUAL OCCUPANCY/PHOTO CONTROL SENSORS CONTROLLING DIMMING (100% HIGH/50% LOW) POWER SUPPLY. THE FIXTURES SHALL BE PROGRAMMED TO SWITCH FROM HIGH TO LOW MODE 5 MINUTES AFTER MOVEMENT IS NO LONGER DETECTED. UPON DETECTING MOVEMENT, FIXTURES ARE TO IMMEDIATELY SWITCH TO HIGH MODE. THE FIXTURES SHALL TURN OFF COMPLETELY OVERRIDING ALL OCCUPANCY SENSOR CONTROL WHEN THE LIGHT LEVEL INCREASES TO 30 FOOT CANDLES AND BACK ON (OCCUPANCY SENSOR CONTROLLING FIXTURES) WHEN THE LIGHT LEVEL IS LESS THAN 20 FOOT CANDLES.

LIGHT FIXTURE SCHEDULE

Table with columns: TYPE, MOUNTING, LAMP, MANUFACTURER, CATALOG NUMBER, REMARKS. Rows include F1 (Trunnion Mount), F2 (Pole Mount), F3 (Wall Arm Mount), F3A (Wall Arm Mount), F4 (Ceiling Surface Mount), F5 (Wall Mount), F5A (Ceiling Mount), F6 (Ceiling Surface Mount), F6A (Ceiling Surface Mount), F7 (Ceiling Surface Mount), F7A (Ceiling Surface Mount), F8 (Wall Mount), F9 (Recessed Ceiling Mount).

ELECTRICAL SYMBOLS



ELECTRICAL ABBREVIATIONS

- A OR AMP AMPERE
AFF ABOVE FINISHED FLOOR
ARCH ARCHITECT (ARCHITECTURAL)
BB BACKBOARD
CIRK CIRCUIT
C CONDUIT
CIP CAST IN PLACE
COMM COMMUNICATIONS
CONC CONCRETE
CWA CONSTANT WATTAGE AUTO-TRANSFORMER
DET DETAIL
DIA DIAMETER
EJ EXPANSION JOINT
GF GROUND FAULT
GFI CIRCUIT INTERRUPTER
GRD GROUND
GT, T2 TIER DESIGNATION: GROUND TIER, 2ND TIER, ETC.
JB JUNCTION BOX
HP HORSEPOWER
HZ HERTZ
HX-HPF HIGH REACTANCE HIGH POWER FACTOR BALLAST
INCAND INCANDESCENT
KVA KILOVOLT-AMPERE
KW KILOWATT
KARCS PARKING ACCESS & REVENUE CONTROL SYSTEM
PC PHOTOELECTRIC CONTROL
PH PHASE
PNL PANEL
RECEPT RECEPTACLE
RC RIGID METAL CONDUIT
SHT SHEET
SIM SIMILAR
SOG SLAB ON GRADE
SP STATIC PRESSURE
STRUCT STRUCTURAL
SW SWITCH
TFA TO FLOOR ABOVE
TFB TO FLOOR BELOW
TRANS TRANSFORMER
TYP TYPICAL
UL UNDERWRITER'S LABORATORIES
UNO UNLESS NOTED OTHERWISE
V VOLT
W WATT
WP WEATHERPROOF
W WITH

LIGHT FIXTURE SCHEDULE, GENERAL NOTES, SYMBOLS & ABBREVIATIONS

E-001

PROJECT:41152.00 DATE: 23 MAY 2016

Drawn By: TDL
 Checked By: KJT
 Approved By: KJT



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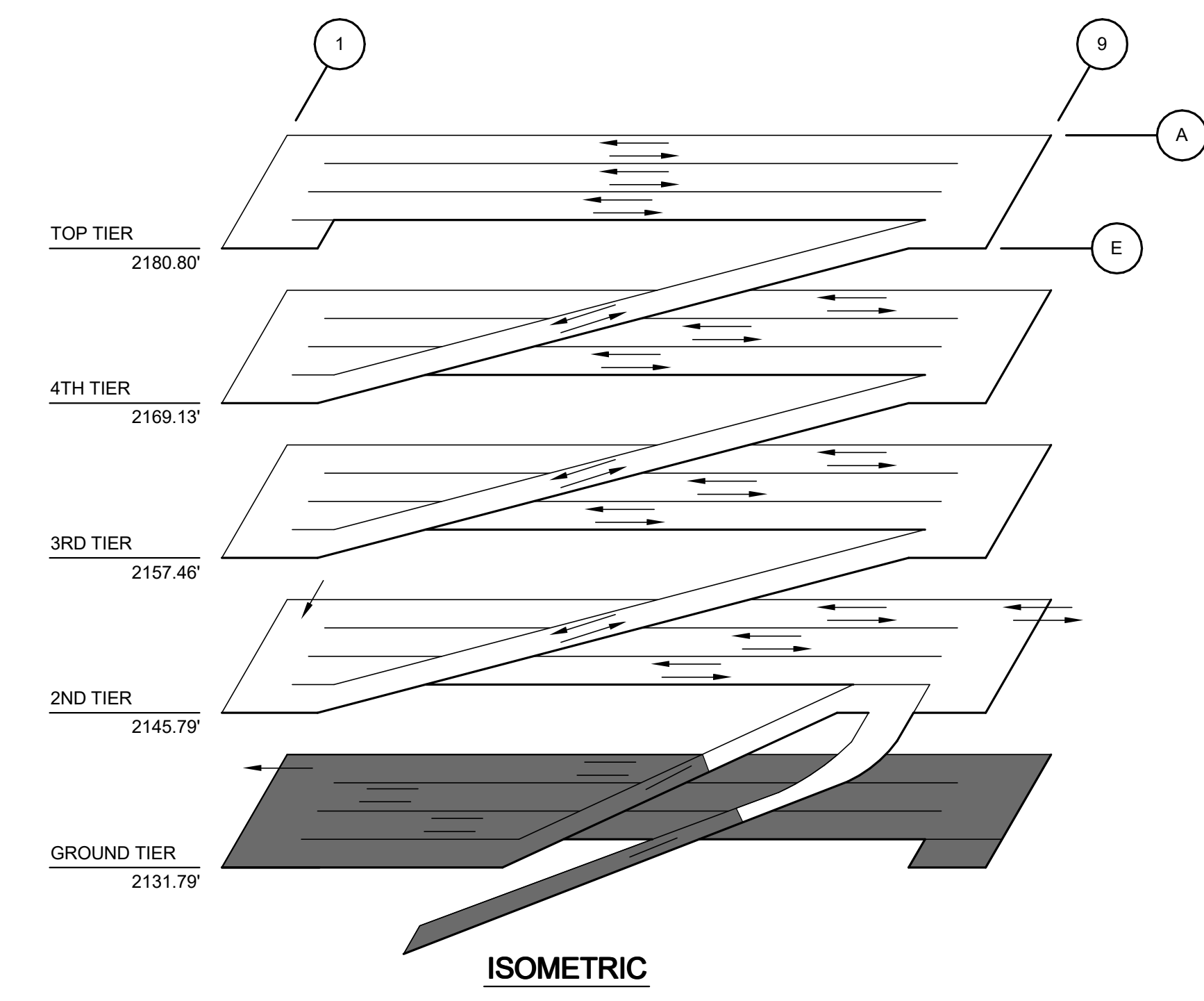
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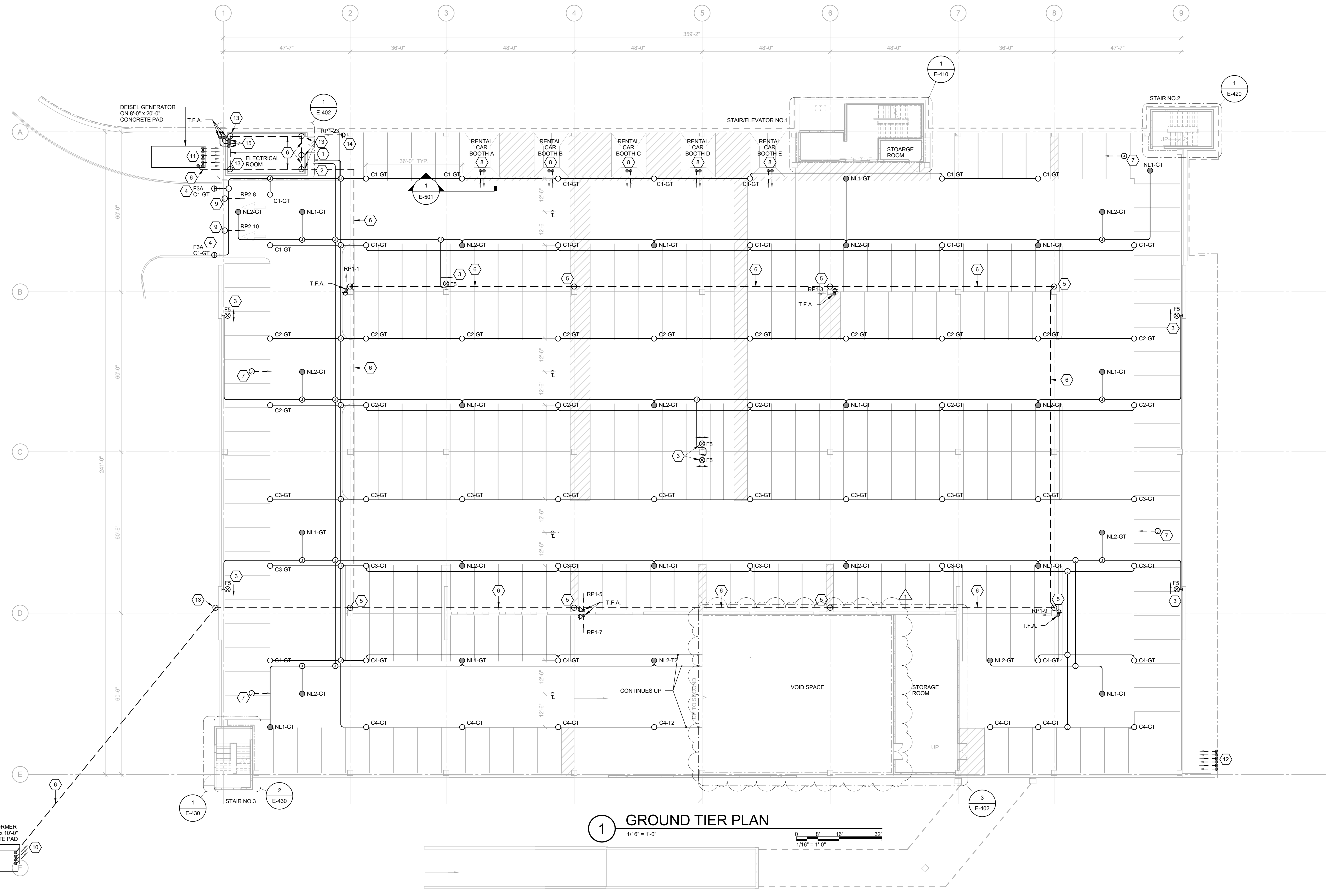


SHEET NOTES

- HOMERUNS TO PANEL LP1 IN THE ELECTRICAL ROOM.
- CONNECT EXIT SIGN TO NEAREST NL CIRCUIT.
- MOUNT F3 FIXTURES EVEN WITH THE FLOOR ABOVE. FIXTURES ARE TO BE FED THROUGH PRECAST WITH EMBEDDED CONDUIT. COORDINATE WITH PRECAST CONTRACTOR.
- DRIVE 10" x 3/4" DIAMETER COPPER CLAD GROUND RODS AT LOCATIONS SHOWN. CONNECT TO TOP TIER LIGHT POLES WITH #40 COPPER CABLE IN 1" P.V.C. CONDUIT. PROVIDE PVC EXPANSION JOINT AS REQUIRED BY NEC.
- CONNECT ALL LIGHT POLE GROUND RODS AND ELECTRICAL ROOM GROUNDING SYSTEM TOGETHER WITH BARE #40 COPPER CABLE LOCATED 1" UNDER SLAB ON GRADE. CONNECT TO CABLES FROM TOP LEVEL LIGHT.
- POLES HOMERUNS TO PANEL LP1 IN THE ELECTRICAL ROOM. CAMERA LOCATION. PROVIDE JUNCTION BOX AND ROUTE ONE 3/4" CONDUIT TO DATA RACK IN NEAREST COMMUNICATIONS ROOM ON SECOND TIER. SEE PLAN SHEETS FOR CLOSET COMMUNICATIONS ROOM. IF MULTIPLE CAMERA RUNS ARE COMBINED, CONDUITS WILL NEED TO BE UP SIZED PER WIRING MANUFACTURER STANDARDS. CONDUIT RUNS ARE TO UTILIZE THE CENTER BLOCK-OUTS IN THE PRECAST TEE STEMS AND NOT TO EXCEED 30" FROM CAMERA TO COMMUNICATIONS ROOM. CAMERA BY OTHERS. COORDINATE EXACT LOCATION AND DETAILS WITH SECURITY INSTALLER.
- CONDUIT STUD LOCATIONS FOR RENTAL CAR BOOTHS. ROUTE ONE 2" CONDUIT TO PANEL RB IN ELECTRICAL ROOM FOR POWER. ROUTE ONE 2" CONDUIT TO STORAGE ROOM LOCATED ON GROUND TIER AT STAIR/ELEVATOR NO. 1. COORDINATE ALL REQUIREMENTS WITH RENTAL CAR AGENCY AND SECURITY INSTALLER.
- EXTERIOR ILLUMINATED SIGN LOCATION. PROVIDE JUNCTION BOX AND ROUTE 3/4" CONDUIT TO PANEL RP2 FOR POWER. SEE SIGNAGE SHEETS FOR ADDITIONAL INFORMATION. COORDINATE WITH SIGN INSTALLER.
- THREE 4" CONDUITS CONDUITS FROM UTILITY TRANSFORMER TO ELECTRICAL ROOM FOR ELECTRICAL SERVICE. SEE ELECTRICAL ROOM PLAN ON E-402 AND POWER ONE LINE DIAGRAM ON E-603 FOR ADDITIONAL INFORMATION.
- SIX 4", FOUR 2", THREE 1" AND THREE 3/4" CONDUITS FROM GENERATOR TO ELECTRICAL ROOM. ONE 4" CONDUIT FROM GENERATOR TO COMMUNICATIONS ROOM LOCATED ON SECOND TIER AT STAIR/ELEVATOR NO. 1. SEE ELECTRICAL ROOM PLAN ON E-402, SECOND TIER STAIR/ELEVATOR NO. 1 PLAN ON E-410 AND POWER ONE LINE DIAGRAM ON E-603 FOR CONDUIT REFERENCE /SIZING AND ADDITIONAL INFORMATION.
- ROUTE FOUR 4" CONDUITS AND TWO 1" CONDUITS UNDER SLAB ON GRADE FROM GENERATOR TO NORTH SIDE OF DECK FOR GENERATOR POWER TO BE USED IN FUTURE EXPANSION. STU, 8" A.F.F. AND CAP NORTH OF GRID LINE 9.
- DRIVE 10" x 3/4" DIAMETER COPPER CLAD GROUND RODS AT LOCATIONS SHOWN. CONNECT TO GROUNDING GRID WITH #4 COPPER CABLE IN 1" P.V.C. CONDUIT. PROVIDE PVC EXPANSION JOINT AS REQUIRED BY NEC.
- HOT BOX RECEPTACLE. SEE MECHANICAL DRAWINGS FOR ADDITIONAL INFORMATION.
- THREE 4" CONDUITS FOR FUTURE SOLAR POWER. STUB AND CAP CONDUITS IN ELECTRICAL ROOM AT LOCATION SHOWN. CONDUITS ARE TO BE ROUTED VERTICALLY ON COLUMN INDICATED UP TO THE CEILING OF THE FOURTH TIER AND CAPPED FOR FUTURE USE.

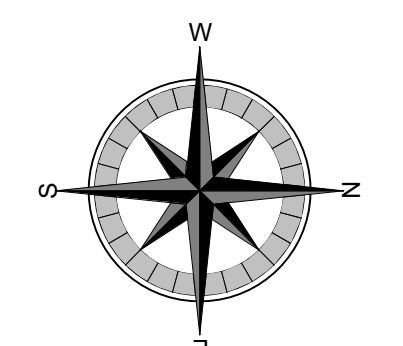


ISOMETRIC



1 GROUND TIER PLAN

1/16" = 1'-0" 0 6' 12' 30'



Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	06/21/17	ASI #07

GROUND TIER PLAN

E-101

PROJECT: 41152.00
 DATE: 23 MAY 2016

6/21/2017 11:15:28 AM

Drawn By: JDL
 Checked By: KJT
 Approved By: KJT

SHEET NOTES

- ROUTE #40 COPPER CABLE IN 1" CONDUIT FROM THE ELECTRICAL ROOM TO GROUNDING GRID SHOWN ON E-101. PANEL MDP TRANSFORMERS TR-1, STR-1 SHALL BE GROUNDED TO THIS GROUNDING GRID WITH #40 COPPER CABLE. IN ADDITION, THIS GROUNDING GRID SHALL BE CONNECTED TO REBAR IN FOUNDATION, METAL SERVICE PIPE AND ANY ADDITIONAL GROUNDS WITH #40 COPPER CABLE PER SECTION 250 OF THE N.E.C. CONNECT TO LIGHT POLE GROUND GRID. SEE SHEET E-101 FOR ADDITIONAL INFORMATION.
- EXHAUST FAN WITH BACK GUARD AND MOTOR OPERATED SHUTTER, WITH INSECT SCREEN, 950 CFM @ 0.125 S.P., 120V, 1 PHASE, 60HZ, MOUNT 6" BELOW BOTTOM OF CEILING. PENN VENTILATOR CO. MODEL NO. P-12R WITH NO. EB SHUTTER, CONTROLLED BY THERMOSTAT.
- INTAKE LOUVER MOTOR OPERATED WITH INSECT SCREEN, ALUMINUM 24" HIGH BY 36" WIDE, 120V, 1 PHASE, 60HZ, MOUNT 14" A.F.F. CONTROLLED BY THERMOSTAT. GREENHECK MODEL EAD-601.
- SQUARE D CATALOG NO. FG-1 MOTOR STARTER SWITCH.
- THERMOSTAT, LINE VOLTAGE, TWO STAGE, HONEYWELL CATALOG NO. T8052A1015. STAGE ONE SHALL OPEN MOTOR OPERATED LOUVER AND FAN SHUTTER AT 60° F. STAGE TWO SHALL START EXHAUST FAN AT 80° F.
- CONNECT F6A FIXTURES PER MANUFACTURERS INSTRUCTIONS SO IT IS SWITCHABLE BUT WILL AUTOMATICALLY TRANSFER TO BATTERY POWER UPON LOSS OF AC POWER.
- ELP-1 CHROMALOX UNIT HEATERS, 4.0 KW, 277V, SINGLE PHASE, WITH INTEGRAL THERMOSTAT/DISCONNECT SWITCH, CATALOG NO. LUH-04-71-35 MOUNT NEAR CEILING. SET THERMOSTAT AT 50° F.
- NOT USED.
- NOT USED.
- THREE 4" CONDUITS FROM FUSED DISCONNECT SWITCH TO UTILITY TRANSFORMER FOR NORMAL POWER FEED. TWO CONDUITS FOR SERVICE CONDUCTORS, ONE SPARE. SEE E-101 FOR CONTINUATION. SEE E-603 FOR ADDITIONAL INFORMATION.
- CONNECT 100 AMP EMERGENCY POWER SERVICE DISCONNECT SWITCH TO SERVICE SUPPLY SIDE OF ADJACENT 800 AMP NORMAL POWER SERVICE DISCONNECT SWITCH. SEE E-603 FOR ADDITIONAL INFORMATION.
- ONE 1" CONDUIT FROM PANEL LP1 FOR GENERATOR 480/277V POWER.
- ONE 1" CONDUIT FROM PANEL RP2 FOR GENERATOR 208/120V POWER.
- ONE 3/4" CONDUIT WITH 5-#10 TO GENERATOR FROM EMERGENCY ATS FOR REMOTE START SIGNAL.
- ONE 3/4" CONDUIT WITH 5-#10 TO GENERATOR FROM STANDBY ATS FOR REMOTE START SIGNAL.
- ONE 3/4" CONDUIT WITH 7-#10 TO GENERATOR FROM SHUNT TRIP AND EMERGENCY STOP SWITCHES.
- ONE 4" CONCRETE ENCASED CONDUIT FROM ELECTRICAL ROOM AND STUBBED UP AT GENERATOR FOR SPARE.
- ONE 1" CONDUIT FROM ELECTRICAL ROOM TO COMMUNICATIONS BACKBOARD IN COMMUNICATIONS ROOM LOCATED ON SECOND TIER AT STAIR/ELEVATOR NO.1 FOR ATSMISCELLANEOUS MONITORING.
- THREE 4" CONDUITS FROM DISCONNECT SWITCH TO GENERATOR FOR NORMAL POWER STANDBY ATS FEED. TWO CONDUITS FOR POWER, ONE SPARE. SEE E-101 FOR CONTINUATION. SEE E-603 FOR ADDITIONAL INFORMATION.
- TWO 2" FROM DISCONNECT SWITCH TO GENERATOR FOR NORMAL POWER EMERGENCY ATS FEED. ONE CONDUIT FOR POWER, ONE SPARE. SEE E-101 FOR CONTINUATION. SEE E-603 FOR ADDITIONAL INFORMATION.
- THREE 4" CONDUITS FOR FUTURE SOLAR POWER. STUB AND CAP CONDUITS IN ELECTRICAL ROOM AT LOCATION SHOWN. CONDUITS ARE TO BE ROUTED VERTICALLY ON COLUMN INDICATED UP TO THE CEILING OF THE FOURTH TIER AND CAPPED FOR FUTURE USE.



GRESHAM SMITH AND PARTNERS

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- Tallahassee FL
- Tampa FL

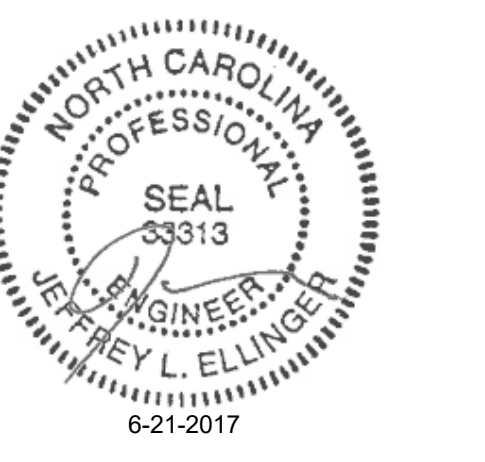
10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



Asheville Regional Airport Parking Garage

Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

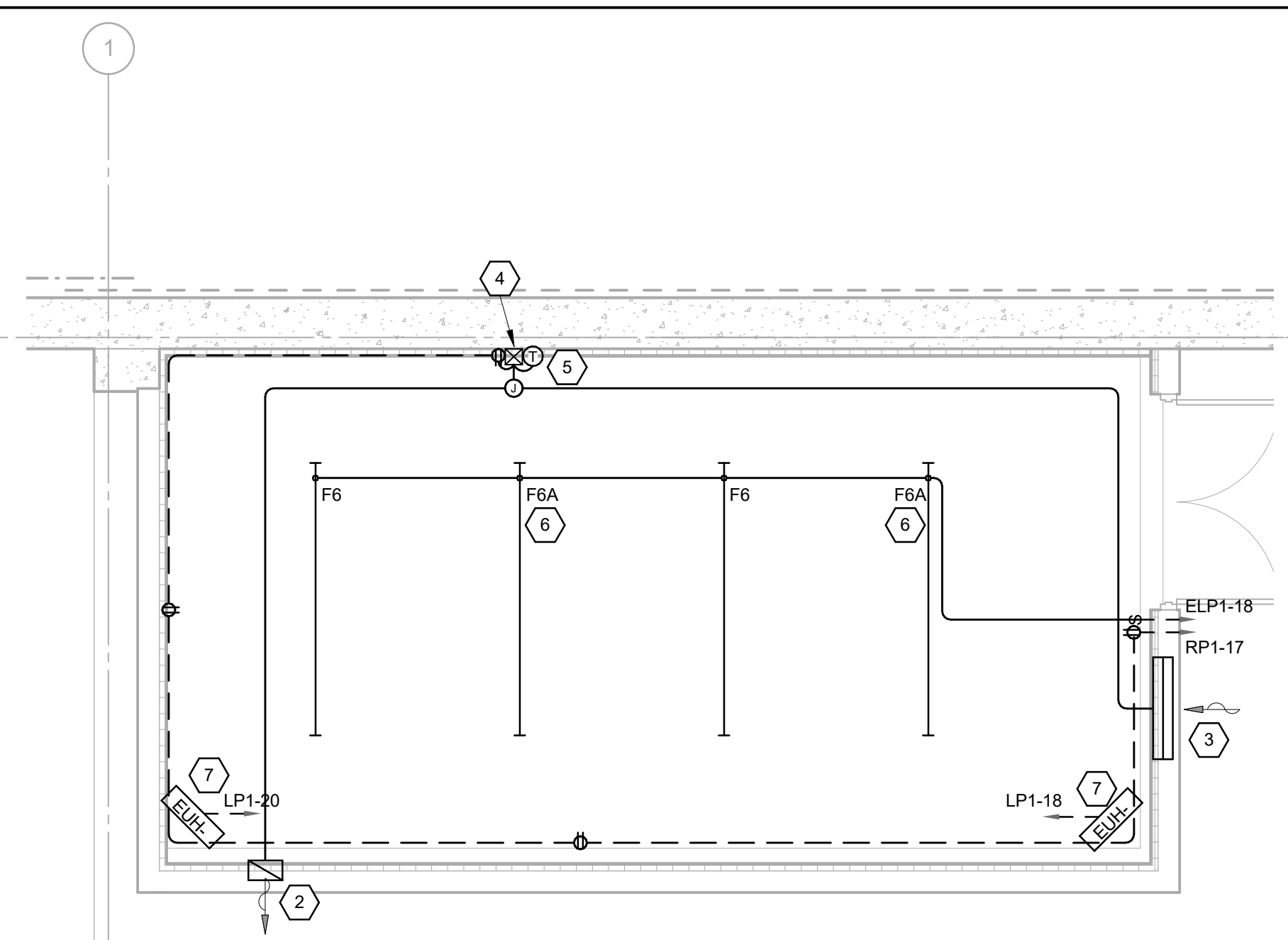


Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	03/16/17	ASI #03
2	06/21/17	ASI #07

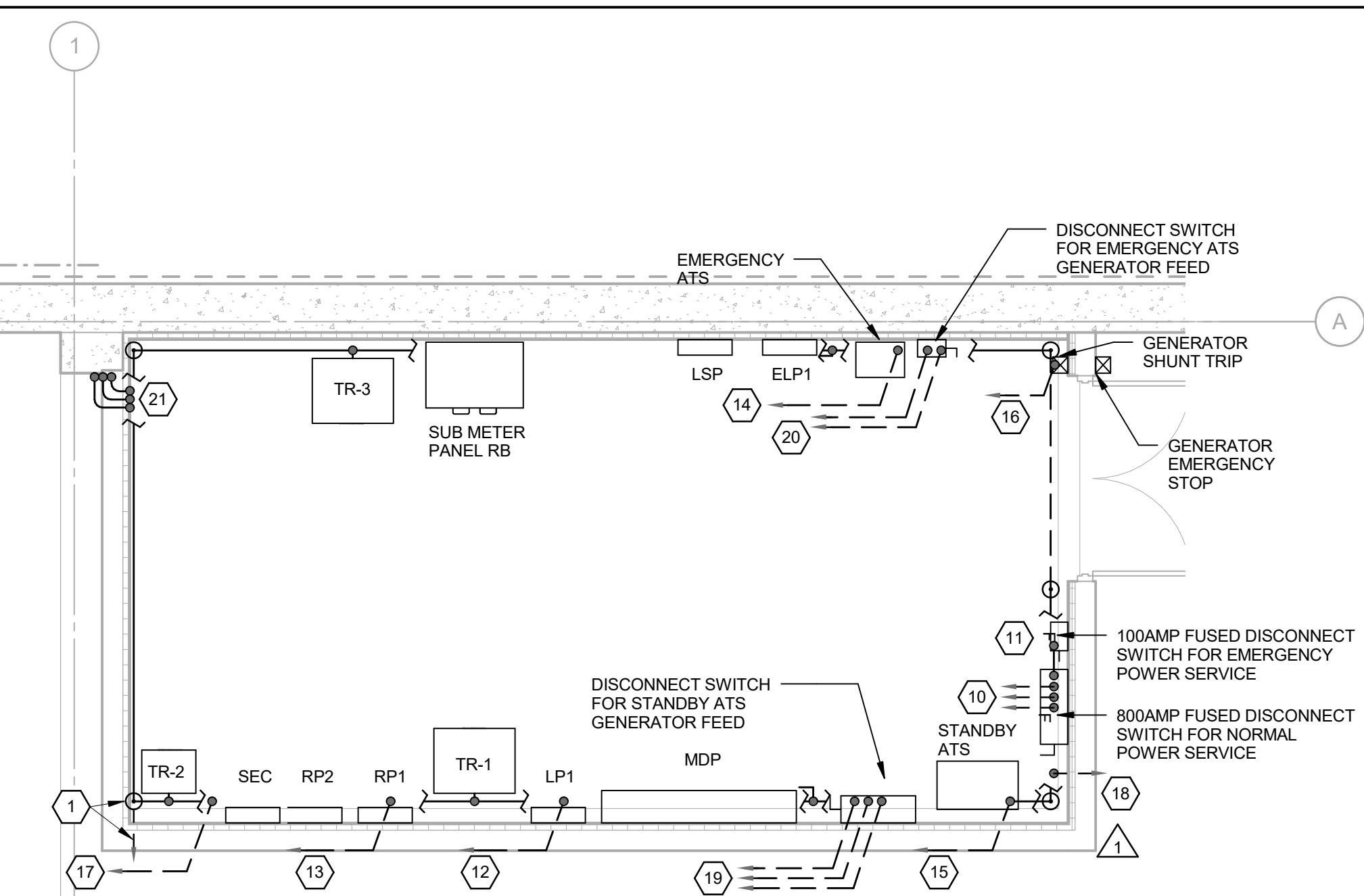
ENLARGED ROOM PLAN

E-402

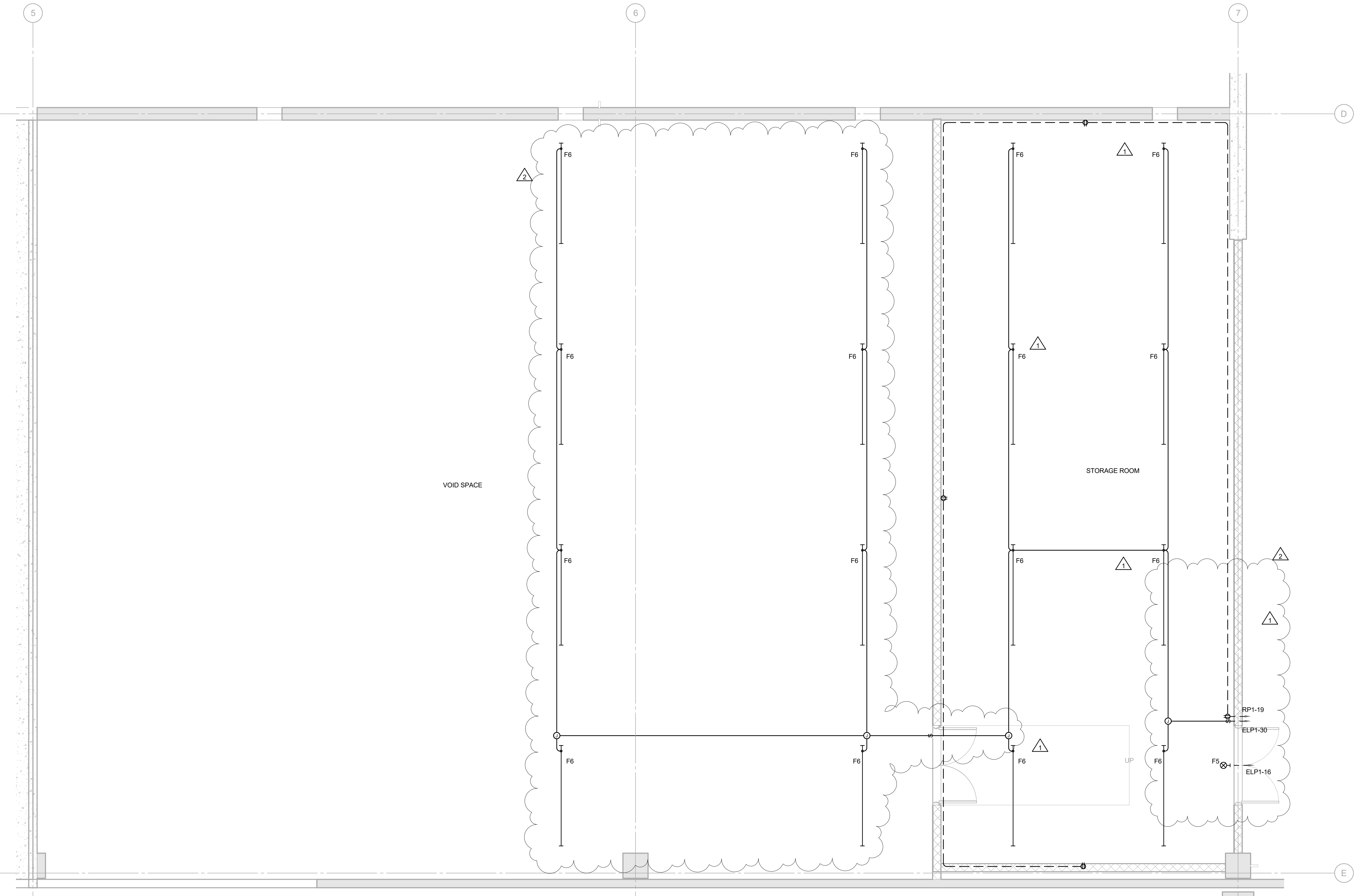
PROJECT: 41152.00
 DATE: 23 MAY 2016



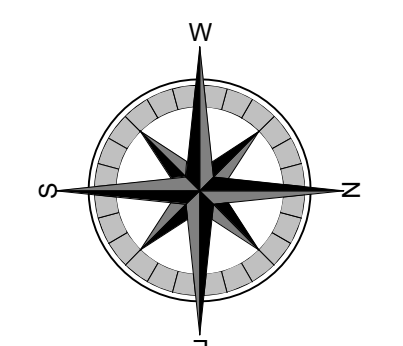
1
 ENLARGED ELECTRICAL ROOM PLAN
 1/4" = 1'-0"



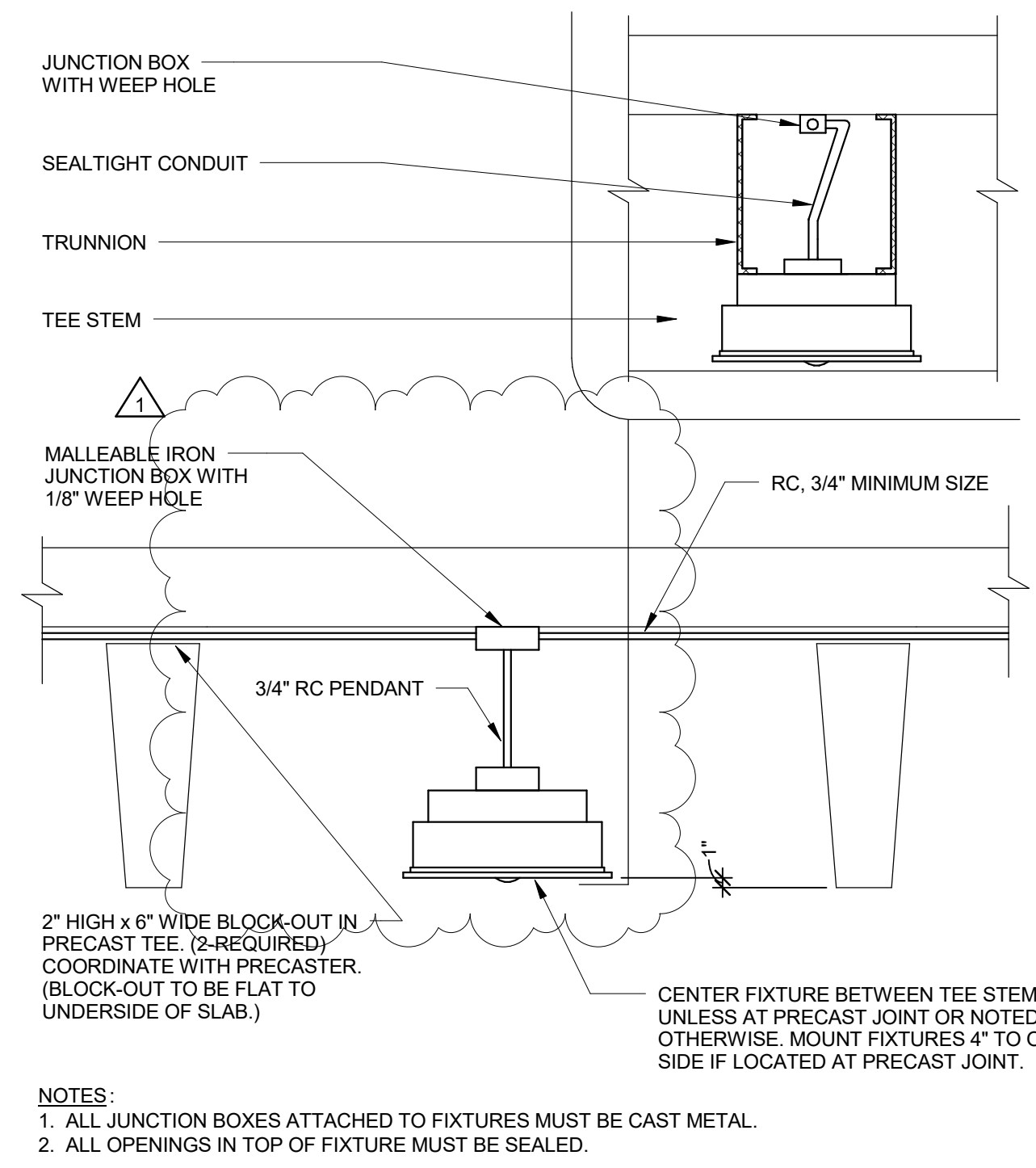
2
 ELECTRICAL ROOM PANEL ARRANGEMENT AND GROUNDING PLAN
 1/4" = 1'-0"



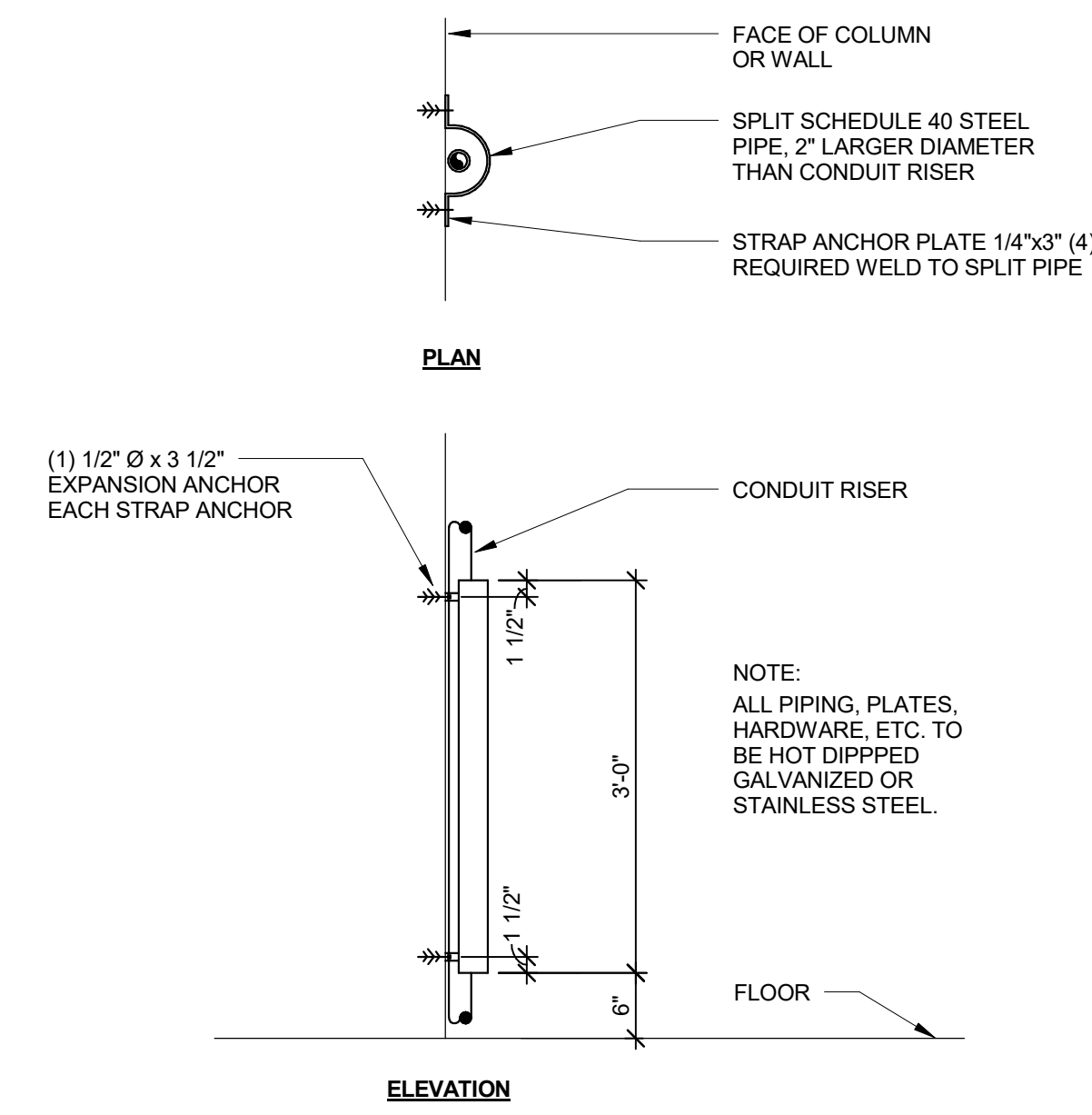
3
 ENLARGED STORAGE ROOM AND VOID SPACE PLAN
 1/4" = 1'-0"



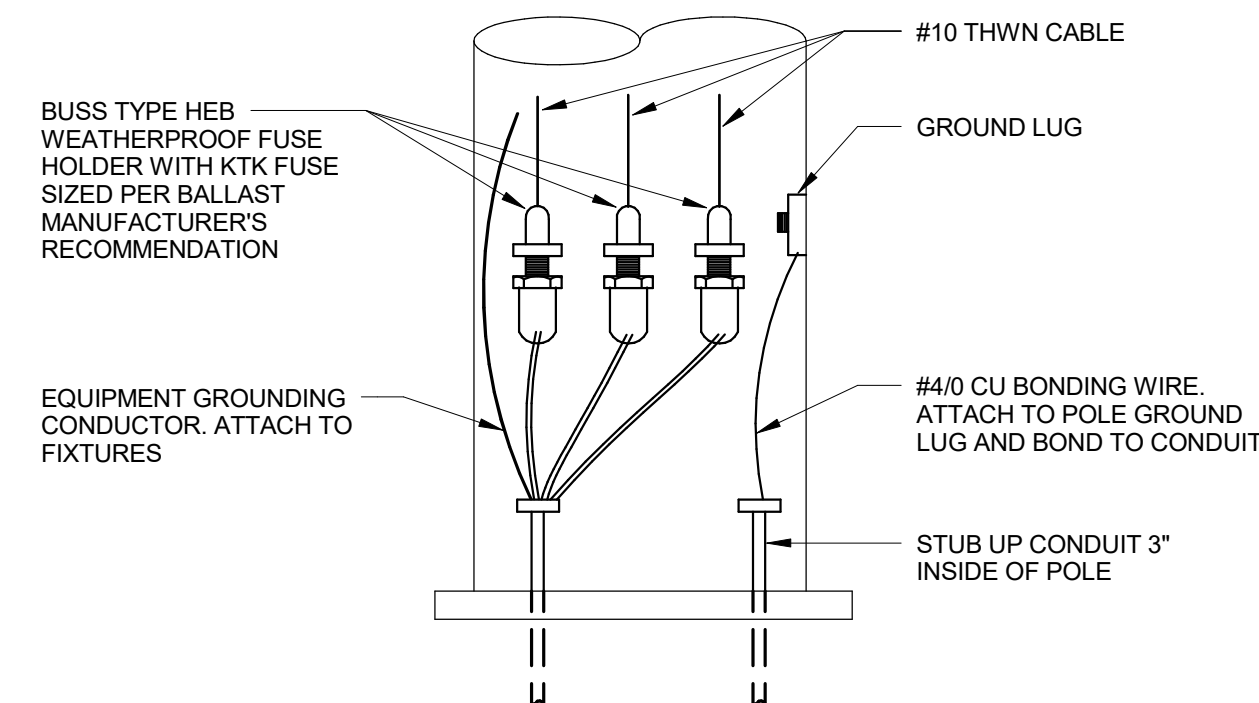
Drawn By: TDL
 Checked By: KCT
 Approved By: KCT



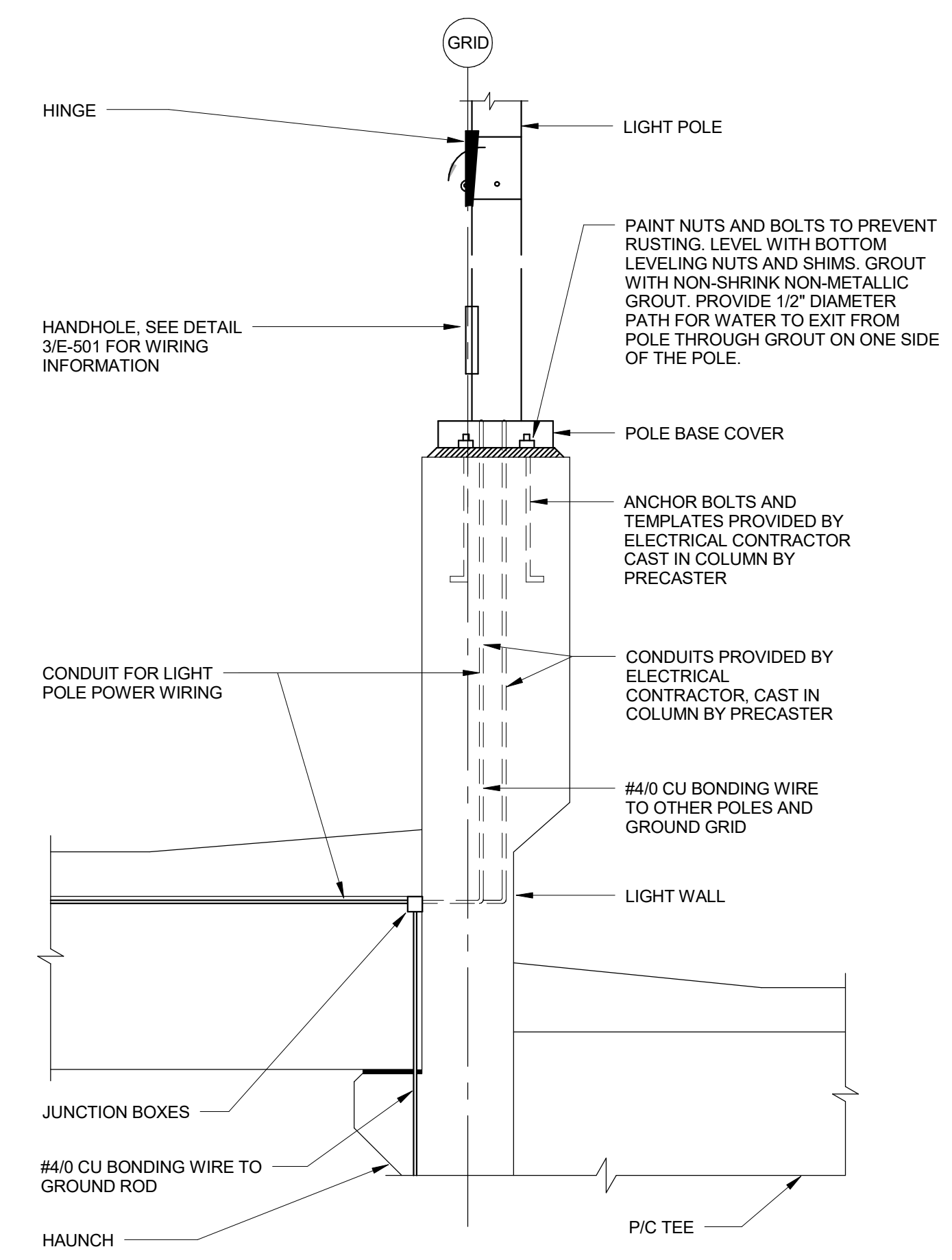
1 FIXTURE F1 MOUNTING DETAIL



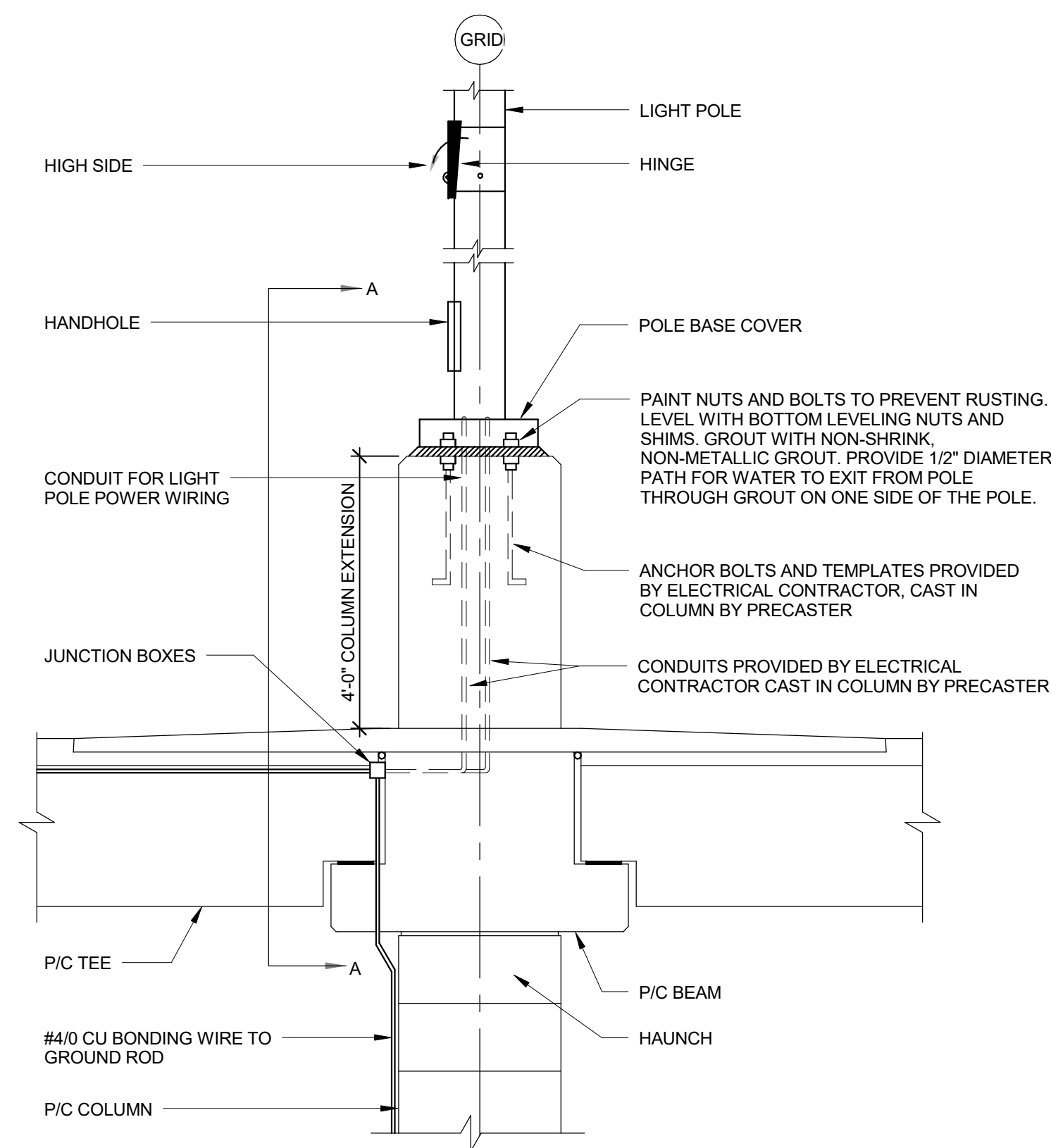
2 CONDUIT RISER GUARD DETAIL



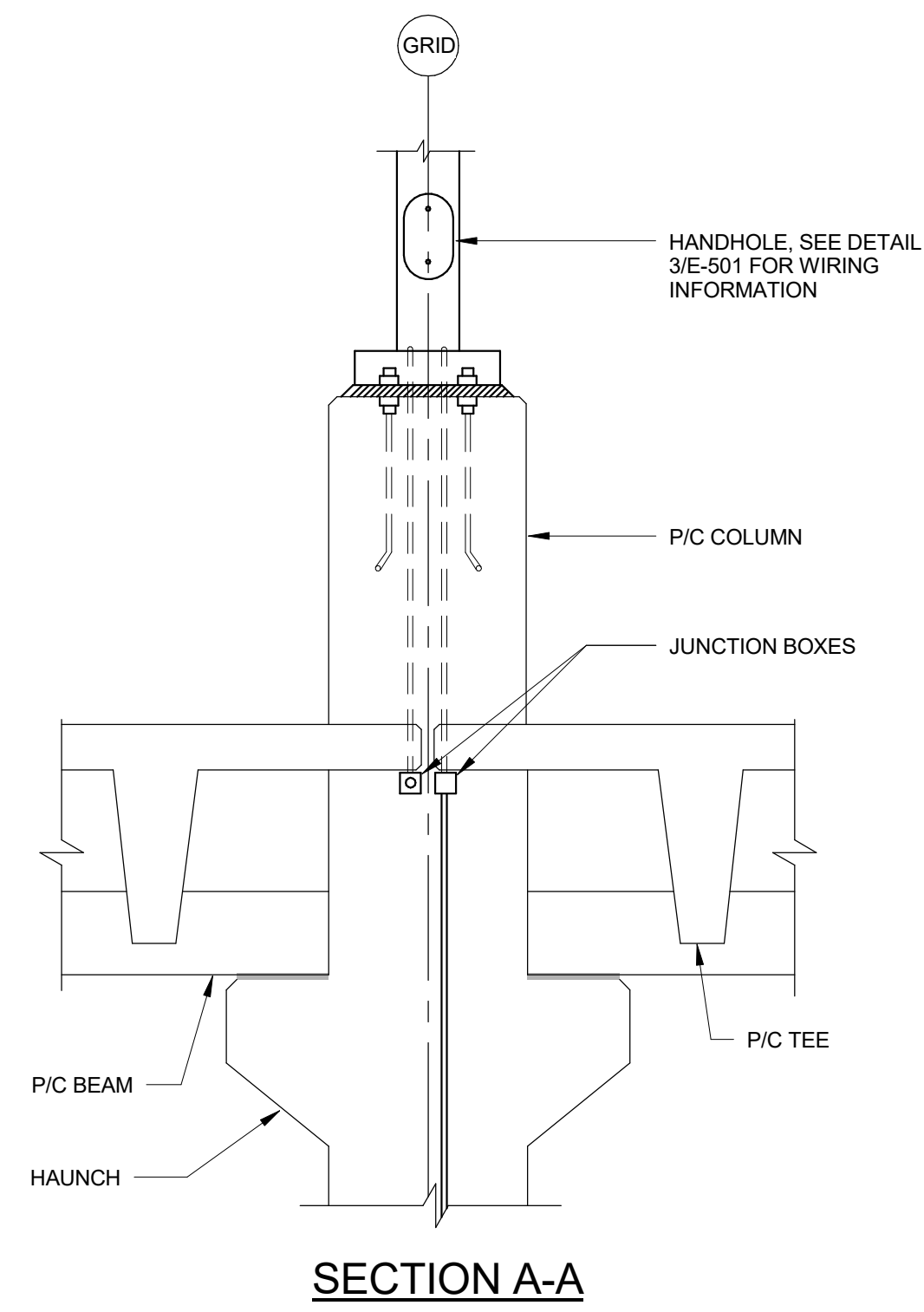
3 POLE WIRING DETAIL



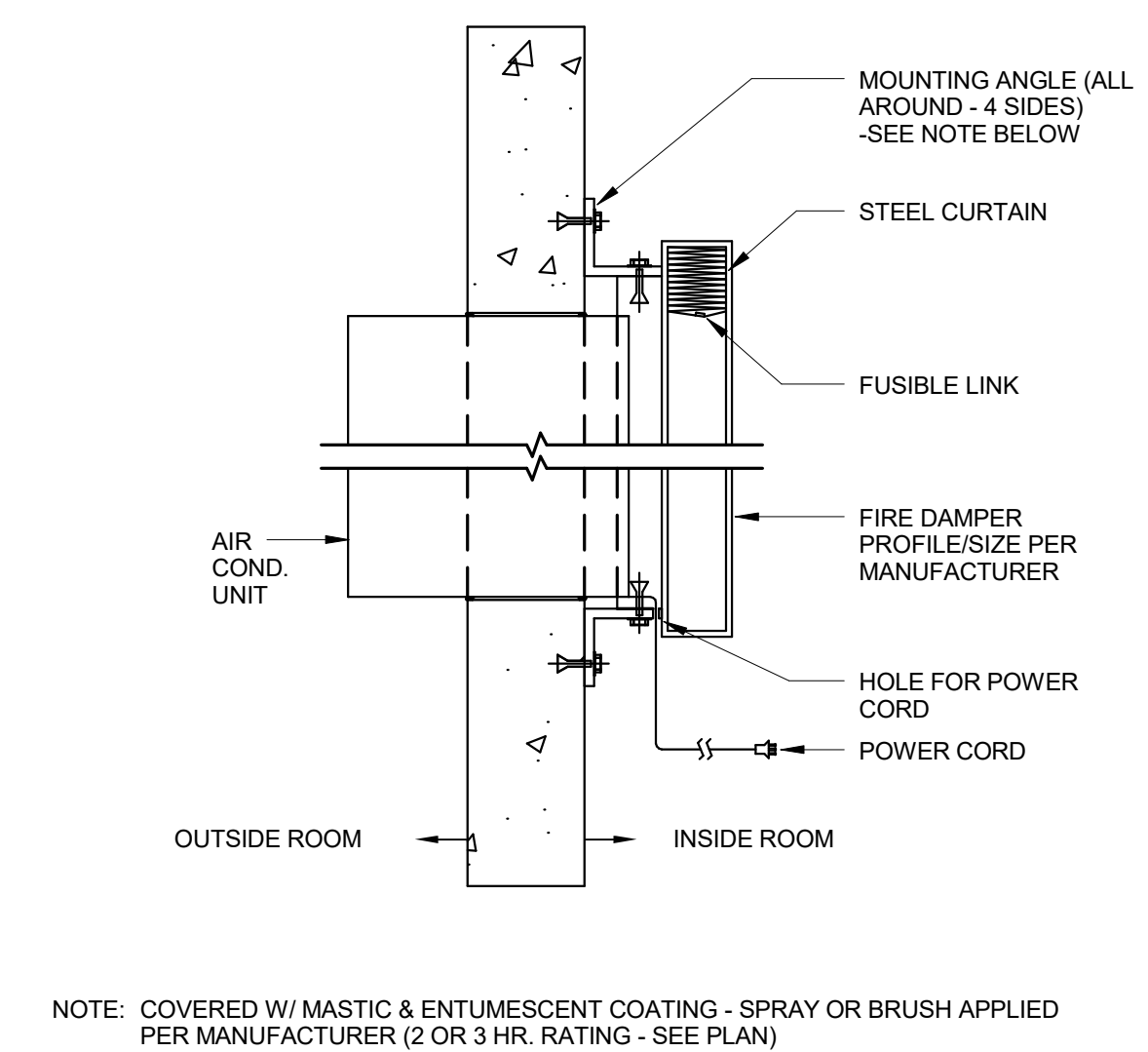
4 POLE MOUNTING DETAIL AT LIGHT WALL



5 POLE MOUNTING DETAIL AT COLUMN



6 A/C CURTAIN FIRE DAMPER DETAIL



NOTE: COVERED W/ MASTIC & ENTUMESCENT COATING - SPRAY OR BRUSH APPLIED PER MANUFACTURER (2 OR 3 HR. RATING - SEE PLAN)



GRESHAM SMITH AND PARTNERS
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 804.788.0710



Asheville Regional Airport
 Parking Garage

Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732



Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	06/21/17	ASI #07

DETAILS
E-501
 PROJECT: 41152.00
 DATE: 23 MAY 2016

Change Order Request # 26

BUILDING ON A GREAT TRADITION



20-Sep-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226

RE: Asheville Regional Airport Parking Garage Change Order Request #26

Dear Mr. Moody

This Change Order Request is for the additional work for the added reflectors, delineator posts, and signs requested at the exterior speed ramp per ASI-09.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
American South General Contractors	1	LS	1,358.41	1,358.41
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			1,358.41	1,358.41
OH&P			10.00%	135.84
SUBTOTAL				<hr/> 1,494.25
BOND			1.0%	14.94
TOTAL CHANGE ORDER PROPOSAL				<hr/> 1,509.19

We have included pages of ASI-09 of where this work shall take place along with the appropriate backup documentation.

Upon approval, please issue a Change Order for the total lump sum amount of **\$1,509.19**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: General Contractor
Contractor: American South GC

Proposal #: _____
Project # 09-163
Date: 8.31.17

Description of Change: Add twenty three (23) Bolt on Amber Guardrail Reflectors, five (5) Delineator Posts, and four (4) Bolt on Chevron Traffic Signs at the exterior Rental Car Ramp. Includes all labor and materials.

Material:		Subtotals
	1. Total Cost of Materials (Attach Backup)	\$865.80
	2. Sales Tax 7%	\$60.61
	3. Overhead & Profit (In overall COR)	\$0.00
		\$926.41
Labor:		
	1 Day for a 2 Man Crew for All @ \$20/Hr Each	
	4. Total Man Hour Cost (Attach Backup) 16 Man Hr	\$320.00
	5. Payroll Burden (Enter Percentage) 35%	\$112.00
	6. Overhead & Profit (In overall COR) 0%	\$0.00
		\$432.00
Equipment:		
	7. Equipment / Equipment Rental (Attach Backup)	
	8. Overhead & Profit 10%	\$0.00
		\$0.00
Unit Prices:		
	9. Unit Price (If Applicable)	\$0.00
Subcontractors:		
	10. Subcontractor (Attach Subcontractor Backup)	
	11. Overhead & Profit 10%	\$0.00
		\$0.00
		Subtotal of Proposal \$1,358.41
	12. Payment & Performance Bond (In Overall COR)	\$0.00
	(Enter Bond Rate Percentage)	
	Total of Change Order Proposal	\$1,358.41

Time Extension Request: 0 Days
Schedule Activity # Affected: N/A Does this effect the critical path? No

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: American South (Seth Chambers) Date: 9.20.17

Chevrons 95.95 (4 Totl)
Chevron Bolts 30.38 (31 Tot.)
Delineators 578.60 (5 Tot.)
Reflectors 160.87 (23 Tot.)

2 Man Crew

Chevrons	2 M/Hr
Reflectors	5 M/Hr
Posts	9 M/Hr
Total	16 Man Hours

Sales Quotation

Quote Number: 5392 Rev. 0
Description: ASHEVILLE AIRPORT ADDITONAL
Quote Date: 8/31/2017
Expires On: 9/30/2017
Ship Via:




Salesperson: MARKO VOJCANIN
Email: MVOJCANIN@IMPACTRECOVERY.COM
FOB:
Terms:
Currency: US Dollar

Contact:

Contact E-mail:

Bill To:
 AMERICAN SOUTH GENERAL CONTRACTORS
 60 N MERRIMON AVE SUITE 101
 ASHEVILLE, NC 28804

Ship To:
 AMERICAN SOUTH GENERAL CONTRACTORS
 60 N MERRIMON AVE
 ASHEVILLE, NC 28804

#	Part No. - Description	Qty	Unit Price	Tax	Total
1	 TP3-36YS-DY-DY-Q - 3" TUFF POST, 36", YELLOW, SHORT SQUEEZE, DG YELLOW, DG YELLOW, QUICK RELEASE TWO YELLOW DG BANDS	5	64.00	0.00	320.00
2	 BS-SMQB - SURFACE MOUNT BASE, QUICK RELEASE, BLACK	6	15.00	0.00	90.00
3	 IM-EPX2 - EPOXY KIT, 2 GALLON INSTALLS APPROX 15 SURFACE MOUNT BASES OR 5 XLP	1	90.00	0.00	90.00

ALL PRICES ARE FOB, SAN ANTONIO, TX | IMAGES ARE FOR REFERENCE ONLY
 STANDARD LEAD TIME FOR SHIPPING ALL ORDERS IS TWO(2) WEEKS ARO.

*All Prices Shown in US Dollar

SEE TERMS AND CONDITIONS ON REVERSE SIDE

The undersigned Buyer accepts the foregoing offer solely on the Terms and Conditions on the reverse side which shall be the prevailing terms and conditions. IRS shall disregard any additional or different terms and conditions proposed by Buyer in any confirmatory documents issued by Buyer.

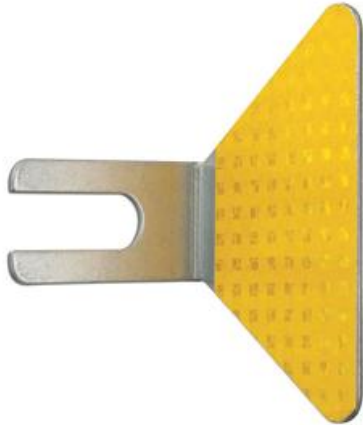
Name: _____

Title: _____

PO No.: _____

Signature: _____ Date: _____

Total Price:	500.00
Freight:	78.60
Total Tax:	0.00
Grand Total:	578.60



Guardrail Reflector: 1 Reflective Sides, 5 in Overall Ht, 2 1/4 in Overall Wd, Amber, Galvanized Steel, Bolt-On

Item # 578Z586

\$6.56 Each

\$6.56 Each x 23 Pieces
Subtotal = \$150.88 Est.
Shipping = \$9.99
Tax = N/A

Total = \$160.87

Product Specs

Number of Reflective Sides:	1
Overall Height:	5 in
Overall Width:	2 1/4 in
Reflector Color:	Amber
Housing Material:	Galvanized Steel
Mount Type:	Bolt-On
Application:	Roadside Guardrails
Features:	High Intensity Sheeting
Indoor or Outdoor Use:	Outdoor
Mounting Hardware Included:	No
Mounting Hardware Required:	Yes
Number of Mount Holes:	1
Permanence:	Removable
Reflector Material:	Acrylic
Shape:	Triangle
Thickness:	0.038462 in

Compliance and Restrictions

Continue Shopping

Free Standard Shipping on orders \$99 or more (before tax)

12" x 18"

My Order (1 item)



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Each

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4

Remove

Enter Promo Code

Subtotal:	\$90.96
Shipping:	\$4.99
Estimate Tax	\$6.72
Estimated Total:	\$102.67

Proceed to Checkout

SHOP ALL LABOR DAY SAVINGS + FREE PARCEL SHIPPING WITH MYLOWE'S. SHOP NOW>



Open until 10PM!
Weaverville Lowe's ▼

Prices, promotions, styles, and availability may vary. Our local stores do not honor online pricing. Prices and availability of products and services are subject to change without notice. Errors will be corrected where discovered, and Lowe's reserves the right to revoke any stated offer and to correct any errors, inaccuracies or omissions including after an order has been submitted.

Shopping Cart



Your purchase is always

Lowe's Of Weaverville, NC | [Change Store >](#)

24 Northridge Commons Parkway, Weaverville, NC 28787



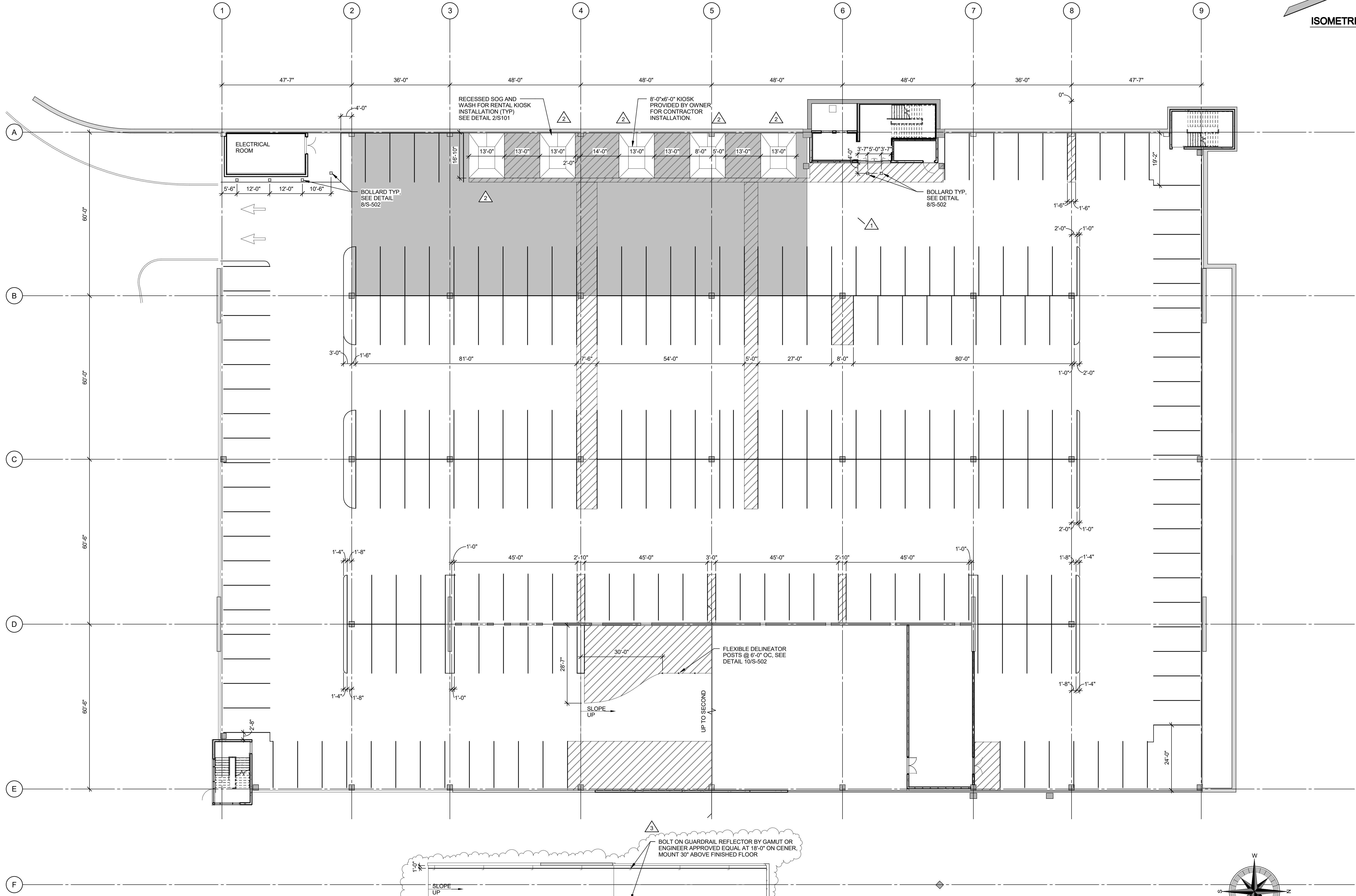
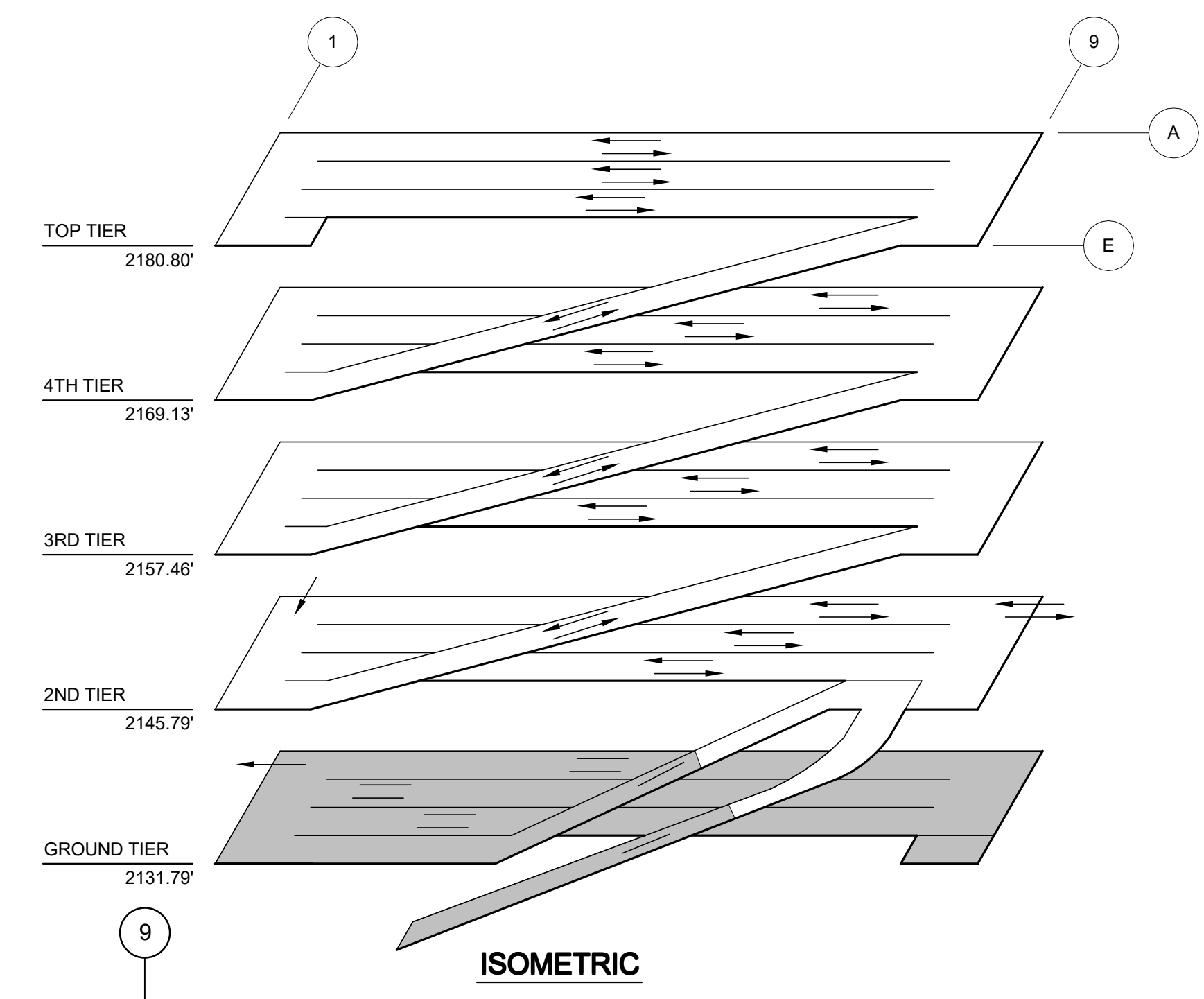
Print

Products in Cart	Select a Delivery Method	Quantity	Unit Price	Total
 <p>Red Head Tapcon Trubolt 2.25-in x 0.25-in Zinc-Plated Carbon Steel Heavy-Duty Wedge Anchor Item #:63214 Model #:52090</p>	<p>Store Pickup Your item is available for pickup today.</p>	<input type="text" value="31"/>	\$0.98	\$30.38
	<p>Lowe's Truck Delivery You'll be contacted within 24 hours to arrange your delivery.</p>			
	<p>Parcel Shipping Sent by carriers like UPS, FedEx, USPS, etc.</p>			
 <p>Bosch 1/4-in x 6-in -in Round Hammer Drill Bits 5-Count Item #:493414 Model #:LBH0045</p>	<p>Store Pickup Your item is available for pickup today.</p>	<input type="text" value="2"/>	\$13.98	\$27.96
	<p>Lowe's Truck Delivery You'll be contacted within 24 hours to arrange your delivery.</p>			
	<p>Parcel Shipping Sent by carriers like UPS, FedEx, USPS, etc.</p>			

Cart Summary

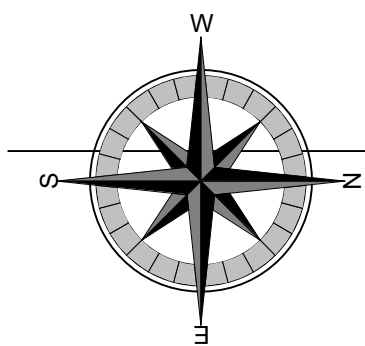
Drawn By: Author
 Checked By: Checker
 Approved By: Approver

CAR COUNT						
9'-0" 90° STANDARD SPACE						
TIER	STANDARD	RENTAL	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL	EFFICIENCY (SQ FT / STALL)
GROUND	0	224	0	0	224	80.923
SECOND	236	0	4	4	244	86.773
THIRD	269	0	8	0	277	86.773
FOURTH	280	0	8	0	288	86.773
FIFTH	272	0	0	0	272	80.884
	1057	224	20	4	1305	422.126



- GENERAL SHEETS NOTES:
- FOR TYPICAL STRIPING DETAILS AND FLOOR ARROWS SEE SHEET AG102 & AG103.
 - ALL DIMENSIONS FOR STRIPING ARE FROM COLUMN CENTERLINE UNO.
 - FOR FLOOR ELEVATIONS SEE "S-100" SERIES DRAWINGS.
 - FOR ENLARGED STAIR / ELEVATOR PLANS & ELEVATIONS SEE "S-400" SERIES DRAWINGS.
 - FOR BUILDING ELEVATIONS SEE ARCH DRAWINGS.
 - ALL STALLS ARE 9'-0" UNLESS NOTED OTHERWISE.

1 GROUND TIER PLAN
 1/16" = 1'-0"



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 Jackson MS Tampa FL

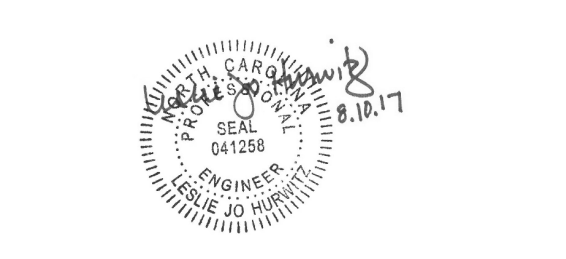
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Asheville Regional Airport Parking Garage

Greater Asheville Regional Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732



Revision		
No.	Date	Description
	9-21-16	CONFORMED CONTRACT DOCUMENTS
1	3-21-17	ASI #05
2	5-3-17	ASI #05 REV
3	8-10-17	ASI #08

GROUND TIER PLAN

AG101

PROJECT: 41152.00
 DATE: April 25, 2016

8/10/2017 3:16:53 PM

Drawn By: Author
 Checked By: Checker
 Approved By: Approver



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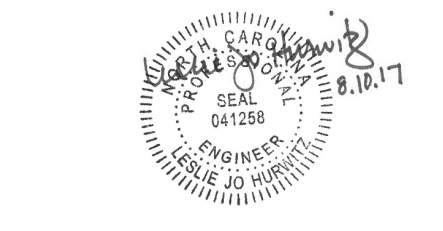
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 804.788.0710



Asheville Regional Airport Parking Garage

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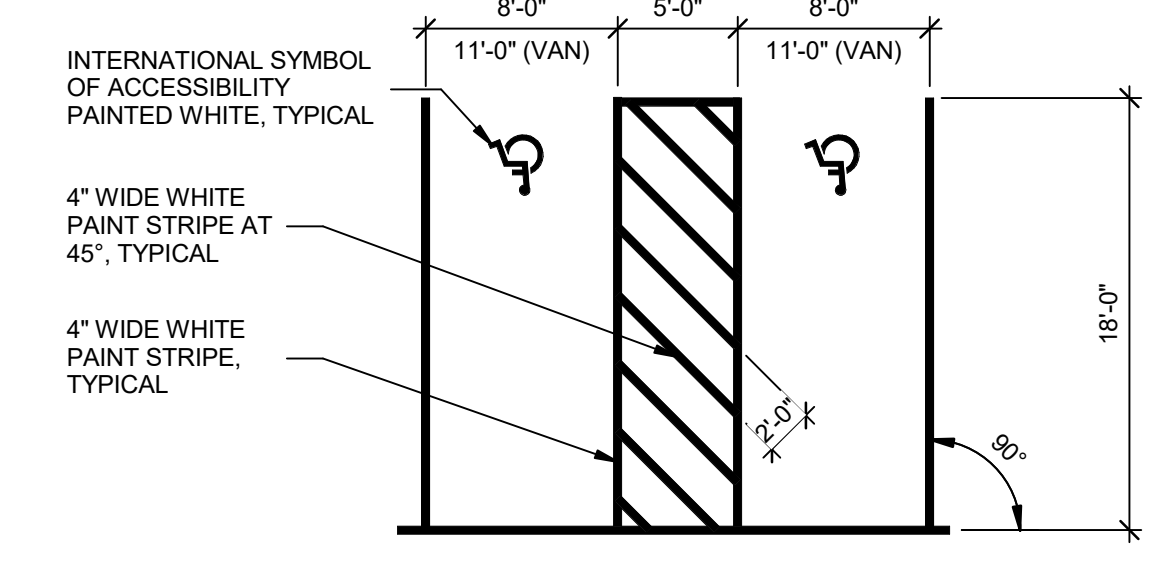


Revision		
No.	Date	Description
9-21-16		CONFORMED CONTRACT DOCUMENTS
1	8-10-17	ASI #08

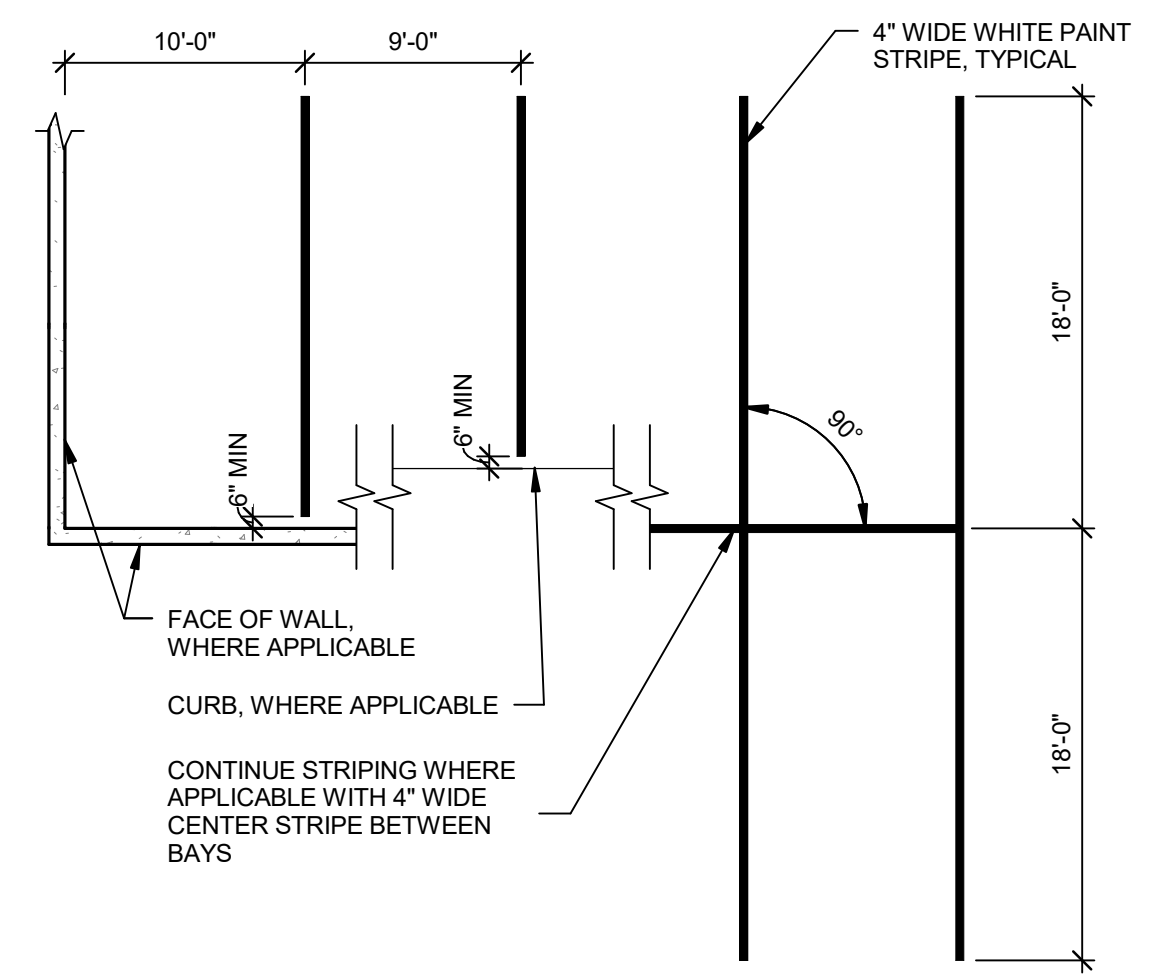
SECOND TIER PLAN

AG102

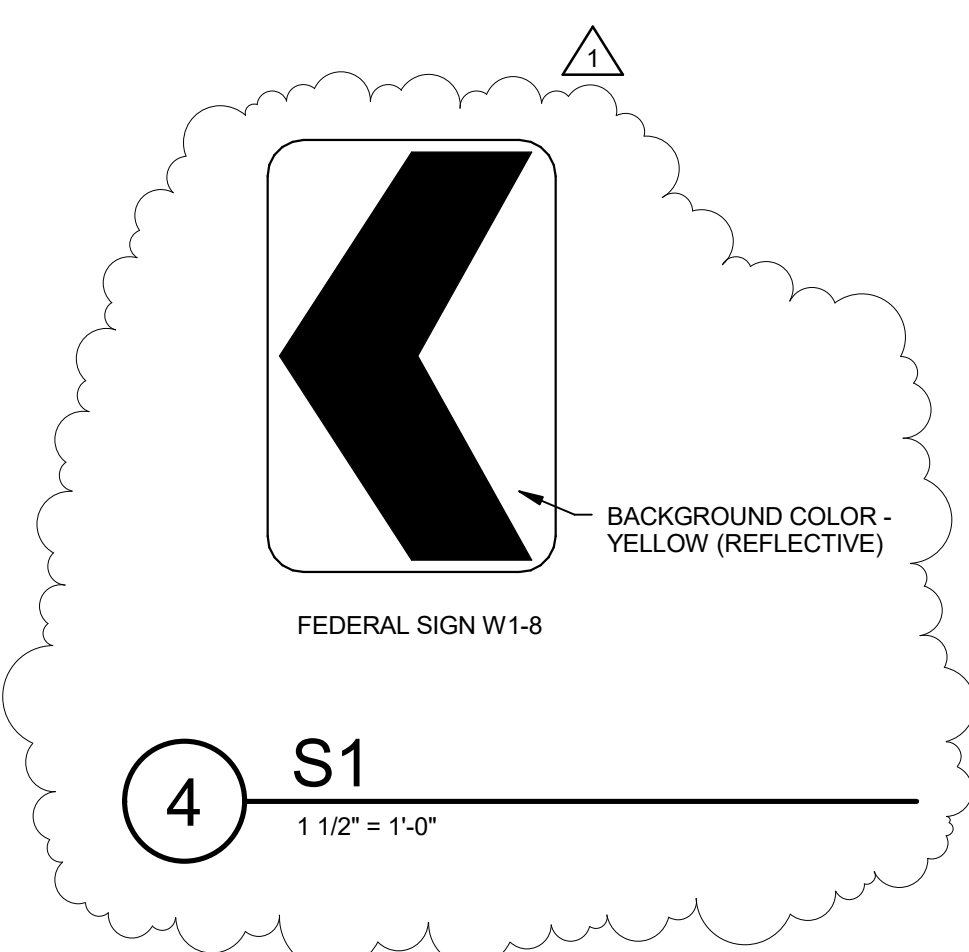
PROJECT: 41152.00
 DATE: April 25, 2016



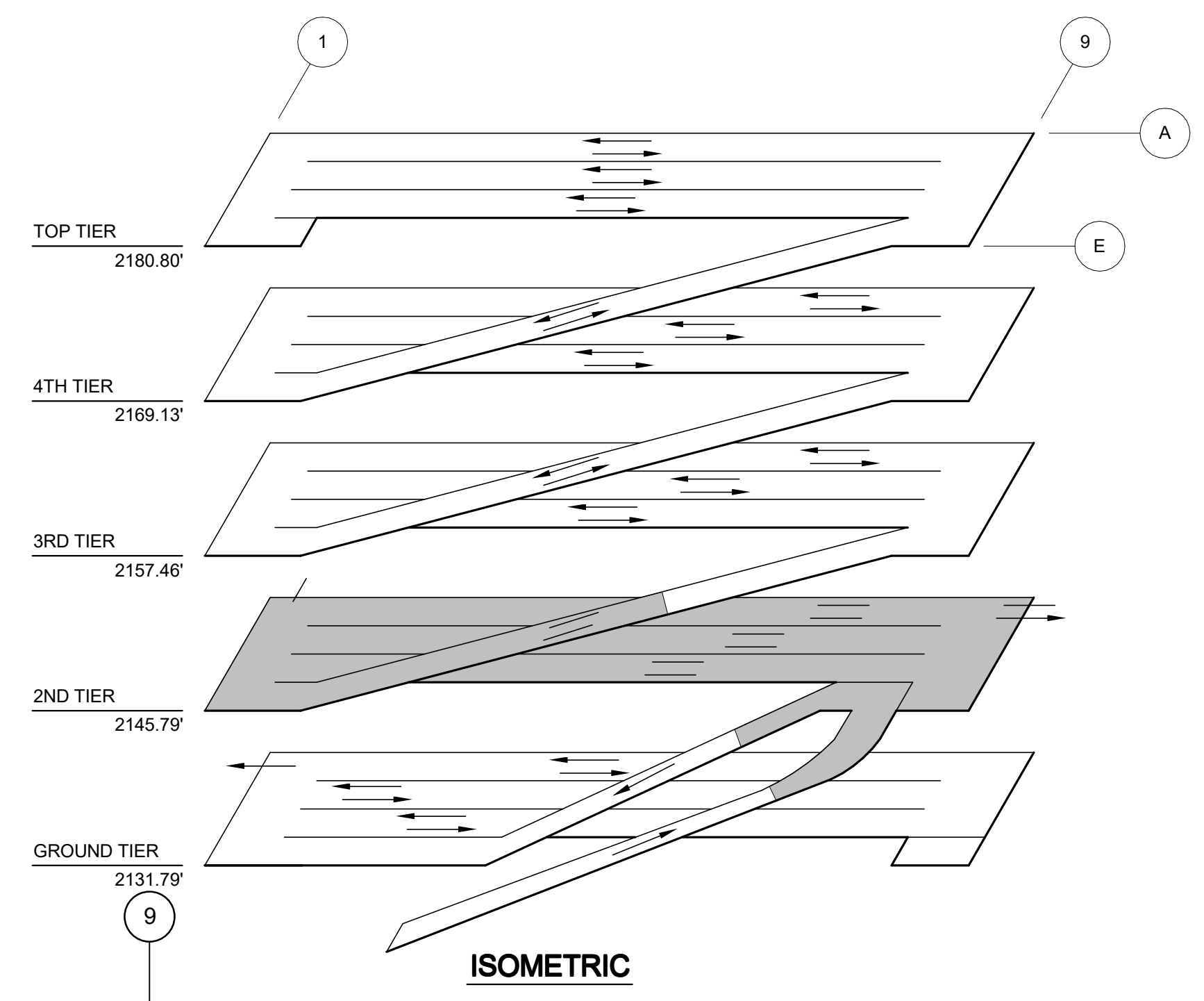
2 90° STRIPING DETAIL
 1/8" = 1'-0"



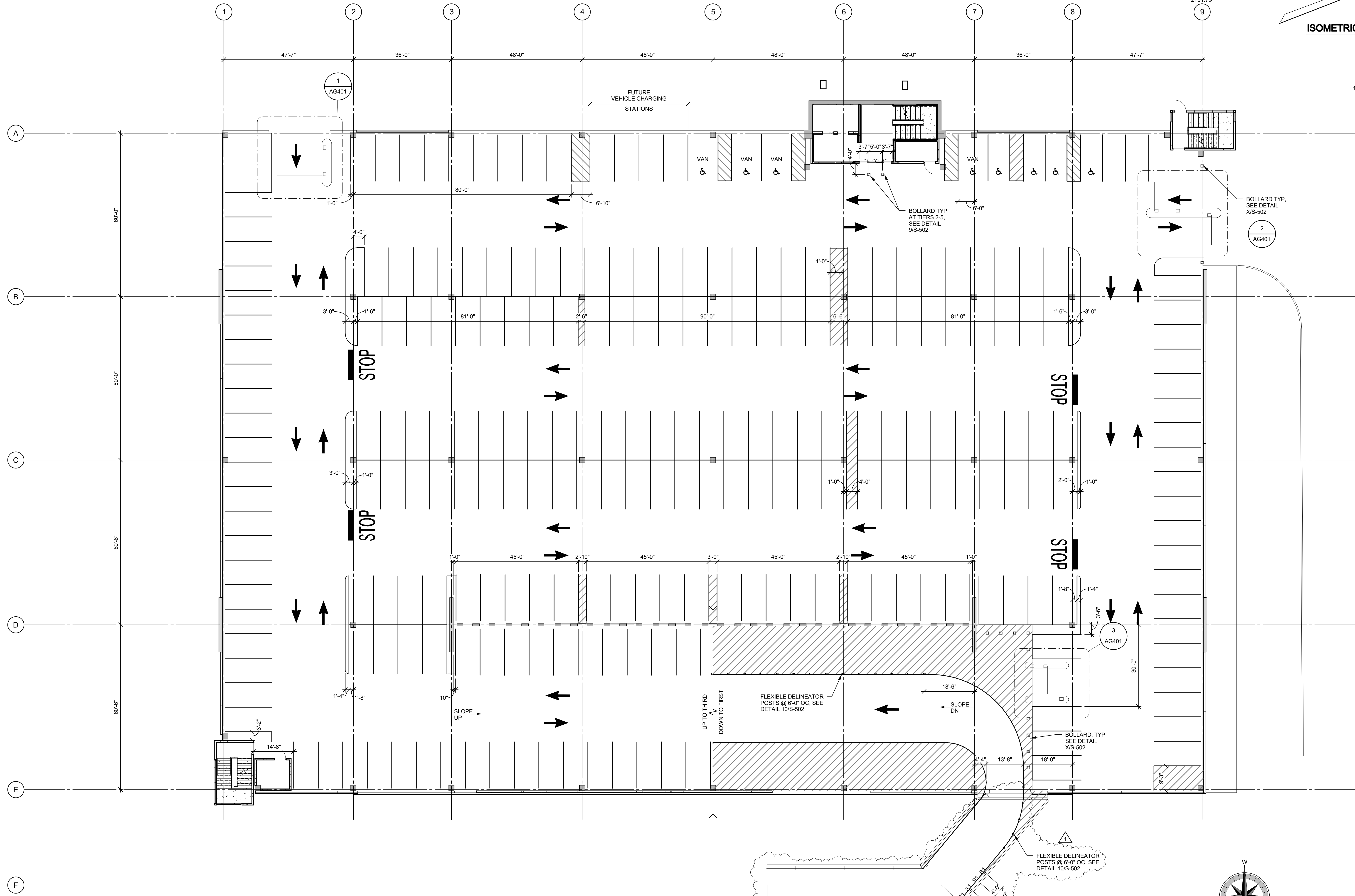
3 90° STRIPING DETAIL
 1/8" = 1'-0"



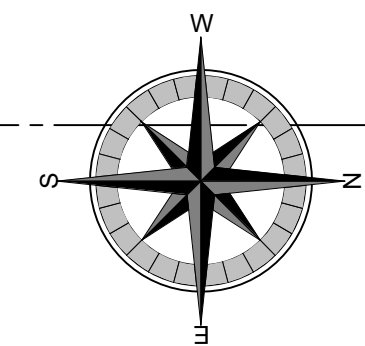
4 S1
 1/1/2" = 1'-0"



ISOMETRIC



1 SECOND TIER PLAN
 1/16" = 1'-0"



8/10/2017 3:16:54 PM

Change Order Request # 27

BUILDING ON A GREAT TRADITION

20-Sep-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #27

Dear Mr. Moody

This Change Order Request is for the additional work to remediate the subgrade to the SOG due to the effects of unusually high rain falls. This price is based off of Unit Price #11 for Unsuitable Excavation.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
American South General Contractors	561.86	CY	35.00	19,665.10
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			35.00	19,665.10
OH&P			10.00%	1,966.51
SUBTOTAL				<hr/> 21,631.61
BOND			1.0%	216.32
TOTAL CHANGE ORDER PROPOSAL				<hr/> 21,847.93

We have included S&ME's final report quantifying the final results in cubic yardage.

Upon approval, please issue a Change Order for the total lump sum amount of **\$21,847.93**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager



September 6, 2017

Delta Airport Consultants, Inc.
11121 Carmel Commons Boulevard
Suite 435
Charlotte, North Carolina 28226

Attention: Mr. Ken Moody

Reference: **Resident Project Representative Services September 1, 2017 Site Visit
Asheville Regional Airport Parking Garage**
Asheville, North Carolina
S&ME Project No. 3341-16-030

Dear Mr. Moody:

The following report outlines my site visit for the Asheville Regional Airport parking garage project. If you have any questions or concerns, please do not hesitate to contact myself by any means you find convenient.

Sincerely,

S&ME, Inc.

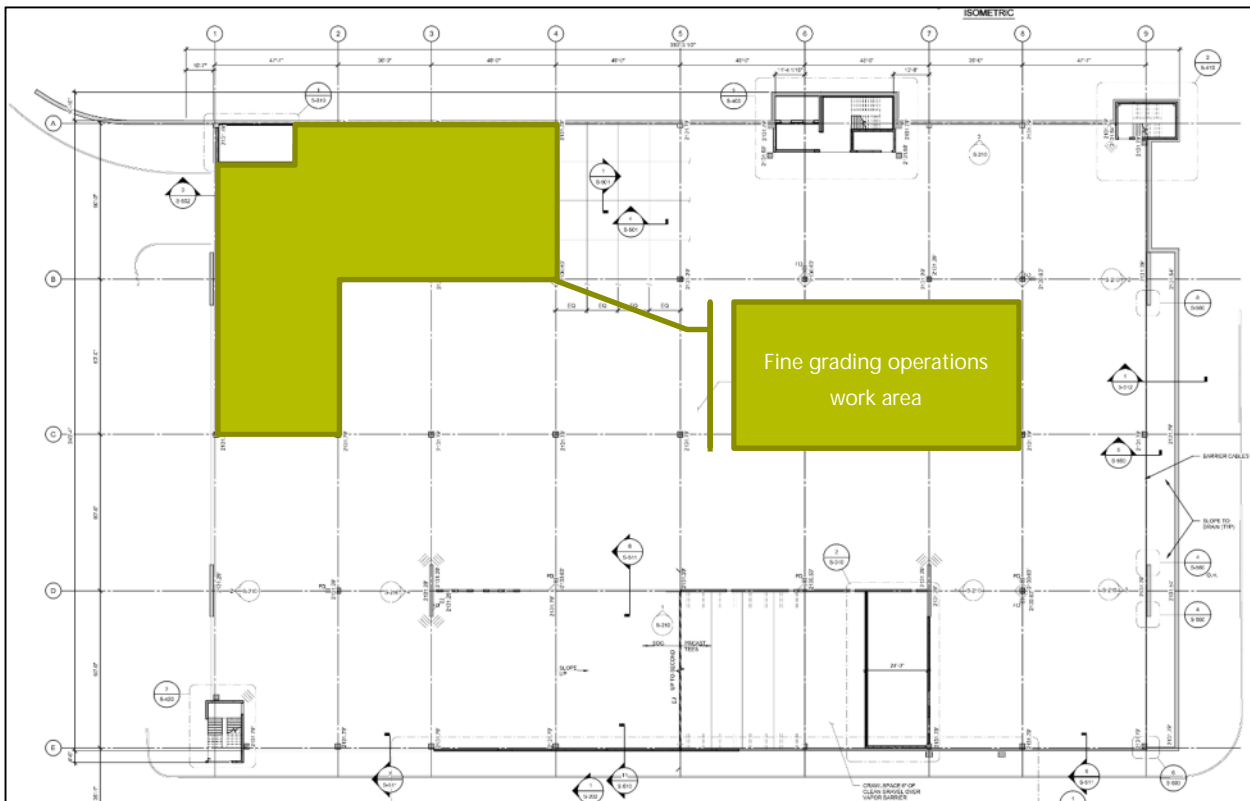
A handwritten signature in blue ink, appearing to read 'V. Kuspa IV'.

Victor P. Kuspa IV
Resident Project Representative



◆ Overview

NHM Constructors has resumed and completed the fine grading / undercut operations on the soil for the ground tier concrete slab-on-grade.





◆ **Undercut Operations**

There were no areas to be remediated this date.

◆ **Fine Grading Operations**

Soils between column lines "A" and "B", 1 and 4 lines, and between column lines "B" and "C", 1 and 2 lines, were put to approximate subgrade.

◆ **Backfill Operations**

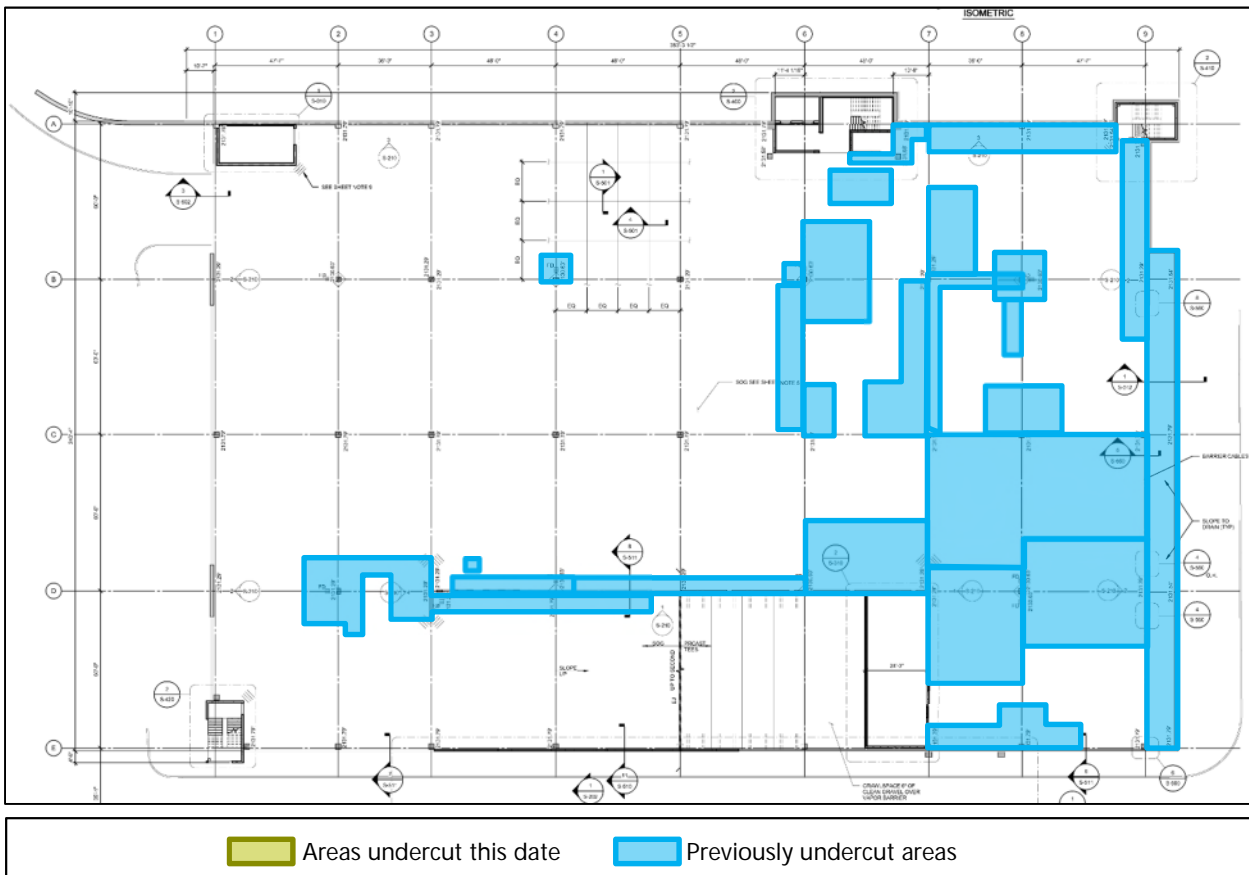
Utility trenches that were not completely backfilled were backfilled with ABC (aggregate base course) stone at the discretion of the contractor and were not totaled for inclusion into the remediation cubic yardage.



◆ Summary

No areas were remediated this date. With the final portion of the ground tier slab-on-grade soil placed to subgrade between column lines "A" and "B" between 1 and 4 line, and between column lines "B" and "C" between 1 and 2 line, the remediation is complete.

- Total approximate quantities removed this date: 0 cubic yards
- Total approximate quantities removed: 561.86 cubic yards





1	Location / Orientation	Between column lines "A" and "B", 1 and 3 lines
	Remarks	Fine grading operations.



Date: 9/1/2017

Photographer: Victor P. Kuspa IV

2	Location / Orientation	Between column lines "B" and "C", 1 and 2 lines
	Remarks	Fine grading operations.



Date: 9/1/2017

Photographer: Victor P. Kuspa IV

Change Order Request # 28

BUILDING ON A GREAT TRADITION

20-Sep-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #28

Dear Mr. Moody

This Change Order Request is for the additional required to move the water meter out from the island into the existing paved parking area.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Unit Price #15	6.18	TN	115.00	710.70
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			115.00	710.70
OH&P			10.00%	71.07
<hr/>				
SUBTOTAL				781.77
BOND			1.0%	7.82
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				789.59

We have included tonnage tickets as supplied by Harrison via NHM.

Upon approval, please issue a Change Order for the total lump sum amount of **\$789.59**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

APAC-ENKA AS16
1188 HARRISON CONSTRUCTION
2870 BOX 6390
KNOXVILLE, TN 37914

WARNING: SEE BACK OF TICKET
FOR SPECIFIC PRODUCT WARNINGS.

Date: 08/24/17 Time: 14:22

Plant: 04142

254962

TICKET #

Customer: 986210

Job: 1

Phase: 1 - RS 9.5B

NHM Constructors LLC

ASHEVILLE AIRPORT 1166

PO Box 6385

Asheville, NC 28816

Map #:

Carrier: 999 - No Haul Carrier

PO #:

Zone: 0

Truck: NHM - NEWMAN HENRY MURPHY

Product: 49990 - RS 9.5B

VDOT #:

JMF: 12-0259-182

*** Silo 3 Loadout ***

GROSS

TARE

NET

1/LDS Today

3/LDS To Date

40520 1b 28160 1b 12360 1b

6.18 Tn

18.48 Tn

20.26 TN 14.08 TN 6.18 TN

5.61 Mg

16.76 Mg

18.38 Mg 12.77 Mg 5.61 Mg

65000 1b --Legal-- 59500 1b

Previous unrelated
patching work.

North Carolina Public Weighmasters
DAVID G CONARD 23320
License Certification:
Exp. Date: 6/30/2018

TEMP: PLANT _____ ROAD _____

INSPECTOR _____

DRIVER NAME *Auto Bennett*

CONTROL NO: 3679830

RECEIVED BY _____

Change Order Request # 29

BUILDING ON A GREAT TRADITION

27-Sep-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #29

Dear Mr. Moody

This Change Order Request is for the additional work to add bollards to the Taxi Lot. This includes all labor and materials to supply and install along with painting the bollards AVL Green and giving the credit back for the bollard sleeves.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
American South (Bollard Cover Credit)	1	LS	-4,144.52	-4,144.52
NHM (Concrete Division) (Footing & Install)	1	LS	9,179.64	9,179.64
Welding Unlimited (Bollard Supply)	1	LS	10,348.60	10,348.60
Southern Painting (Bollard Painting)	1	LS	2,461.46	2,461.46
	1	LS		0.00
<hr/>				
SUBTOTAL			17,845.18	17,845.18
OH&P	(On Add Only)		10.00%	2,198.97
<hr/>				
SUBTOTAL				20,044.15
BOND			1.0%	200.44
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				20,244.59

We have included the revised sheet C3.2 showing these new bollards along with the subcontractor backup documentation.

Upon approval, please issue a Change Order for the total lump sum amount of **\$20,244.59**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: _____
Contractor: _____

Proposal #: ASGC 29
Project # 09-163
Date: _____

Description of Change: _____

Material:

Subtotals

1. Total Cost of Materials (Attach Backup)		-\$4,022.92	
2. Sales Tax	7%	-\$281.60	
3. Overhead & Profit	10%		-\$4,304.52

Labor:

1 Man - 1 Day - @ \$20/HR

4. Total Man Hour Cost (Attach Backup)		\$160.00	
5. Payroll Burden (Enter Percentage)	0%	\$0.00	
6. Overhead & Profit	10%		\$160.00

Equipment:

7. Equipment / Equipment Rental (Attach Backup)			
8. Overhead & Profit	10%	\$0.00	\$0.00

Unit Prices:

9. Unit Price (If Applicable)			\$0.00
-------------------------------	--	--	--------

Subcontractors:

10. Subcontractor (Attach Subcontractor Backup)			
11. Overhead & Profit	10%	\$0.00	\$0.00

		Subtotal of Proposal	-\$4,144.52
12. Payment & Performance Bond (Enter Bond Rate Percentage)			\$0.00
		Total of Change Order Proposal	-\$4,144.52

Time Extension Request: _____ Days

Schedule Activity # Affected _____ **Does this effect the critical path?** _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: _____ Date: _____



2525 Clark Street □ Detroit, MI 48209
 (313) 842-7290
 www.idealshield.com

Proposal

Quote Number:	153482
Date:	07/20/2016
Sales Person:	Bindu Abraham
Phone	313-551-2283
E-Mail	babraham@idealshield.com
Valid For	14 Days
Payment Terms	

Bill To	Ship To
AMERICAN SOUTH GENERAL CONTRACTORS INC BILL MORGAN PO BOX 10 SANFORD, NC 27331 USA (919) 721-0931	AMERICAN SOUTH GENERAL CONTRACTORS INC BILL MORGAN ASHEVILLE, NC 28801 USA (919) 721-0931

Bumper Post Sleeves Consist of:

- Dome Top-Lo-Density
- Thermoplastic Polyethylene (LDPE) w/ Ultra-Violet and Anti-Static Additives
- Secured with Ideal Shield Patented tape
- ¼" nominal wall thickness

Quantity	Part Number:	Product	Unit Price	Ext. Price
24	PPH-YL-08-069-S	PPH-YL-08-069-S YELLOW CUT TO 48"	\$62.9900	\$1,511.7600
24	BPD-YL-10-069-S	BPD-YL-10-069-S BPD-YELLOW-10-069-S	\$90.0000	\$2,160.0000

Subtotal:	\$3,671.7600
Tax:	Not Included
Shipping:	\$351.1600
Total:	\$4,022.9200

Grand Total

Currency:	USD	Subtotal:	\$3,671.7600
		Tax:	Not Included
Shipping Provider:		Shipping:	\$351.1600
		Total:	\$4,022.9200

Prices are subject to change without written notification. Specific quotes are valid for only 14 days from the date issued unless revoked in writing by Ideal Shield L.L.C.

Thank you for the opportunity to quote this project. The above pricing is on material only and does not include taxes or installation costs. If you have any questions concerning this quote, please feel free to contact me at 313-551-2283 at any time.

Sales tax may be applicable in Michigan, Florida and Arizona

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Concrete
Contractor: NHM Constructors (Concrete)

Proposal #: 3-R2
Project # 09-163
Date: 9/27/17

Description of Change: _____

Material:	Concrete, Brick, Form/Kicker Mat, Patch Mat, Ex. Jt Mat, Nails, & Rebar	Subtotals
	1. Total Cost of Materials (Attach Backup)	\$2,150.00
	2. Sales Tax 7%	\$150.50
	3. Overhead & Profit 10%	\$230.05
		\$2,530.55

Labor:	4 Men @ 3 Days @ \$20/HR = 96 Manhours + One Operator @ \$50/HR = 24 Manhours	
	4. Total Man Hour Cost (Attach Backup)	\$3,120.00
	5. Payroll Burden (Enter Percentage) 35%	\$1,092.00
	6. Overhead & Profit 10%	\$421.20
		\$4,633.20

Equipment:	Mini-Excavator @ 550/Day and \$100 Delivery	
	7. Equipment / Equipment Rental (Attach Backup)	\$1,750.00
	8. Overhead & Profit 10%	\$175.00
		\$1,925.00

Unit Prices:		
	9. Unit Price (If Applicable)	\$0.00

Subcontractors:		
	10. Subcontractor (Attach Subcontractor Backup)	
	11. Overhead & Profit 10%	\$0.00
		\$0.00

		Subtotal of Proposal	\$9,088.75
	12. Payment & Performance Bond (Enter Bond Rate Percentage)	1%	\$90.89
		Total of Change Order Proposal	\$9,179.64

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____
 Subcontractor: _____ Date: _____

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Structural Steel
Contractor: Welding Unlimited

Proposal #: #3 revised
Project # 09-163
Date: 9/27/2017

Description of Change: Add (14) 6" galvanized pipe bollards and 5 removable 6" galvanized pipe bollards
NOTE: Removable bollards only set in concrete 12" Delivered only no installation and no paint
Per ASI 006

Material:			Subtotals
1. Total Cost of Materials (Attach Backup)		\$3,295.00	
5 removable bollards			
2. Sales Tax		\$230.65	
3. Overhead & Profit	10%	\$353.00	\$3,878.65
Labor:			
4. Total Man Hour Cost (Attach Backup)			
5. Payroll Burden (Enter Percentage)			
6. Overhead & Profit			\$0.00
Equipment:			
7. Equipment / Equipment Rental (Attach Backup)			
8. Overhead & Profit			\$0.00
Unit Prices:			
9. Unit Price (If Applicable)	2040	\$3.00	\$6,120.00
(14) 6" pipe bollards galvanized			
Subcontractors:			
10. Subcontractor (drawings)			
11. Overhead & Profit	10%	\$0.00	\$0.00
		Subtotal of Proposal	\$9,998.65
12. Payment & Performance Bond		3.5%	\$349.95
(Enter Bond Rate Percentage)			
		Total of Change Order Proposal	\$10,348.60

Time Extension Request: 1 Days

Schedule Activity # Affected _____ **Does this effect the critical path?** _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____

Date: _____

Subcontractor: Vernon Klein

Date: 9/27/2017

Change order Req. 3			
Quantity	part		pounds
14	6" galvanized pipe		1993
14	Caps		47
		total	2040

1-800-BOLLARDS

The Bollard Experts

37525 Interchange Drive, Farmington Hills, MI 48335-1027
t. (800) BOLLARDS f. 949-528-6012

QUOTE

Number AAAQ2554
Date Jul 18, 2017

Sold To

Welding Unlimited
Vernon
3632 Butler Bridge Rd.
Mills River, NC 28759

Phone (828) 891-8501
Fax

Ship To

Welding Unlimited
Vernon
3632 Butler Bridge Rd.
Mills River, NC 28759

Phone
Fax

Salesperson	P.O. Number	Ship Via	Terms
Administrator			

Line	Qty	Description	Unit Price	Ext. Price
1	5	Removable Padlock Carbon Steel 6" Schedule 40 Cap: DOME Length: 54" Up:42" Down: 12" RAL#: EPOXY PRIMED PLEASE ADD 50.00 per bollard if you prefer galvanized	\$345.01	\$1,725.05
2	5	6" Embedment sleeve Depth: 12"	\$239.00	\$1,195.00

SubTotal	\$2,920.05
Tax	\$0.00
Shipping	\$124.94
Total	\$3,044.99

5 x 50 250

SEE ATTACHED CONDITIONS OF SALE

Please proceed according to this sales proposal.

PO #: _____ Print Name: _____

Credit Card Type: Visa / Mastercard / Amex Exp. Date: _____ CCV Code: _____

Credit Card #: _____ Payment Amount: _____

Signature: _____ Date: _____

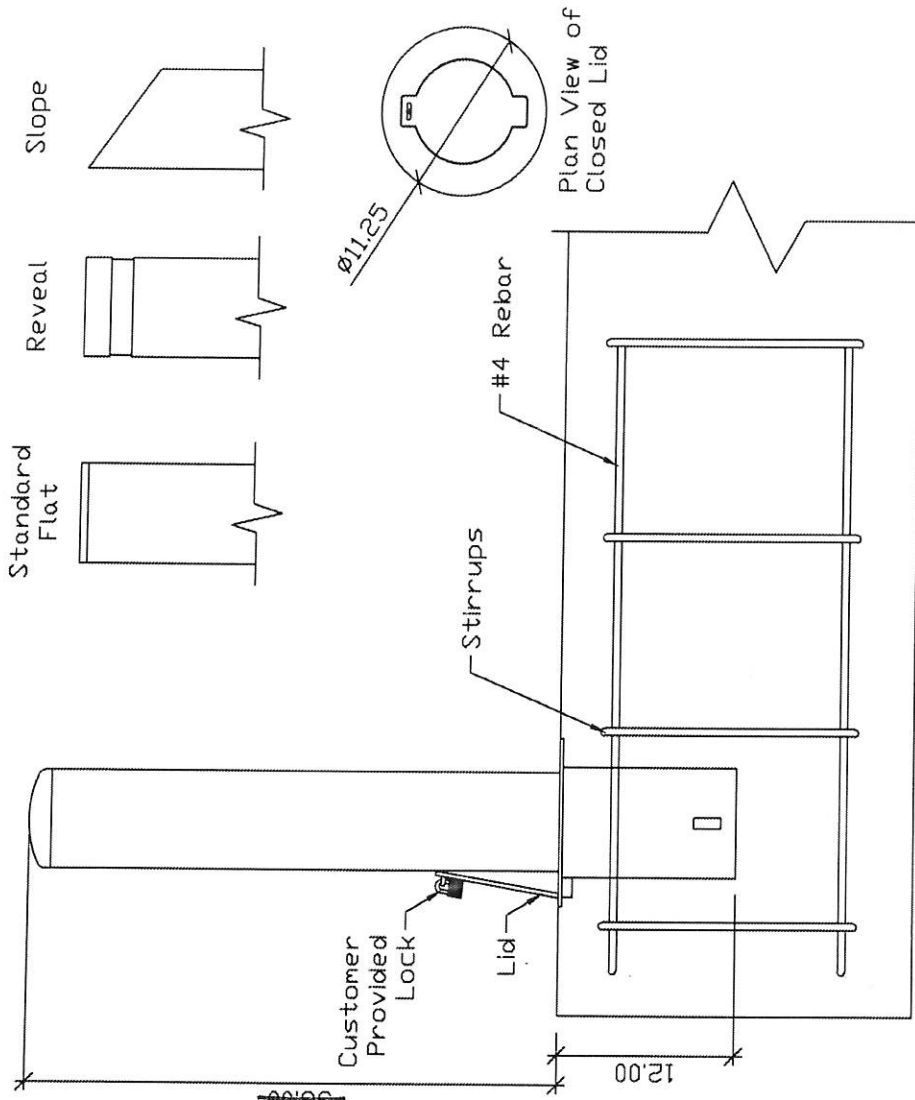
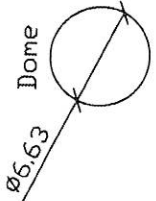
3295⁰⁰

OPTIONS

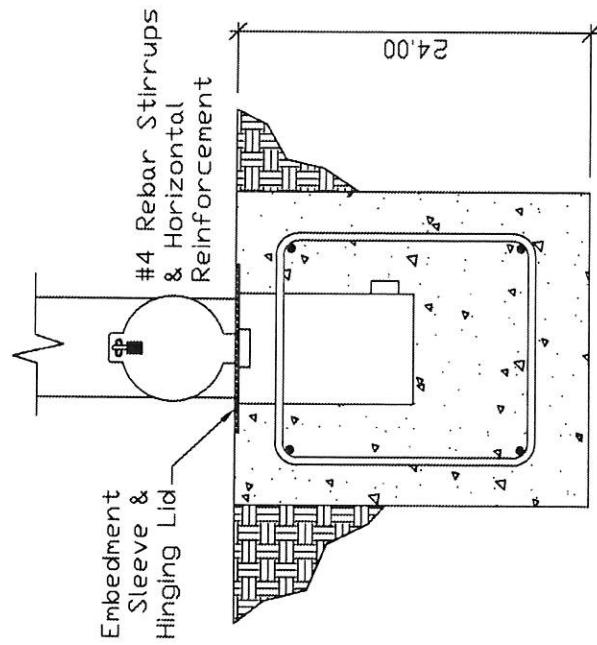
- Bollard Material (6" Nominal Size)
- PSS06000 Sch 10 Stainless Steel
 - PSS06040 Sch 40 Stainless Steel
 - PSS06080 Sch 80 Stainless Steel
 - PCS06040 Sch 40 ASTM A-53 Carbon Steel
 - PCS06080 Sch 80 ASTM A-53 Carbon Steel
- Stainless Steel Bollard Finish
- Polished #4 Finish
 - As Specified

- Carbon Steel Bollard Finish
- Powdercoat RAL #
 - As Specified *Galvalume*
- Stainless Steel Bollard Material
- Type 304
 - Type 316

- Bollard Cap Style
- Dome
 - Standard Flat
 - Reveal
 - Sloped



ELEVATION VIEW - FOUNDATION
(CONCEPTUAL DESIGN ONLY)



END VIEW - FOUNDATION

REVISIONS		
LTR	DESCRIPTION	DATE

SERIES NUMBER	XX-S060-XX	CASE CODE	6N0J8	SIZE	DRAWING NUMBER
PRODUCT FAMILY	FIXED BOLLARD	DRAWN BY	MJV	64" PL CUTSHEET SIZE A FDRB	
PRODUCT CLASS	STANDARD OR CUSTOMER DEFINED	CHECKED BY	MJV	DRAWING REV.	
PRODUCT LINE	BOLLARD	APPROVED BY		SHEET 1 OF 1	
PRODUCT AND LINE	LOCKING	RELEASE DATE			
1-800-BOLLARDS, INC.					
24370 Northwest Terr. Hwy, Suite 250					
Southfield, MI 48075					
PHONE: 1-800-BOLLARDS					
WEB: WWW.BOLLARDS.COM					
EMAIL: INFO@BOLLARDS.COM					
BOLLARD, LOCKING					

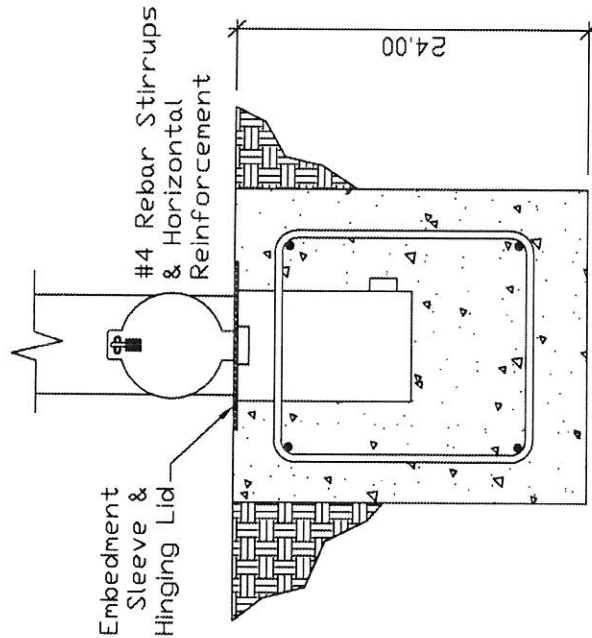
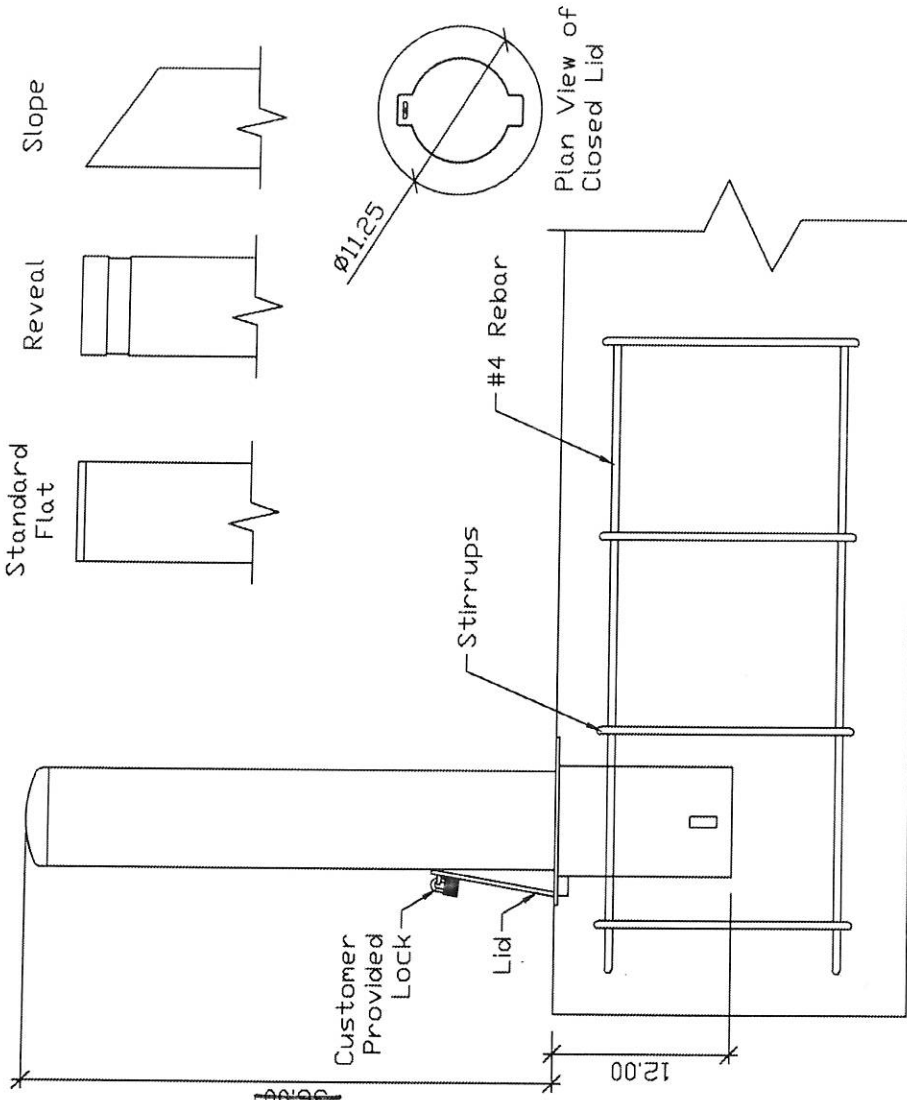
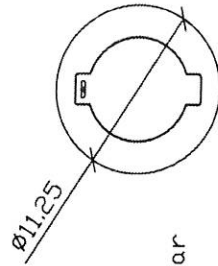
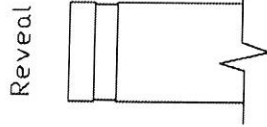
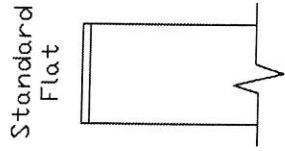
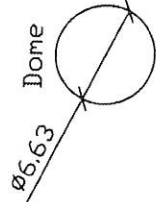
APPLICATION	
NEXT ASSY	USED ON

OPTIONS

- Bollard Material (6" Nominal Size)
- PSS06000 Sch 10 Stainless Steel
 - PSS06040 Sch 40 Stainless Steel
 - PSS06080 Sch 80 Stainless Steel
 - PCS06040 Sch 40 ASTM A-53 Carbon Steel
 - PCS06080 Sch 80 ASTM A-53 Carbon Steel
- Stainless Steel Bollard Finish
- Polished #4 Finish
 - As Specified

- Carbon Steel Bollard Finish
- Powdercoat RAL # _____
 - As Specified *Galvanized*
- Stainless Steel Bollard Material
- Type 304
 - Type 316

- Bollard Cap Style
- Dome
 - Standard Flat
 - Reveal
 - Sloped



ELEVATION VIEW - FOUNDATION
(CONCEPTUAL DESIGN ONLY)

APPLICATION	USED ON
NEXT ASSY	

SERIES NUMBER	XX-S060-XX	CAGE CODE	SIZE	DRAWING NUMBER	6th PL OUTSHEET SIZE A FISH
PRODUCT FAMILY	FIXED BOLLARD	6NQJB		DRAWN BY:	MJM
PRODUCT CLASS	STANDARD OR CUSTOMER DEFINED			CHECKED BY:	
PRODUCT LINE	BOLLARD			APPROVED BY:	
PRODUCT AND LINE	LOCKING			SHEET:	1 OF 1
TITLE					
BOLLARD, LOCKING					
1-800-BOLLARDS, Inc.					
54376 Northwestern Hwy, Suite 250					
Phoenix, AZ 85026					
PHONE: 480-988-8888					
WEB: WWW.BOLLARDS.COM					
EMAIL: INFO@BOLLARDS.COM					

REVISIONS		
LTR	DESCRIPTION	DATE

APPVD

6" DIA A36 STEEL, STRUCTURAL PIPE.
PAINTED AVL GREEN TO MATCH
EXISTING AIRFIELD BOLLARDS. FILL
WITH CONCRETE.

3" RADIUS WELDED STEEL CAP

SLOPE CONCRETE
TO DRAIN

GRADE

1"

3'-6"

4'

6"

3,000 PSI
CONCRETE
BASE (TYP)

4" TYP

PIPE BOLLARD DETAIL

NTS

DRAWING: 15086-ext-bollard.dwg LAYOUT: L1



www.deltaairport.com

PIPE BOLLARD DETAIL ASHEVILLE REGIONAL AIRPORT

DRAWN BY: DWS CHECKED BY: KWM SCALE: NONE DATE: JULY 2016

EXHIBIT
1

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: _____
Contractor: Southern Painting

Proposal #: 1/R
Project # 09-163
Date: _____

Description of Change: Painting Pipe Bollards

Material:							Subtotals
	1. Total Cost of Materials (Attach Backup)		\$500.50				
	2. Sales Tax	7%	\$35.04				
	3. Overhead & Profit	10%	\$53.55				\$589.09

Labor:							
	4. Total Man Hour Cost (Attach Backup)		\$1,200.00				
	5. Payroll Burden (Enter Percentage)	40%	\$480.00				
	6. Overhead & Profit	10%	\$168.00				\$1,848.00

Equipment:							
	7. Equipment / Equipment Rental (Attach Backup)						
	8. Overhead & Profit	10%	\$0.00				\$0.00

Unit Prices:							
	9. Unit Price (If Applicable)						\$0.00

Subcontractors:							
	10. Subcontractor (Attach Subcontractor Backup)						
	11. Overhead & Profit	10%	\$0.00				\$0.00

	12. Payment & Performance Bond		Subtotal of Proposal	\$2,437.09			
	(Enter Bond Rate Percentage)	1%	\$24.37				\$24.37
			Total of Change Order Proposal	\$2,461.46			\$2,461.46

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____
 Subcontractor: _____ Date: _____



**SHERWIN
WILLIAMS**

PRICE QUOTATION

Account #: 5310-7155-3

07/25/2017
Quote #: 3788283

SOUTHERN PAINTING
PO BOX 8924
GREENVILLE, SC 296048924
(864) 232-4010

Project:	ASHEVILLE REGIONAL AIRPORT
Start Date:	07/25/2017
Completion Date:	09/25/2017
Purchase Type:	Single Purchase

We are pleased to quote you as follows:

SALES NUMBER	SIZE	PRODUCT/REX NUMBER	DESCRIPTION	QTY	PRICE
6401-15325	GALLON	B50WZ0001	Kem Kromik® Universal Metal Primer Off White	1	\$42.00
617-4072	GALLON	B54Y00037	Industrial Enamel Safety Yellow	1	\$53.50

We thank you for your consideration of Sherwin-Williams products and look forward to supplying these products to you.

Note: All prices are per gallon/unit.

TERMS OF THE SALE

Quotation Expires: 09/25/2017
F.O.B. Location:
Freight Terms:
Terms: As Agreed

By: Larry Thompson

Store Address: 1139 WHITE HORSE RD
City: GREENVILLE State: SC Zip: 29605 4933
Store Number: 2735
Phone: (864) 236-9116
Territory #: 5726

NOTICE: Please take notice that the quotation set forth above is not a contract and is subject to and conditioned upon approval by SHERWIN-WILLIAMS. In the event such approval is not obtained, you will be provided with a revised quotation and the quotation set forth above shall be null, void and of no force or effect. The pricing and recommendations detailed in this proposal represent confidential information provided by SHERWIN-WILLIAMS. We request that it not to be copied or shared with others outside your firm.

Drawn By: DWS
 Checked By: RWI
 Approved By: RWM



GRESHAM SMITH AND PARTNERS
 Design Services For The Built Environment

Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



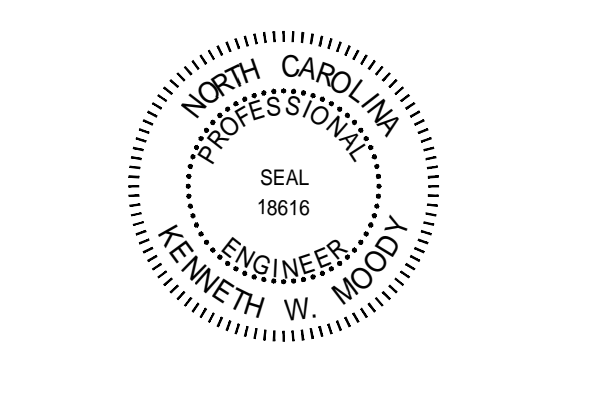
Asheville Regional Airport
 Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

DELTA AIRPORT CONSULTANTS, INC.
 1121 Carmel Commons Blvd., Suite 435
 Charlotte, NC 28226
 704.521.9101
 Delta Project No. 15086

WALKER PARKING CONSULTANTS



BENCHMARK SCHEDULE			
BM#	NORTHING	EASTING	ELEVATION
CP-1	631313.4090	947164.5280	2116.34
CP-2	631320.9580	946905.0860	2123.56
CP-3	631543.6880	946800.3160	2132.25
CP-4	631641.1570	946663.8360	2144.86
CP-5	631707.8960	946962.7130	2128.52
CP-6	631947.4710	946963.9970	2131.42
CP-7	632094.7640	946579.6150	2145.79
CP-8	632401.1060	946804.9430	2145.24

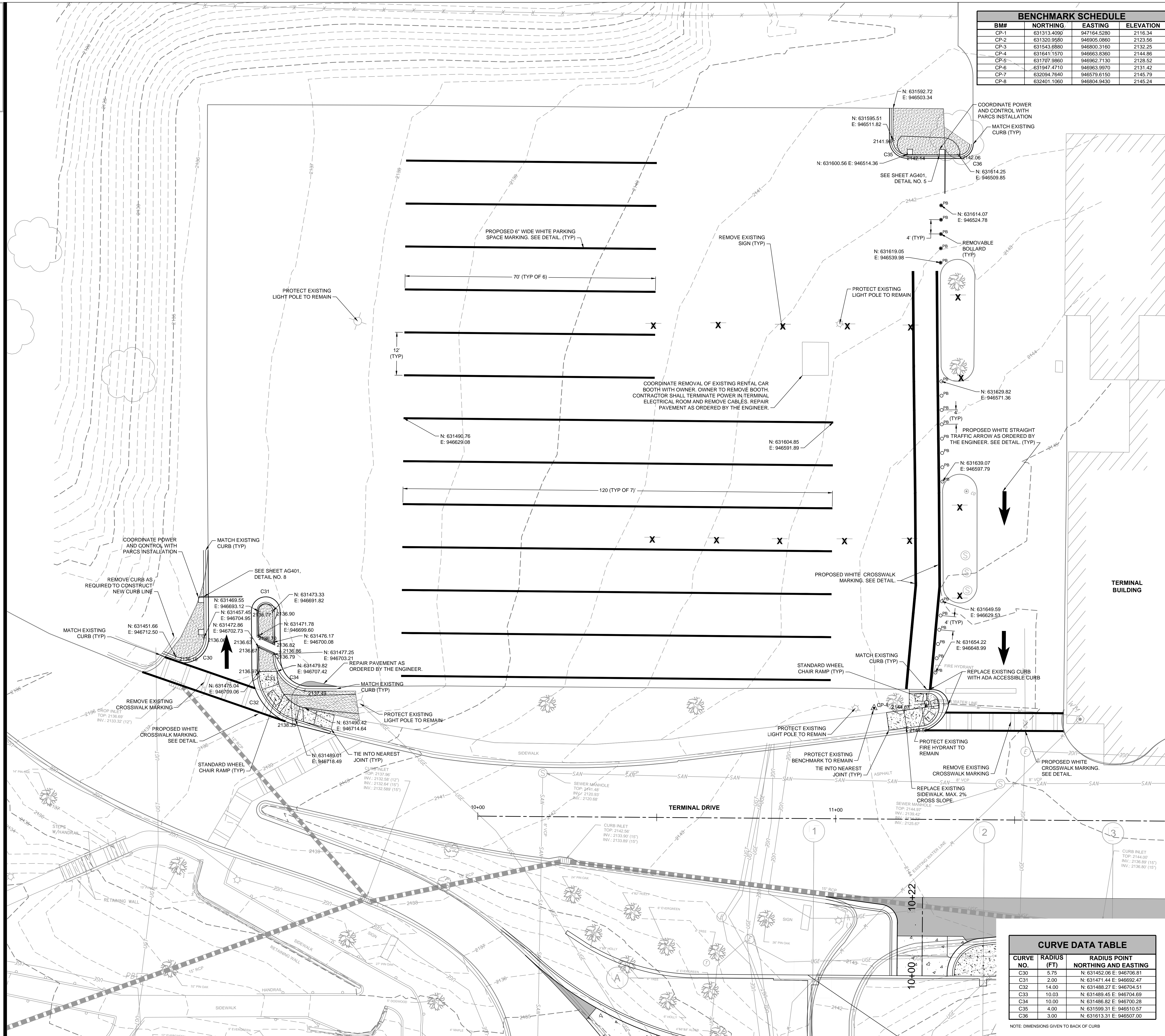
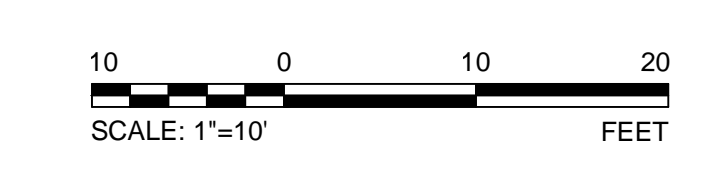


LEGEND	
BM-X	BENCHMARK LOCATION
B-X	BORING LOCATION (APPROX)
918.33	EXISTING SPOT ELEVATION
905.44	PROPOSED SPOT ELEVATION
[Symbol]	EXISTING DROP INLET
[Symbol]	EXISTING ENDWALL/HEADWALL/WINGWALL
[Symbol]	EXISTING STORM MANHOLE
[Symbol]	EXISTING FIRE HYDRANT
[Symbol]	EXISTING POLE (LIGHTED)
[Symbol]	EXISTING TREE
[Symbol]	EXISTING ROADWAY SIGN
X	EXISTING ROADWAY SIGN TO BE REMOVED
[Symbol]	EXISTING POLE (LIGHTED)
[Symbol]	PROPOSED BOLLARD
[Symbol]	PROPOSED REMOVABLE BOLLARD
[Symbol]	PROPOSED CURB & GUTTER
[Symbol]	EXISTING DITCH
UG-E	EXISTING UNDERGROUND ELECTRICAL LINE
X	EXISTING FENCE
[Symbol]	PROPOSED FENCE
SF	PROPOSED SILT FENCE
FO	EXISTING FIBER OPTIC LINE
UG-T	EXISTING UNDERGROUND TELEPHONE LINE
W	EXISTING WATER LINE
[Symbol]	PROPOSED PAVEMENT REMOVAL
[Symbol]	PROPOSED PCC SIDEWALK
[Symbol]	PROPOSED BITUMINOUS PAVEMENT
[Symbol]	PROPOSED GRASS/LANDSCAPE AREA

- NOTES**
- NORTHING AND EASTING COORDINATES ARE GIVEN TO THE BACK OF CURB.
 - NORTHING AND EASTING COORDINATES ARE BASED ON NORTH CAROLINA STATE PLANE GRID (NAD 83).
 - THE CONTRACTOR SHALL FIELD STAKE ALL CURB LINES PRIOR TO CONSTRUCTION.
 - REMOVE ALL SIGNAGE, PARKING EQUIPMENT, ETC. ASSOCIATED WITH RENTAL CAR COMPANIES AND RETURN TO OWNER.

CURVE DATA TABLE		
CURVE NO.	RADIUS (FT)	RADIUS POINT NORTHING AND EASTING
C30	5.75	N: 631452.06 E: 946706.81
C31	2.00	N: 631471.44 E: 946892.47
C32	14.00	N: 631488.27 E: 946704.51
C33	10.03	N: 631489.45 E: 946704.89
C34	10.00	N: 631486.82 E: 946700.28
C35	4.00	N: 631599.31 E: 946510.57
C36	3.00	N: 631613.31 E: 946507.00

NOTE: DIMENSIONS GIVEN TO BACK OF CURB



12/18/2015 11:06:35 AM

Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	04/07/17	REVISED GROUND TRANSPORTATION LOT
2	07/05/17	REVISED PEDESTRIAN WALKWAYS
3	07/13/17	REVISED BOLLARDS

GROUND TRANSPORTATION LOT IMPROVEMENTS

C3.2

PROJECT: 41152.00
 DATE: 23 MAY 2016

Change Order Request # 30

BUILDING ON A GREAT TRADITION



4-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226

RE: Asheville Regional Airport Parking Garage Change Order Request #30

Dear Mr. Moody

This Change Order Request is for the additional work associated with the demolition of signs on the Civil drawings and the requirement to reinstall these signs in different locations not shown on the drawings, as well as the work associated with relocated signs to new locations that differ from our drawings which cause additional lengths of conduit and wire.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Fountain Electric Services (All Associated Electrical Work)	1	LS	19,419.63	19,419.63
NHM (Civil Division) (Tree Removal & Haul Off)	1	LS	1,873.04	1,873.04
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			21,292.67	21,292.67
OH&P			10.00%	2,129.27
SUBTOTAL				<hr/> 23,421.94
BOND			1.0%	234.22
TOTAL CHANGE ORDER PROPOSAL				<hr/> 23,656.16

We have included the new sign location layout drawings as well as marked up drawings showing the cause and impact at each location.

Upon approval, please issue a Change Order for the total lump sum amount of **\$23,656.16**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Electrical
Contractor: Fountain Services, LLC

Proposal #: FES 12-R4
Project # 09-163
Date: 10.4.17

Description of Change: Add conduits and wire to EXTERIOR SIGNS

Material:				Subtotals
1. Total Cost of Materials (Attach Backup)			\$2,135.69	
2. Sales Tax	7%		\$149.50	
3. Overhead & Profit	10%		\$228.52	<u>\$2,513.71</u>

Labor:				
4. Total Man Hour Cost (Attach Backup)			\$6,099.23	
5. Payroll Burden (Enter Percentage)	0%		\$0.00	
6. Overhead & Profit	10%		\$609.92	<u>\$6,709.15</u>

Equipment:				
7. Equipment / Equipment Rental (Attach Backup)			\$3,545.00	
8. Overhead & Profit	10%		\$354.50	<u>\$3,899.50</u>

Unit Prices:				
9. Unit Price (If Applicable)				<u>\$0.00</u>

Subcontractors:				
10. Subcontractor (Attach Subcontractor Backup)			\$5,550.00	
11. Overhead & Profit	10%		\$555.00	<u>\$6,105.00</u>

			Subtotal of Proposal	\$19,227.36
12. Payment & Performance Bond (Enter Bond Rate Percentage)	1%		\$192.27	<u>\$19,419.63</u>
			Total of Change Order Proposal	<u>\$19,419.63</u>

Time Extension Request: 5 Days
Schedule Activity # Affected _____ **Does this effect the critical path?** _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: _____ Date: _____

electrical mechanic	47	112.89	5305.83
foreman	58	11.3	655.4
project manager	69	2	138
			6099.23

Job ID: 160706 ASHEVILLE REGIONAL PARKING GARAGE
Project: 160706 ASHEVILLE REGIONAL PARKING GARAGE



CO: CO-0009: SIGN VERIFY

Summary by Item Number

3 Oct 2017 11:36:36

Item #	Size	Description	Q/M	Quantity	U/M	Mat Unit	Mat Result	Labor Unit	Lab Result
10199	1	PVC SCH 80	M	302	FT	0.4492	135.66	0.0644	19.43
10202	2	PVC SCH 80	M	240	FT	2.1339	512.14	0.0957	22.97
40043	OUNCE	PVC (GLUE) CEMENT	M	29	C	0.2655	7.70	0.0165	0.48
70036	6.	THHN/THWN CU (STR)	M	2,331	FT	0.4523	1,054.33	0.0127	29.62
390479	2"	ROCKY HORIZONTAL BORE	E	185	FT	0.0000	0.00	0.0262	4.85
390586	3"	RED TRENCH CAUTION TAPE	M	570	FT	0.0293	16.72	0.0050	2.82
390770	4">6"W 30"D	DITCH WITCH RT12 CLAY TRENCH	E	542	FT	0.0000	0.00	0.0532	28.83
400085	13"x 24"x 12"	HANDHOLE STR-WALL OPN-BOTM 5K	M	1	EA	136.3800	136.38	0.9713	0.97
400085	13"x 24"x 12"	HANDHOLE STR-WALL OPN-BOTM 5K	M	2	EA	136.3800	272.76	1.4570	2.91
Phase/Group totals:							2,135.69		112.89
Job totals:							2,135.69		112.89

Appling Boring Co., Inc.

Invoice

426 Baxter Cemetery Rd
Forest City, NC 28043

Date	Invoice #
5/12/2017	8169

Bill To

Fountain Electric Services LLC
244 Callahan-Koon Road
Spindale NC 28160

P.O. No.	Terms	Rep	Project
8169		JL	

Description	Qty	Rate	Amount
May 8 & 9 (1) 60' Bore (3- (1-1/2") Conduit)	60	30.00	1,800.00
May 8 & 9 (1) 60' Bore (3- (1") Conduit)	60	30.00	1,800.00
May 10 (2) 100' 4"Bores	200	30.00	6,000.00
May 11 (1) 60' Bore (3- (1") Conduit)	60	30.00	1,800.00
Work performed at Asheville Airport			
Total			\$11,400.00
Payments/Credits			\$0.00
Balance Due			\$11,400.00

Phone #	Fax #	E-mail
828-657-6397	828-657-4537	caycee.lovingood@gmail.com



Rental Rates

Equipment pricing may vary based on availability and market conditions.

\$500

4 Hour

\$500

Daily

\$1,330

Weekly

\$3,545

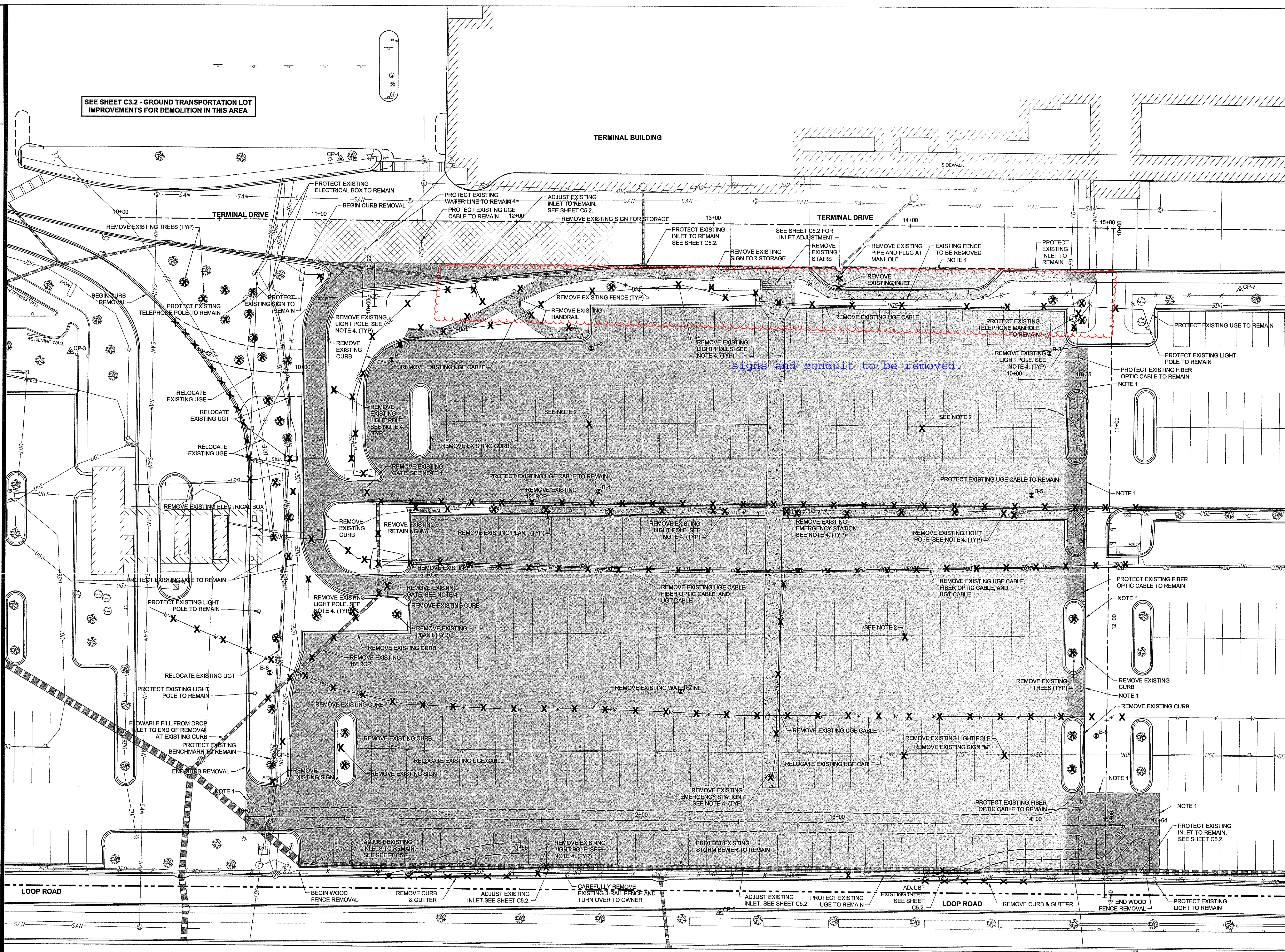
4 Week



Add to Cart

Drawn By: DMS
 Checked By: FWH
 Approved By: RWH

SEE SHEET C3.2 - GROUND TRANSPORTATION LOT IMPROVEMENTS FOR DEMOLITION IN THIS AREA



G S & P
GRESHAM SMITH AND PARTNERS
 Design Services For The Built Environment

Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710

- LEGEND**
- BM-X BENCHMARK LOCATION
 - EXISTING DROP INLET
 - EXISTING ENDWALL/HEADWALL/WINGWALL
 - EXISTING STORM MANHOLE
 - EXISTING SANITARY MANHOLE
 - EXISTING FIRE HYDRANT
 - ↑ EXISTING HOSE BIB
 - ⊕ EXISTING WATER METER
 - ⊕ EXISTING WATER VALVE
 - EXISTING ROADWAY SIGN
 - X EXISTING ROADWAY SIGN TO BE REMOVED
 - X EXISTING POLE (LIGHTED)
 - X EXISTING POLE (LIGHTED) TO BE REMOVED
 - X EXISTING POLE (NON-LIGHTED)
 - EXISTING TREE
 - EXISTING TREE TO BE REMOVED
 - EXISTING STORM DRAIN
 - EXISTING STORM DRAIN TO BE REMOVED
 - EXISTING DITCH
 - EXISTING UNDERGROUND ELECTRICAL LINE
 - X EXISTING UNDERGROUND ELECTRICAL LINE TO BE REMOVED
 - EXISTING FENCE
 - X EXISTING FENCE TO BE REMOVED
 - EXISTING FIBER OPTIC LINE
 - X EXISTING FIBER OPTIC LINE TO BE REMOVED
 - EXISTING GUARDRAIL
 - EXISTING AIRPORT PROPERTY LINE
 - EXISTING SANITARY SEWER LINE
 - STREAM
 - EXISTING UNDERGROUND TELEPHONE LINE
 - X EXISTING UNDERGROUND TELEPHONE LINE TO BE REMOVED
 - EXISTING WATER LINE
 - X EXISTING WATER LINE TO BE REMOVED
 - PROPOSED ASPHALT DEMOLITION
 - PROPOSED CONCRETE DEMOLITION
 - MILLING LIMITS



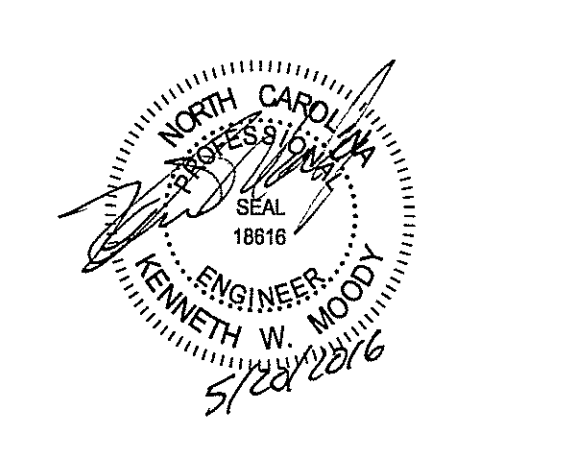
Asheville Regional Airport Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

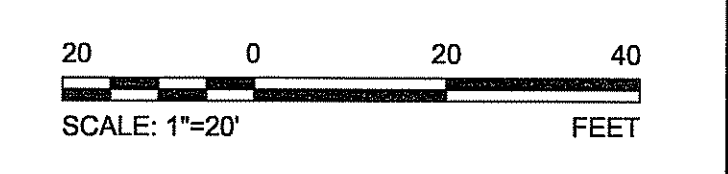
DELTA AIRPORT CONSULTANTS, INC.
 11121 Carmel Commons Blvd, Suite 435
 Charlotte, NC 28226
 704.521.9101
 Delta Project No. 15086

WALKER PARKING CONSULTANTS



Revision		
No.	Date	Description
1	04/25/2016	Issue for Permit
2	05/23/2016	Issue for Bid

- NOTES**
- SAW CUT NEAT VERTICAL FACE.
 - REMOVE AND RELOCATE EXISTING SIGN. SEE "SITE SIGN LOCATION PLAN" SHEETS.
 - DEMOLITION AND PAVEMENT REMOVAL SHALL BE PHASED TO ACCOMMODATE EROSION CONTROL REQUIREMENTS AND OPERATIONAL NEED OF THE AIRPORT PARKING LOT.
 - ALL GATE EQUIPMENT, LIGHT POLES, SECURITY EQUIPMENT, CAMERAS, BLUE PHONES, PARKING LOT CONTROLS, ETC. SHALL BE REMOVED AND TURNED OVER TO OWNER FOR STORAGE DURING CONSTRUCTION.
 - PAVEMENT REMOVAL AND EXCAVATION SHALL BE PHASED AS REQUIRED BY THE EROSION AND SEDIMENT CONTROL REQUIREMENTS.
 - SEE "GEOMETRIC LAYOUT" FOR LIMITS OF CURB REMOVAL.
 - REMOVE EXISTING PARKING BLOCKS WITHIN LIMITS OF CONSTRUCTION AND PAVEMENT REMOVAL.
 - COORDINATE ADJUSTMENT OF EXISTING STORM DRAININETS WITH STORM DRAIN PLAN.
 - BARRICADES SHALL BE INSTALLED IN AREAS OF FENCE REMOVAL FOR THE DURATION OF THE PROJECT. EXISTING FENCE TO REMAIN AS LONG AS POSSIBLE.



SEE SHEET C3.3 - REMOTE PARKING LOT IMPROVEMENTS FOR ADDITIONAL FENCE GATE DEMOLITION

12/18/2015 11:06:35 AM

DEMOLITION PLAN

C1.1
 PROJECT-41152.00
 DATE: 23 MAY 2016

Drawn By: Author
Checked By: Checker
Approved By: Approver

MESSAGE SCHEDULE - SITE				
Level	Sign Location	Sign Type	Message	Comments
GROUND TIER	EXT-02	EXT-B1	[SIDE A] Airport Exit Rental Car Return Terminal [ARROW: UP] Cell Phone Lot [ARROW: RIGHT]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-03	EXT-B1	[SIDE A] Garage Parking Rental Car Return [ARROW: UP]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-04	EXT-B1	[SIDE A] Garage Parking Rental Car Return [ARROW: UP]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-05	EXT-B1	[SIDE A] Garage Parking [ARROW: LEFT]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-06	EXT-B1	[SIDE A] Airport Exit Rental Car Return [ARROW: UP]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-08	EXT-B1	[SIDE A] Airport Parking Garage Parking [ARROW: LEFT] Left Lane	Existing Sign; Replace Sign Face
GROUND TIER	EXT-11	EXT-B1	[SIDE A] Airport Parking Garage Parking [ARROW: LEFT] Left Lane	Existing Sign; Replace Sign Face
GROUND TIER	EXT-12.1	EXT-B1	[SIDE A] Airport Parking Garage Parking Short Term [ARROW: UP] Long Term [ARROW: LEFT]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-12.2	EXT-B1	[SIDE A] Welcome to Asheville Regional Airport	Existing Sign; Replace Sign Face
GROUND TIER	EXT-13	EXT-B1	[SIDE A] Airport Exit [ARROW: UP] Rental Car Return Return to Terminal [ARROW: LEFT]	Existing Sign; Replace Sign Face
GROUND TIER	EXT-18	EXT-B1	[SIDE A] Asheville Regional Airport Rental Car Return [ARROW: LEFT]	Existing Sign; Replace Sign Face
SECOND TIER	EXT-20	EXT-B3.1	[SYMBOL: ENTER] ENTRANCE [SYMBOL: ENTER]	Reversible
SECOND TIER	EXT-21	EXT-B4	[SIDE A] CLEARANCE 8'- 2" [SIDE B] CLEARANCE 6'- 6"	Coordinate Installation Location With GC; Mounts To Face Of Rental Car Return Ramp
GROUND TIER	EXT-21.1	EXT-B4.1	[Side A] CLEARANCE 9'-0"	Coordinate Installation Location With GC; Reference Detail 04/Sheet S310 Reversible
GROUND TIER	EXT-21.2	EXT-B4	[SIDE A] CLEARANCE 9'-0" [SIDE B] CLEARANCE 6'-6"	Coordinate Installation Location With GC; Reference Detail 04/Sheet S310 Reversible
GROUND TIER	EXT-21.3	EXT-B4	[SIDE A] CLEARANCE 9'-0" [SIDE B] CLEARANCE 6'-6"	Coordinate Installation Location With GC; Reference Detail 04/Sheet S310 Reversible
GROUND TIER	EXT-21.4	EXT-B4	[SIDE A] CLEARANCE 9'-0" [SIDE B] CLEARANCE 6'-6"	Coordinate Installation Location With GC; Reference Detail 04/Sheet S310 Reversible
GROUND TIER	EXT-21.5	EXT-B4	[SIDE A] CLEARANCE 8'-2" [SIDE B] CLEARANCE 6'-6"	Coordinate Installation Location With GC; Reference Detail 04/Sheet S310 Reversible
SECOND TIER	EXT-22	EXT-B3.1	[SYMBOL: DNE] EXIT ONLY [SYMBOL: DNE]	Wall-Mount 1-Sided
SECOND TIER	EXT-23	EXT-B2.2	[SYMBOL: PARKING P]	Reversible
SECOND TIER	EXT-24	EXT-B3.2	[SYMBOL: ENTER] RENTAL CAR RETURN [SYMBOL: ENTER]	Reversible
SECOND TIER	EXT-25	EXT-B4	[SIDE A] CLEARANCE 8'- 2" [SIDE B] CLEARANCE 6'- 6"	Reversible
GROUND TIER	EXT-26	EXT-B3.1	[SIDE A] [SYMBOL: DNE] EXIT ONLY [SYMBOL: DNE] [SIDE B] EXIT	Reversible
GROUND TIER	EXT-27	EXT-B3.1	[SIDE A] [SYMBOL: DNE] EXIT ONLY [SYMBOL: DNE] [SIDE B] EXIT	Reversible
SECOND TIER	EXT-28	EXT-B4	[SIDE A] CLEARANCE 8'- 2" [SIDE B] CLEARANCE 6'- 6"	Reversible
SECOND TIER	EXT-29	EXT-B3.1	[SYMBOL: ENTER] ENTRANCE [SYMBOL: ENTER]	Flag-Mount 2-Sided
SECOND TIER	EXT-30	EXT-B2.1	[SYMBOL: PARKING P]	Flag-Mount 2-Sided
GROUND TIER	EXT-31	EXT-B1	[SIDE A] Toll Plaza [ARROW: RIGHT] [SIDE B] Short Term Garage Parking [ARROW: UP] Long Term [ARROW: RIGHT] Toll Plaza [ARROW: LEFT]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-32.1	EXT-B1	[SIDE A] Toll Plaza [ARROW UP]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-32.2	EXT-B1	[SIDE A] Airport Parking Garage Parking [ARROW: LEFT] Terminal [ARROW: UP]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner

MESSAGE SCHEDULE - SITE				
Level	Sign Location	Sign Type	Message	Comments
GROUND TIER	EXT-33	EXT-B1	[SIDE A] Rental Car Return [ARROW: LEFT] Return to Terminal [ARROW: UP]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-34	EXT-B1	[SIDE A] Rental Car Return Terminal [ARROW: RIGHT]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-36	EXT-B1	[SIDE A] Rental Car Return Next Right [ARROW: RIGHT]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-38	EXT-B1.1	[SIDE A] Airport Next Right [ARROW: RIGHT] Rental Car Return [ARROW: UP]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner; final sign height to be determined by sign fabricator via on-site survey
GROUND TIER	EXT-39	EXT-B1	[SIDE A] Exit to Toll Plaza Long Term [ARROW: UP]	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-41	EXT-B5	[SIDES A-D] A	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-42	EXT-B5	[SIDES A-D] B	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-43	EXT-B5	[SIDES A-D] C	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-44	EXT-B5	[SIDES A-D] D	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-45	EXT-B5	[SIDES A-D] E	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-46	EXT-B5	[SIDES A-D] F	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-47	EXT-B5	[SIDES A-D] G	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-48	EXT-B5	[SIDES A-D] H	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-49	EXT-B5	[SIDES A-D] J	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-50	EXT-B5	[SIDES A-D] K	Replace Existing Parking Lot Area ID Pole-Mounted Sign With New 4-Sided Sign
GROUND TIER	EXT-51	EXT-B6	[SIDE A] NO OVERSIZE VEHICLES Oversize vehicles proceed to south entry remote lot	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner
GROUND TIER	EXT-52	EXT-B6	[SIDE A] NO OVERSIZE VEHICLES Oversize vehicles proceed to south entry remote lot	New Sign; Survey and Stake Location For Review and Approval By Architect/Owner

MESSAGE SCHEDULE - SITE				
Level	Sign Location	Sign Type	Message	Comments



GRESHAM SMITH AND PARTNERS
Design Services For The Built Environment

- Atlanta GA Jacksonville FL
- Birmingham AL Knoxville TN
- Charlotte NC Louisville KY
- Cincinnati OH Memphis TN
- Columbus OH Miami FL
- Dallas TX Nashville TN
- Ft Lauderdale FL Richmond VA
- Houston TX Tallahassee FL
- Jackson MS Tampa FL

10 South 6th Street, Suite 100
Richmond, VA 23219
804.788.0710



Asheville Regional Airport
Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
Fletcher, NC 28732



Revision		
No.	Date	Description
1	04/25/16	Issue for Permit
2	05/23/16	Issue for Bid
3	07/01/16	Addendum 01

MESSAGE SCHEDULE-SITE

G900

PROJECT: 41152.00
DATE: 23 MAY 2016



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



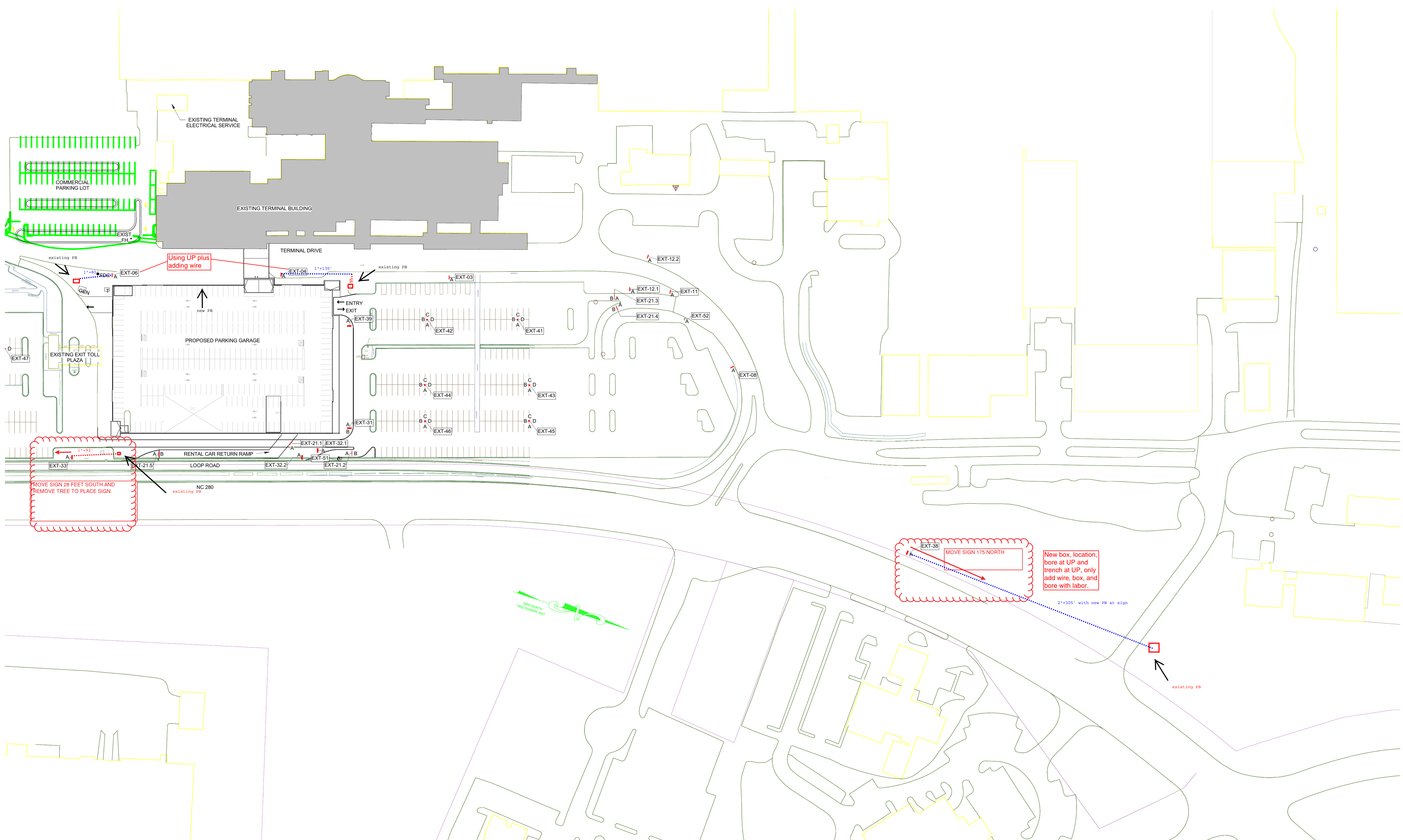
Asheville Regional Airport Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

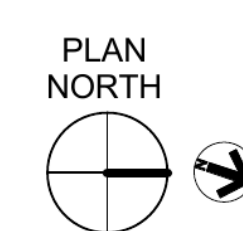


Drawn By: Author
 Checked By: Checker
 Approved By: Approver



8/29/2016 2:20:42 PM

1 SITE SIGN LOCATION PLAN - NORTH
 1/64" = 1'-0"

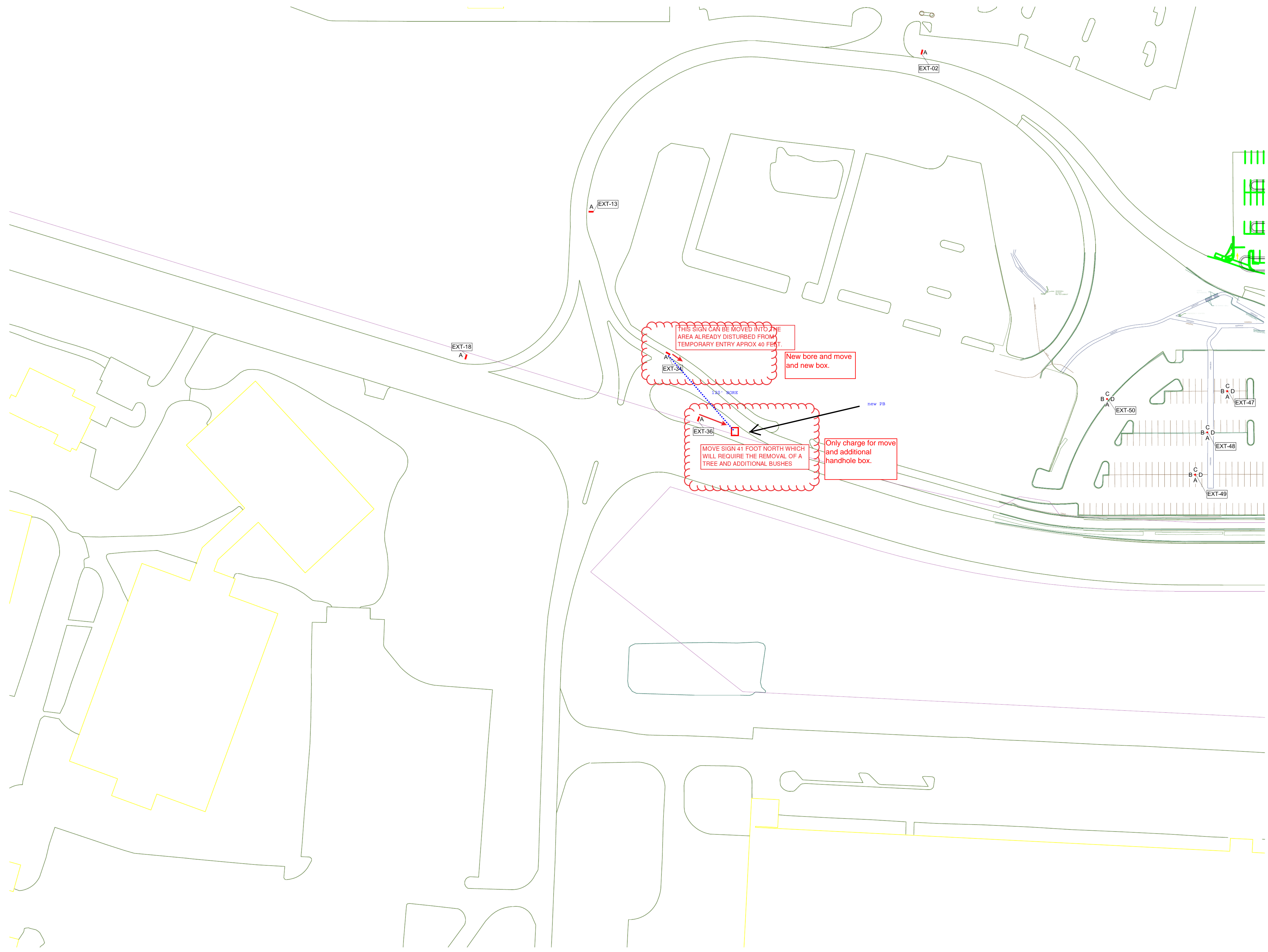


Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS

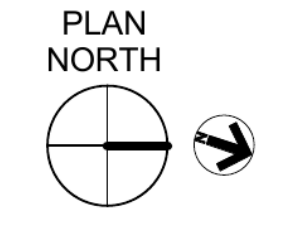
SITE SIGN LOCATION PLAN - NORTH

G200N
 PROJECT: 41152.00
 DATE: 21 SEPTEMBER 2016

Drawn By: Author
 Checked By: Checker
 Approved By: Approver



1 SITE SIGN LOCATION PLAN - SOUTH
 1/64" = 1'-0"



8/29/2016 2:20:46 PM



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

- Atlanta GA Jacksonville FL
- Birmingham AL Knoxville TN
- Charlotte NC Louisville KY
- Cincinnati OH Memphis TN
- Columbus OH Miami FL
- Dallas TX Nashville TN
- Ft Lauderdale FL Richmond VA
- Houston TX Tallahassee FL
- Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



Asheville Regional Airport
 Parking Garage

Greater Asheville Regional Airport
 Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732



Revision		
No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS

SITE SIGN LOCATION
 PLAN - SOUTH

G200S

PROJECT: 41152.00
 DATE: 21 SEPTEMBER 2016

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Sitework
Contractor: NHM Constructors, LLC

Proposal #: 11
Project # 09-163
Date: 9/27/2017

Description of Change: Removal of Trees

Material:					Subtotals
1. Total Cost of Materials (Attach Backup)			\$0.00		
2. Sales Tax	7%		\$0.00		
3. Overhead & Profit	10%		\$0.00		\$0.00

Labor:					
4. Total Man Hour Cost (Attach Backup)			\$660.00		
5. Payroll Burden (Enter Percentage)	36%		\$237.60		
6. Overhead & Profit	10%		\$89.76		\$987.36

Equipment:					
7. Equipment / Equipment Rental (Attach Backup)			\$780.00		
8. Overhead & Profit	10%		\$78.00		\$858.00

Unit Prices:					
9. Unit Price (If Applicable)			\$0.00		\$0.00
Pricing based upon unit prices established in contract.					

Subcontractors:					
10. Subcontractor (Attach Subcontractor Backup)			\$0.00		
11. Overhead & Profit	10%		\$0.00		\$0.00

		Subtotal of Proposal	\$1,845.36
12. Payment & Performance Bond (Enter Bond Rate Percentage)	2%		\$27.68
Total of Change Order Proposal			\$1,873.04

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: *Ad W. R.* Date: 9/27/17

NHM Constructors, LLC
 Asheville Parking Garage
 Change Order: (11) Removal of Trees

<u>Equipment</u>				
<u>Item</u>	<u>Unit Price</u>	<u>Qty.</u>	<u>Unit</u>	<u>Total</u>
Track Hoe	\$ 45.00	6	HR	\$ 270.00
Dump Truck	\$ 85.00	6	HR	\$ 510.00
Pick Up	\$ 18.00	6	HR	\$ 108.00
				<u>\$ 780.00 Total</u>

<u>Labor</u>				
<u>Item</u>	<u>Unit Price</u>	<u>Qty.</u>	<u>Unit</u>	<u>Total</u>
Foreman	\$ 55.00	6	HR	\$ 330.00
Operator	\$ 25.00	6	HR	\$ 150.00
Laborer	\$ 15.00	6	HR	\$ 90.00
Laborer	\$ 15.00	6	HR	\$ 90.00
				<u>\$ 660.00 Total</u>

Change Order Request # 31

BUILDING ON A GREAT TRADITION

4-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #31

Dear Mr. Moody

This Change Order Request is for the additional work to seal coat the Future Taxi Lot. Please note that this is based on 4,320 SY but we have confirmed that the unit price plus OH&P adjustments shown on the attached sheet is still applicable so long as the final amounts are a minimum of 4,000 SY. The unit price would be \$2.70/SY. American South's understanding is that the only seal coating desired is at the Future Taxi Lot as shown on the attached colored C3.2 plan sheet.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
NHM Constructors (Civil Division)	1	LS	10,517.43	10,517.43
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
SUBTOTAL			10,517.43	10,517.43
OH&P			10.00%	1,051.74
SUBTOTAL				11,569.17
BOND			1.0%	115.69
TOTAL CHANGE ORDER PROPOSAL				11,684.86

We have included sheet C3.2 showing our assumption of where this work shall take place along with the appropriate backup documentation provided by our Subcontractor.

Upon approval, please issue a Change Order for the total lump sum amount of **\$11,684.86**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Sitework
Contractor: NHM Constructors, LLC

Proposal #: 12
Project #: 09-163
Date: 10/4/2017

Description of Change: Installation of asphalt seal coat onto existing rental parking lot. Pricing is based off parking lot being roughly 4,320 SY.

Material:

1. Total Cost of Materials (Attach Backup)		\$0.00	
2. Sales Tax	7%	\$0.00	
3. Overhead & Profit	10%	\$0.00	\$0.00

Labor:

4. Total Man Hour Cost (Attach Backup)		\$0.00	
5. Payroll Burden (Enter Percentage)	36%	\$0.00	
6. Overhead & Profit	10%	\$0.00	\$0.00

Equipment:

7. Equipment / Equipment Rental (Attach Backup)		\$780.00	
8. Overhead & Profit	10%	\$78.00	\$858.00

Unit Prices:

9. Unit Price (If Applicable)		\$0.00	\$0.00
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Pricing based upon unit prices established in contract.

Subcontractors:

10. Subcontractor (Attach Subcontractor Backup)		\$8,640.00	
11. Overhead & Profit	10%	\$864.00	\$9,504.00

		Subtotal of Proposal	\$10,362.00
12. Payment & Performance Bond (Enter Bond Rate Percentage)	2%	\$155.43	\$155.43
Total of Change Order Proposal			\$10,517.43

Time Extension Request: _____ Days

Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____

Date: _____

Subcontractor: *Ad W. R.*

Date: *10/4/17*

New Dimensions in Pavement Markings

231 Rutledge Road
 Fletcher, NC 28732
 Voice: 828-654-8181
 Fax: 828-654-7590

PROPOSAL

Proposal Number: 17583
 Proposal Date: Sep 29, 2017

To:
 NHM Constructors LLC
 P.O. Box 6385
 Asheville, NC 28816

Location:
 Airport Lot Sealing

Payment Terms
 Net 30 Days

Quantity	Item	Description	Unit Price	Amount
4,000.00 4,320	300	Asphalt sealcoating: Clean entire surface of asphalt. Sealcoat throughout with a 4 lbs sand/gallon mix	2.00	8,000.00 \$ 8,640.00
107.00	202	Parking stalls	10.00	1,070.00
200.00	200	4" Painted Pavement Markings	0.40	80.00
			TOTAL	\$ 8,640.00

Subtotal	9,150.00
Sales Tax	
Freight	0.00
TOTAL PROPOSAL AMOUNT	9,150.00

This proposal is based on the work as specified to our company via blueprint, email, meeting, or on-site review. Any and all work required above and beyond the above referenced items will be invoiced at a per item price. Unless otherwise noted, this proposal covers ONE mobilization to complete this work. This proposal will be valid for 60 days beginning with the proposal date. If it is not accepted prior to then the pricing will be subject to re-evaluation as necessary.

I have read and agree to the terms of this proposal and the work quoted herein

Signed _____

Date _____

Drawn By: DWS
 Checked By: RWI
 Approved By: RWM



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft. Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710



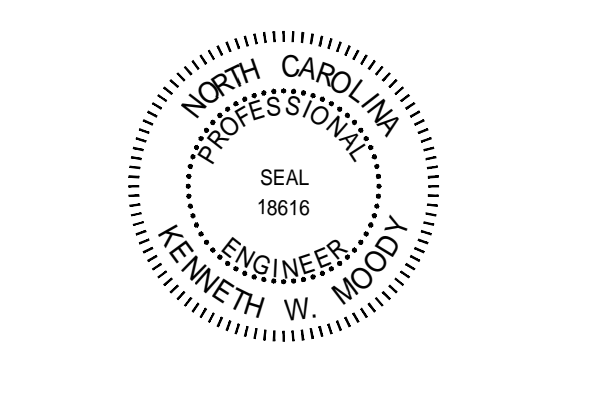
Asheville Regional Airport Parking Garage

Greater Asheville Regional Airport Authority

61 Terminal Drive, Suite 1
 Fletcher, NC 28732

DELTA AIRPORT CONSULTANTS, INC.
 1121 Carmel Commons Blvd., Suite 435
 Charlotte, NC 28226
 704.521.9101
 Delta Project No. 15086

WALKER PARKING CONSULTANTS

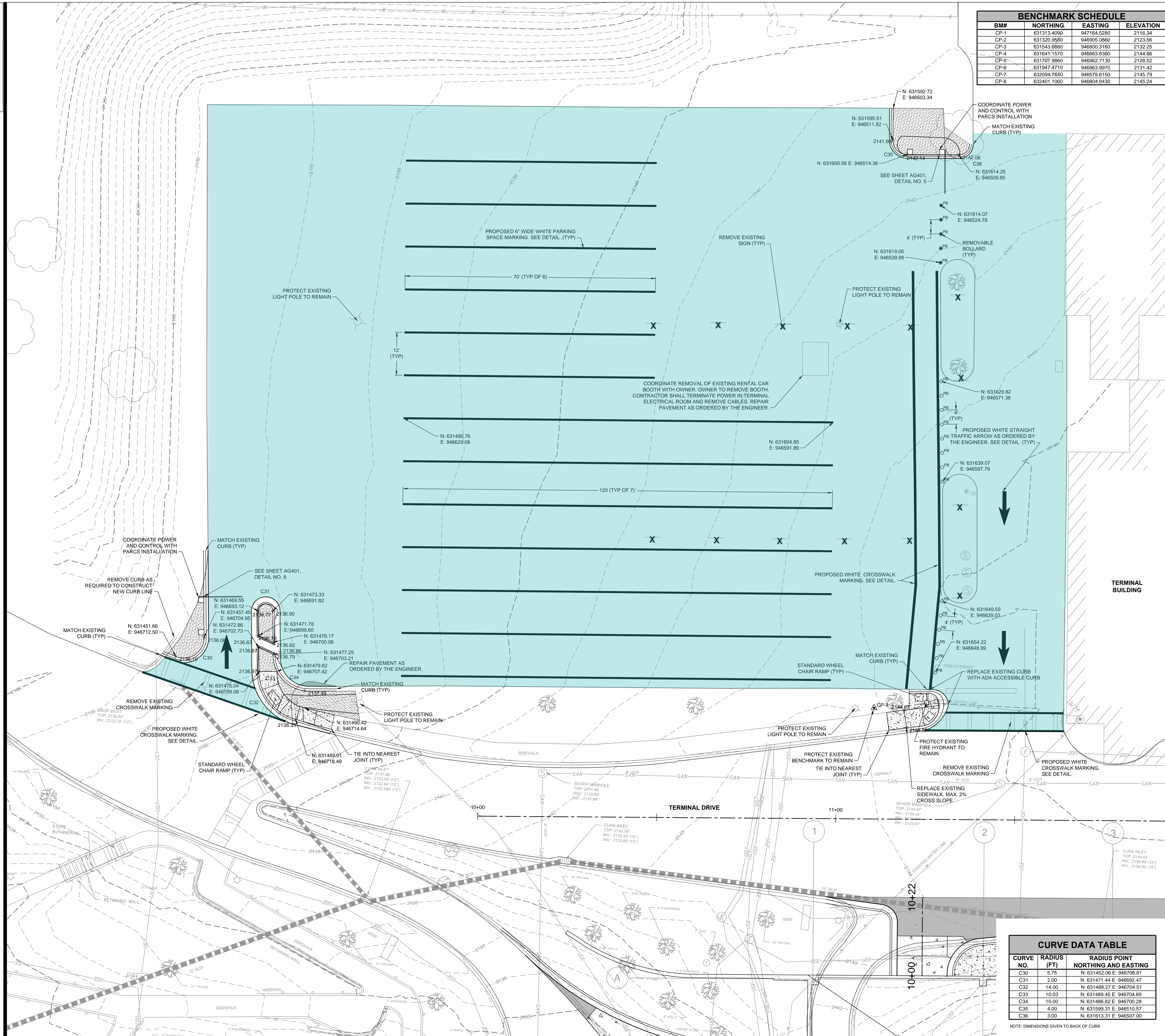


BENCHMARK SCHEDULE			
BM#	NORTHING	EASTING	ELEVATION
CP-1	631313.4090	947164.5280	2116.34
CP-2	631320.9580	946905.0860	2132.56
CP-3	631543.6880	946800.3160	2132.25
CP-4	631641.1570	946663.8360	2144.86
CP-5	631707.8960	946962.7130	2128.52
CP-6	631947.4710	946963.9970	2131.42
CP-7	632094.7640	946579.6150	2145.79
CP-8	632401.1060	946804.9430	2145.24



LEGEND

	BENCHMARK LOCATION
	BORING LOCATION (APPROX)
	EXISTING SPOT ELEVATION
	PROPOSED SPOT ELEVATION
	EXISTING DROP INLET
	EXISTING ENDWALL/HEADWALL/WINGWALL
	EXISTING STORM MANHOLE
	EXISTING FIRE HYDRANT
	EXISTING POLE (LIGHTED)
	EXISTING TREE
	EXISTING ROADWAY SIGN
	EXISTING ROADWAY SIGN TO BE REMOVED
	EXISTING POLE (LIGHTED)
	PROPOSED BOLLARD
	PROPOSED REMOVABLE BOLLARD
	PROPOSED CURB & GUTTER
	EXISTING DITCH
	EXISTING UNDERGROUND ELECTRICAL LINE
	EXISTING FENCE
	PROPOSED FENCE
	PROPOSED SILT FENCE
	EXISTING FIBER OPTIC LINE
	EXISTING UNDERGROUND TELEPHONE LINE
	EXISTING WATER LINE
	PROPOSED PAVEMENT REMOVAL
	PROPOSED PCC SIDEWALK
	PROPOSED BITUMINOUS PAVEMENT
	PROPOSED GRASS/LANDSCAPE AREA



CURVE DATA TABLE

CURVE NO.	RADIUS (FT)	RADIUS POINT NORTHING AND EASTING
C30	5.75	N: 631452.06 E: 946706.81
C31	2.00	N: 631471.44 E: 946892.47
C32	14.00	N: 631488.27 E: 946704.51
C33	10.03	N: 631489.45 E: 946704.89
C34	10.00	N: 631486.82 E: 946700.28
C35	4.00	N: 631599.31 E: 946510.57
C36	3.00	N: 631613.31 E: 946507.00

NOTE: DIMENSIONS GIVEN TO BACK OF CURB

- NOTES**
- NORTHING AND EASTING COORDINATES ARE GIVEN TO THE BACK OF CURB.
 - NORTHING AND EASTING COORDINATES ARE BASED ON NORTH CAROLINA STATE PLANE GRID (NAD 83).
 - THE CONTRACTOR SHALL FIELD STAKE ALL CURB LINES PRIOR TO CONSTRUCTION.
 - REMOVE ALL SIGNAGE, PARKING EQUIPMENT, ETC. ASSOCIATED WITH RENTAL CAR COMPANIES AND RETURN TO OWNER.



Revision

No.	Date	Description
	09/21/16	CONFORMED CONTRACT DOCUMENTS
1	04/07/17	REVISED GROUND TRANSPORTATION LOT
2	07/05/17	REVISED PEDESTRIAN WALKWAYS
3	07/13/17	REVISED BOLLARDS

GROUND TRANSPORTATION LOT IMPROVEMENTS

C3.2

PROJECT: 41152.00
 DATE: 23 MAY 2016

12/18/2015 11:06:35 AM

CHANGE ORDER NO. EIGHT (8)

PROJECT (Name and address):

Construct Parking Garage
Asheville Regional Airport
61 Terminal Drive, Suite 1
Fletcher, NC 28732

CHANGE ORDER NUMBER: 008

DATE: 11/02/2017

OWNER

PRIME CONSULTANT

CONTRACTOR

FIELD

OTHER

DELTA PROJECT NO: 15086

TO CONTRACTOR (Name and address):

American South General Contractors
60 N. Merrimon Ave. #101
Asheville, NC 28804

CONTRACT DATE: September 6, 2016

CONTRACT FOR:

General Construction of Parking Garage

THE CONTRACT IS CHANGED AS FOLLOWS:

(Include, where applicable, any undisputed amount attributable to previously executed Construction Change Directives)

COR #32	Elevator "Cants"	\$ 1,721.63 ADD
COR #34	Aluminum caps on metal panel tubes	\$ 7,315.38 ADD
COR #35	PARCS System Conduit and Cable	\$ 27,326.89 ADD
COR #36	Stop signs for Garage Exits	\$ 1,111.62 ADD
COR #37	Painting of Stair Towers B&C, Communication Rooms, and Electrical Room	\$ 18,225.31 ADD
COR #38R	Herbicide and weed barrier for river rock bed at Generator	\$ 286.99 ADD
TOTAL:		\$ 55,987.82 ADD

Terms and Conditions: In consideration of this modification agreed to herein as complete equitable adjustments, the Contractor hereby releases the Owner from any and all liability under this Contract for further equitable adjustments as it directly or indirectly relates to all items enumerated in this Contract Modification. This Contract Modification represents full and final accounting for all direct and indirect costs associated with the Work described above, both in terms of cost and time.

REASON FOR CHANGE ORDER(S):

COR #32 - Additional work to add cants to the tops of elevator / curtainwall beams that are greater than 4" wide, per requirement of Elevator Inspector.

COR #34 - Addition of aluminum caps to the tops and bottoms of the metal panel tubes.

COR #35 - Additional conduit and wire to accommodate TIBA PARCS.

COR #36 - Three stop signs added at Garage Exits.

COR #37 - Painting of Stair Towers B&C, east walls of the Communication Rooms on 2nd and 4th floor, and the south wall of the Electrical Room on ground floor.

COR #38R - Addition of herbicide and a weed barrier to the river rock bed around the generator.

The original Contract Sum was	\$ 20,244,000.00
The net change by previously authorized Change Orders	\$ 389,488.13
The Contract Sum prior to this Change Order was	\$ 20,633,488.13
The Contract Sum will be increased by the Change Order in the amount of	\$ 55,987.82
The new Contract Sum including this Change Order will be	\$ 20,689,475.95

CHANGE ORDER NO. EIGHT (8)

NOTE: This Change Order does not include changes in the Contract Sum, Contract Time or Guaranteed Maximum Price which have been authorized by Construction Change Directive until the cost and time have been agreed upon by both the Owner and Contractor, in which case a Change Order is executed to supersede the Construction Change Directive.

NOT VALID UNTIL SIGNED BY THE PRIME CONSULTANT, CONTRACTOR AND OWNER.

<u>Delta Airport Consultants</u> PRIME CONSULTANT <i>(Company name)</i>	<u>American South General Contractors</u> CONTRACTOR <i>(Company name)</i>	<u>Greater Asheville Regional Airport Authority</u> OWNER <i>(Company name)</i>
<u>11111 Carmel Commons Blvd., Ste 435</u> Charlotte, NC 28226 ADDRESS	<u>60 N. Merrimon Ave. #101</u> Asheville, NC 28804 ADDRESS	<u>61 Terminal Drive, Suite 1</u> Fletcher, NC 28732 ADDRESS
<u>BY</u> <i>(Signature)</i>	<u>BY</u> <i>(Signature)</i>	<u>BY</u> <i>(Signature)</i>
<u>Kenneth W. Moody, P.E., C.M.</u> <i>(Typed Name)</i>	<u>Bill Morgan, Vice President</u> <i>(Typed Name)</i>	<u>Lew Bleiweis / Executive Director</u> <i>(Typed Name)</i>
<u>11/02/2017</u> DATE	<u>DATE</u>	<u>DATE</u>

Change Order Request # 32

BUILDING ON A GREAT TRADITION

23-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #32

Dear Mr. Moody

This Change Order Request is for the additional work and material required to add "Cants" to the tops of the elevator/curtainwall beams that are greater than 4" wide. The Elevator Inspector is requiring these at the tube steel beams that support both the curtainwall and elevator divider beams. This appears to be an interpretation as American South argued that these beams also support the divider beams. However, it was stated that the divider beams could have been installed vertically versus horizontally as designed.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Benton Roofing	1	LS	1,299.62	1,299.62
Kone Elevators	2	HR	125.00	250.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			1,424.62	1,549.62
OH&P			10.00%	154.96
<hr/>				
SUBTOTAL				1,704.58
BOND			1.0%	17.05
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				1,721.63

We have included backup information provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$1,721.63**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck **Proposal #:** 3
Bid Package: _____ **Project #** 09-163
Contractor: Benton Roofing **Date:** 10/23/2017

Description of Change: Furnish and install triangle tops on the the beams in the elevator shaft

Material: **Subtotals**

1. Total Cost of Materials (Attach Backup)		<u>\$250.00</u>	
2. Sales Tax	7%	<u>\$17.50</u>	
3. Overhead & Profit	10%	<u>\$26.75</u>	<u>\$294.25</u>

Labor:

4. Total Man Hour Cost (Attach Backup)		<u>\$275.00</u>	
5. Payroll Burden (Enter Percentage)	0%	<u>\$0.00</u>	
6. Overhead & Profit	10%	<u>\$27.50</u>	<u>\$302.50</u>

Equipment:

7. Equipment / Equipment Rental (Attach Backup)		<u>\$0.00</u>	
8. Overhead & Profit	10%	<u>\$0.00</u>	<u>\$0.00</u>

Unit Prices:

9. Unit Price (If Applicable)	<u>Admin Fee</u>	<u>\$250.00</u>	<u>\$250.00</u>
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Subcontractors:

10. Subcontractor (Attach Subcontractor Backup)		<u>\$400.00</u>	
11. Overhead & Profit	10%	<u>\$40.00</u>	<u>\$440.00</u>

		Subtotal of Proposal	\$1,286.75
12. Payment & Performance Bond (Enter Bond Rate Percentage)		1%	<u>\$12.87</u>
		Total of Change Order Proposal	<u>\$1,299.62</u>

Time Extension Request: _____ Days

Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: 10/23/2017
ASGC

Subcontractor: _____ Date: 10/23/2017
Benton Roofing



site storage (\$1,600/month per unit) and/or labor for double handling of the materials (\$4,000). Additionally, this Proposal is based upon the ability of KONE to unload materials at the job site and begin installation at that time. If these items are not in place at time of delivery, KONE may not be available to begin the installation for up to an additional 6 weeks (after site preparation is complete) due to labor re-allocation. Any labor associated with relocation of material (once stored onsite) is not included in this Proposal.

OPERATOR TIME: KONE is unable to estimate the amount of operator time needed by other trades. Unless a specific amount is specified elsewhere in this proposal, KONE has not included any operator time. If the General Contractor (or other subcontractors) requires access to the shaft or the use of the elevator platform for any reason (prior to Final Acceptance – after which, standard KONE service rates will apply), KONE will provide an operator for an additional charge of \$125/hour (straight time) or \$250/hour (OT). Availability of an operator will be determined at the time of the request. The agreed schedule shall be extended by the time needed by other trades for access to the shaft.

TEMPORARY USE: Our proposal does not include any temporary use of the elevator. Should temporary use be required, a monthly fee of \$1,100/elevator, plus a fee of \$250 for each temporary inspection will be applicable. A fee of \$2,000 for screening the hoistway (only applicable on multi-car hoistways) and any costs for readjusting/refurbishment is required. In addition, you and/or the General Contractor will protect the elevators, provide electrical service, a temporary car enclosure, protection of elevator hoistway openings, an operator (if needed) and clean down and turnover the elevator in a "like new" condition upon completion of temporary use. Furthermore, the KONE Temporary Use Form shall be executed before any elevator is placed into temporary service. KONE requires 1-2 weeks of uninterrupted use of the elevators prior to final turnover.

HOISTWAY CLEANING: KONE is unable to estimate the cleanliness of an elevator hoistway on a construction site, as the amount of debris/dust is dependent on work completed by other trades within the building. As such, KONE has not included any costs for clean down of the elevator shaft, but can provide a price if conditions warrant.

OTHER TRADE WORK: KONE is unable to estimate the amount of on-site mechanic labor required to coordinate the work of other trades. Thus, KONE has not included any additional time and/or costs (outside of the equipment installation and inspection time) for coordination with the life safety system, security system, or any other trades. KONE shall be entitled to an extension of time and/or additional costs incurred by additional time expended for coordination with other trades.

PHONE: We are offering our standard, one hands-free ADA compliant speakerphone per cab; it will automatically dial to a determined location. A KONE KRMS monitoring agreement must be completed, (either accepting or denying KONE's monitoring service) 2 weeks prior to inspections.

Drawn By: Author
 Checked By: Checker
 Approved By: Approver

KEYNOTES	
034100.07	PRECAST STRUCTURAL CONCRETE WALL PANEL
084229.01	SLIDING AUTOMATIC ENTRANCE
084413.01	GLAZED ALUMINUM CURTAIN WALL SYSTEM

GLASS TYPE LEGEND	
1	FGL-1 = GLASS TYPE 1, FIRE RATED
2	FGL-2 = GLASS TYPE 2, FIRE RATED
3	FGL-3 = GLASS TYPE 3, FIRE RATED
4	GL-4 = GLASS TYPE 4
5	GL-5 = GLASS TYPE 5
6	GL-6 = GLASS TYPE 6, INSULATED UNIT
7	GL-7 = GLASS TYPE 7, INSULATED UNIT
8	GL-8 = GLASS TYPE 8, INSULATED UNIT
9	GL-9 = GLASS TYPE 9, INSULATED UNIT

REFER TO SPECIFICATIONS SECTIONS 08 8000 AND 08 8913 FOR GLASS TYPE DESCRIPTIONS.



GRESHAM SMITH AND PARTNERS

Design Services For The Built Environment

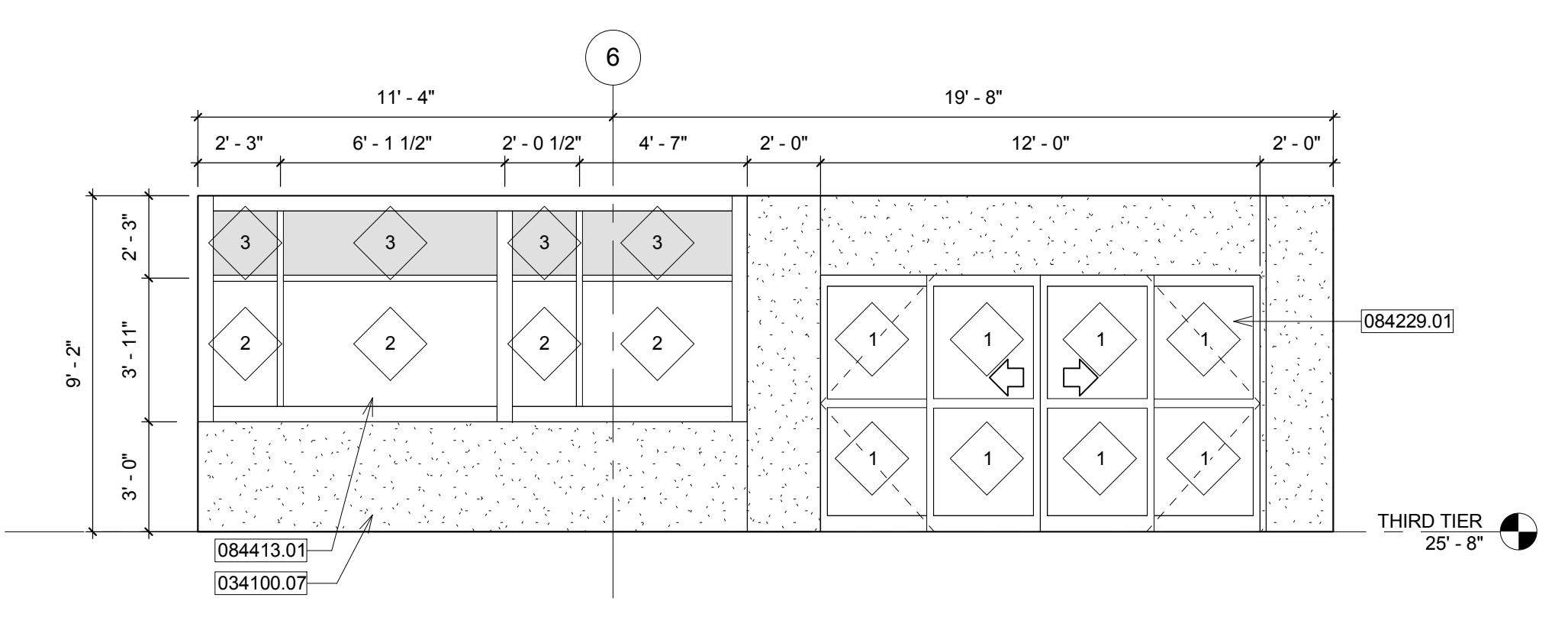
Atlanta GA Jacksonville FL
 Birmingham AL Knoxville TN
 Charlotte NC Louisville KY
 Cincinnati OH Memphis TN
 Columbus OH Miami FL
 Dallas TX Nashville TN
 Ft Lauderdale FL Richmond VA
 Houston TX Tallahassee FL
 Jackson MS Tampa FL

10 South 6th Street, Suite 100
 Richmond, VA 23219
 804.788.0710

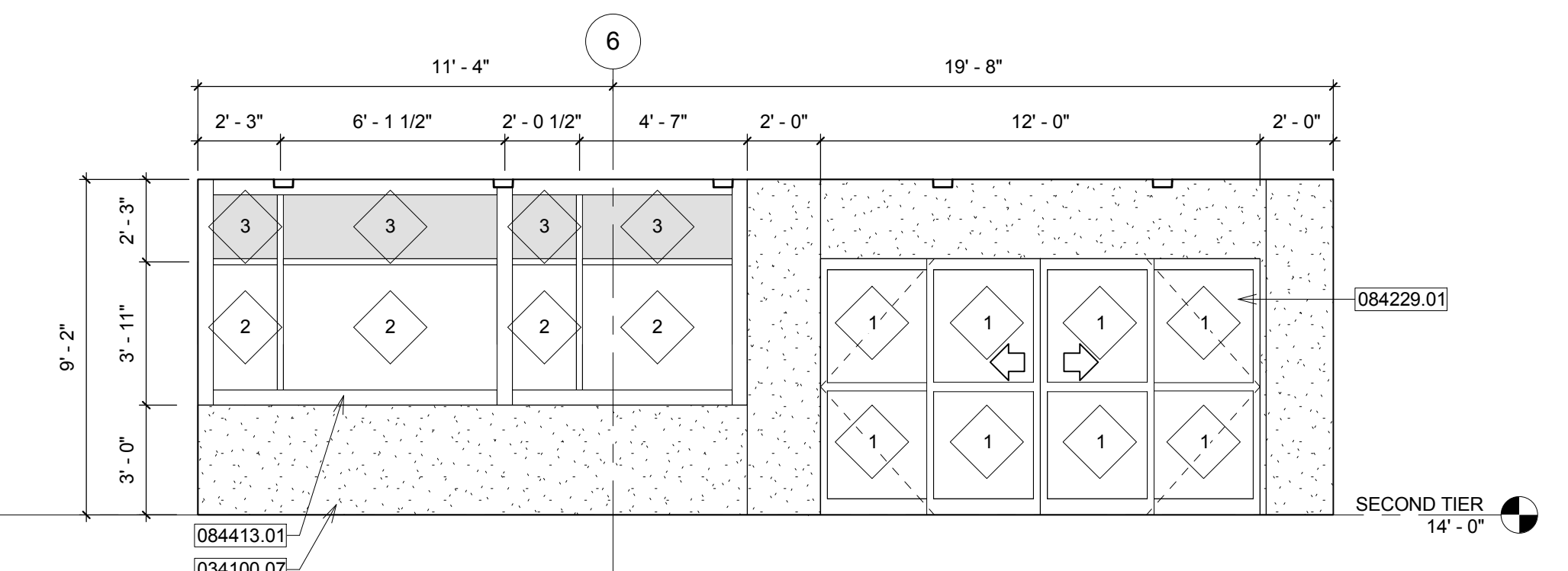


Asheville Regional Airport Parking Garage

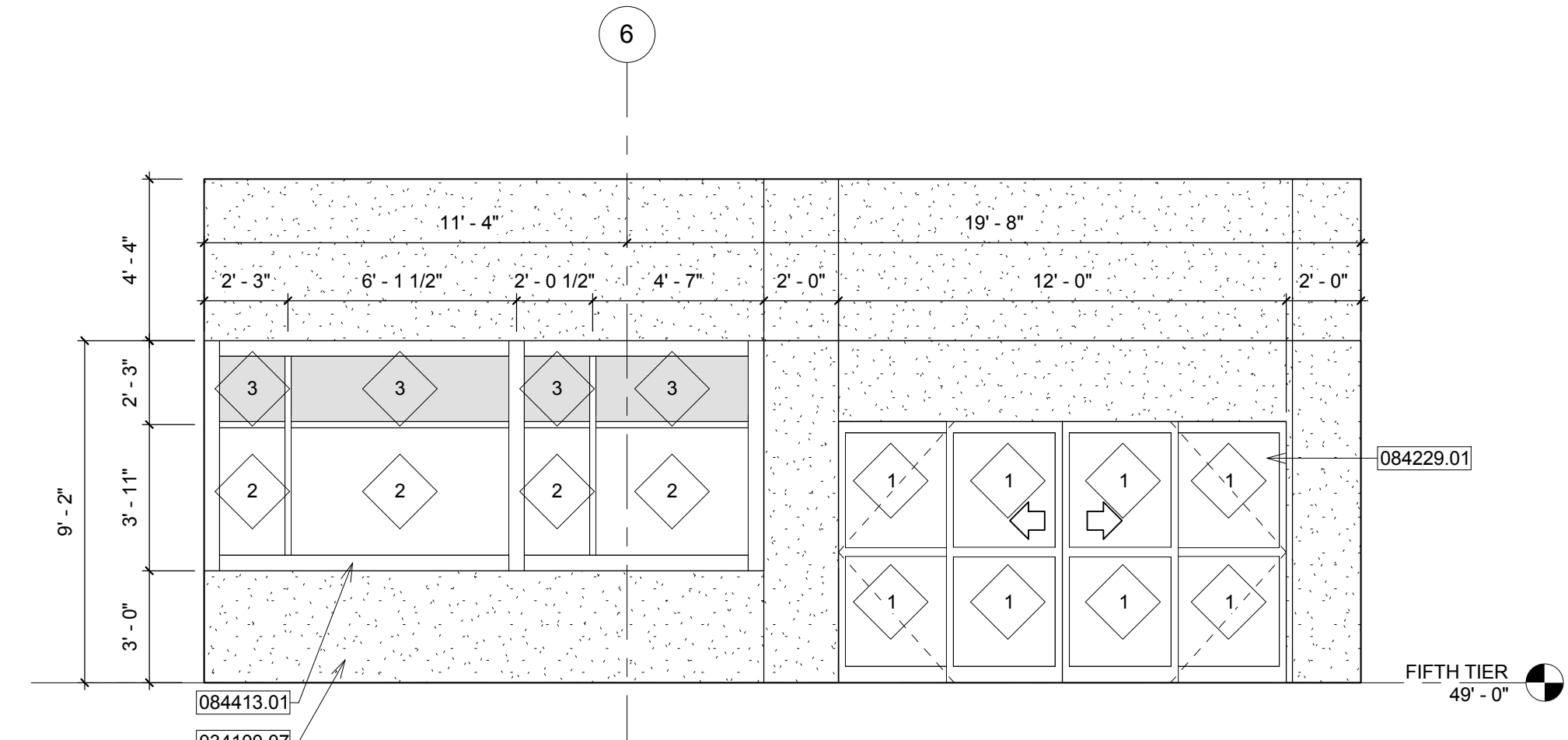
Greater Asheville Regional Airport Authority
 61 Terminal Drive, Suite 1
 Fletcher, NC 28732



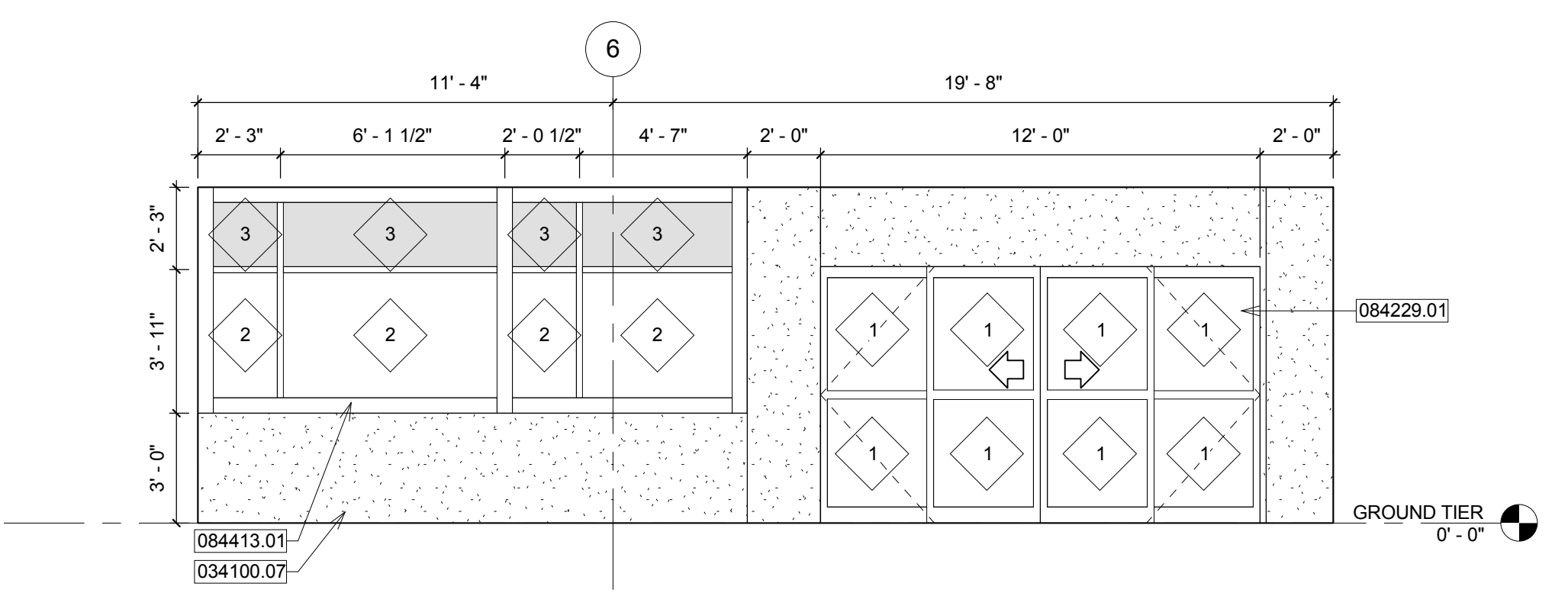
3 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - LEVEL 3 - EAST
 1/4" = 1'-0"



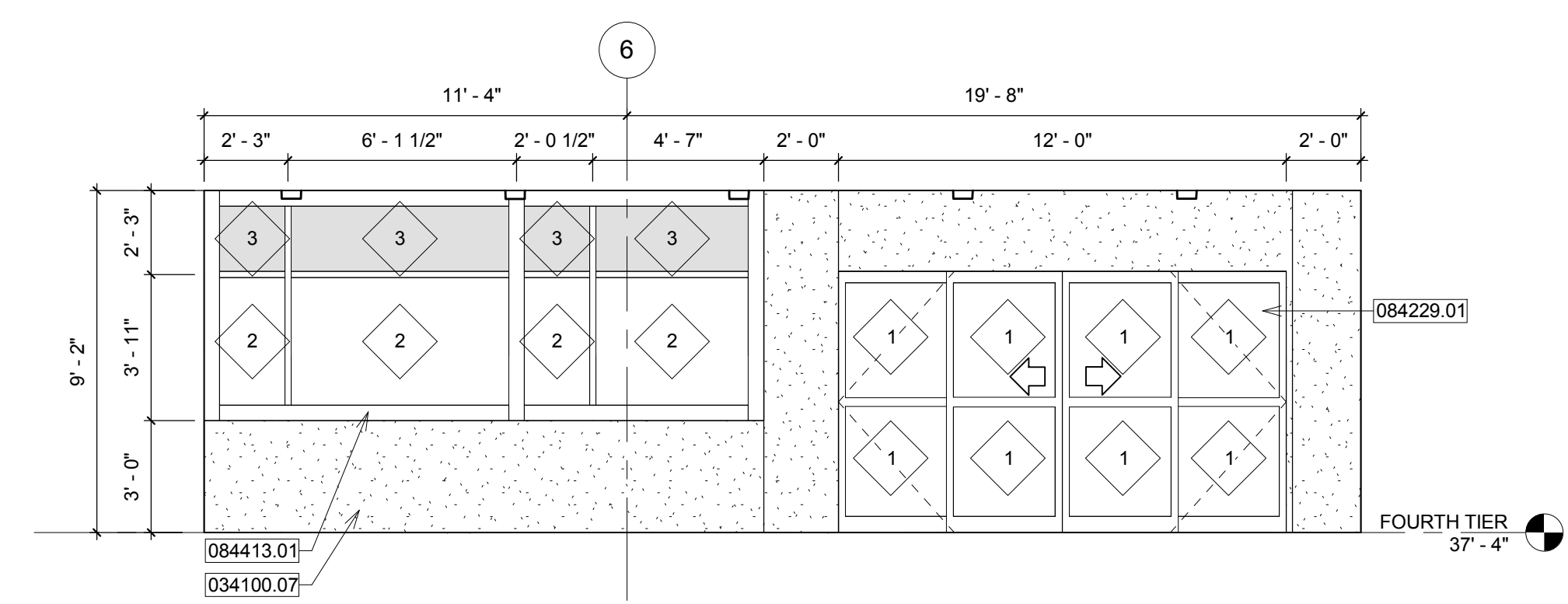
2 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - LEVEL 2 - EAST
 1/4" = 1'-0"



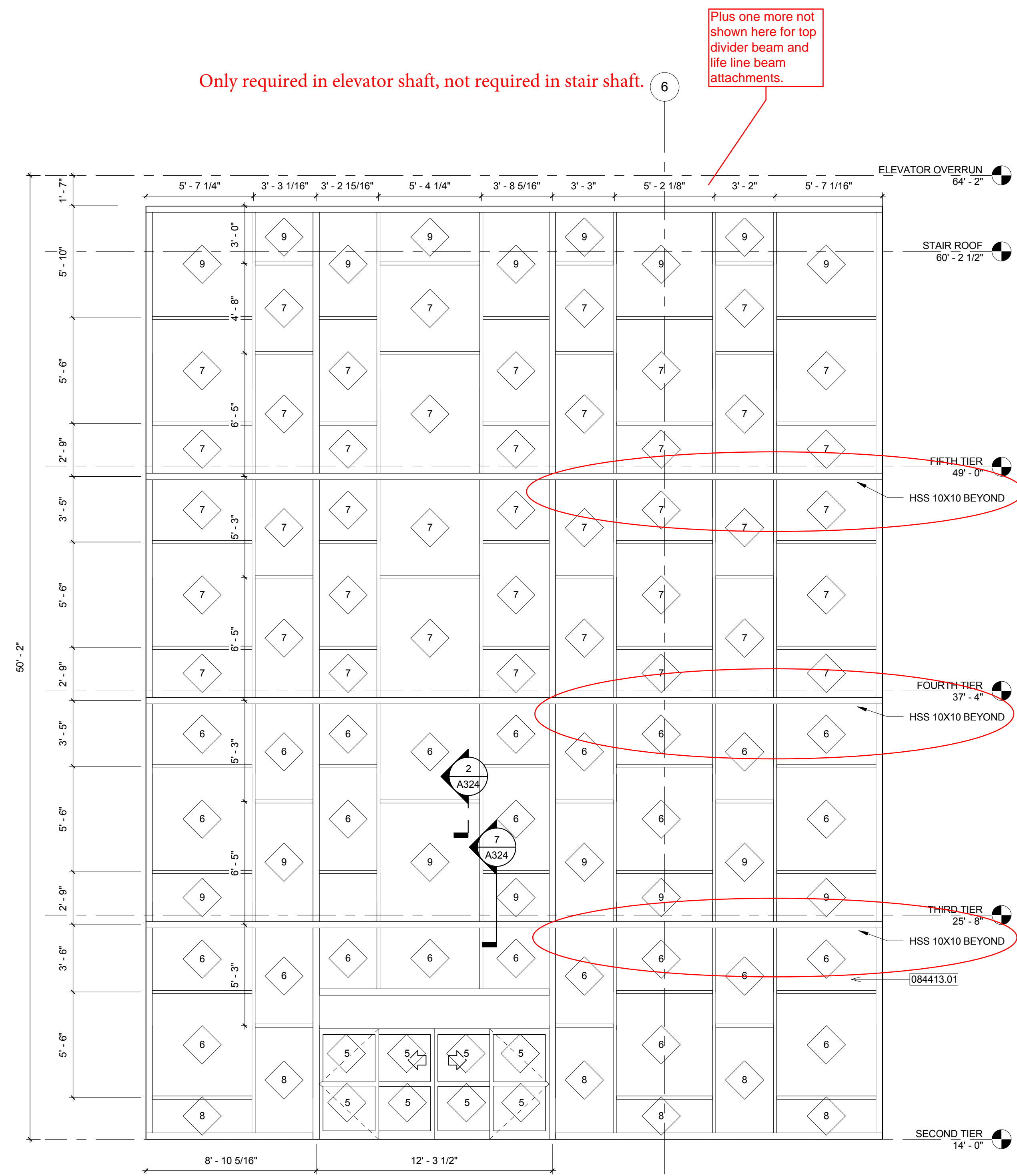
5 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - LEVEL 5 - EAST
 1/4" = 1'-0"



1 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - LEVEL 1 - EAST
 1/4" = 1'-0"



4 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - LEVEL 4 - EAST
 1/4" = 1'-0"



6 CURTAIN WALL ELEVATION - STAIR A/ELEVATOR - WEST
 1/4" = 1'-0"

Only required in elevator shaft, not required in stair shaft.

Plus one more not shown here for top divider beam and life line beam attachments.

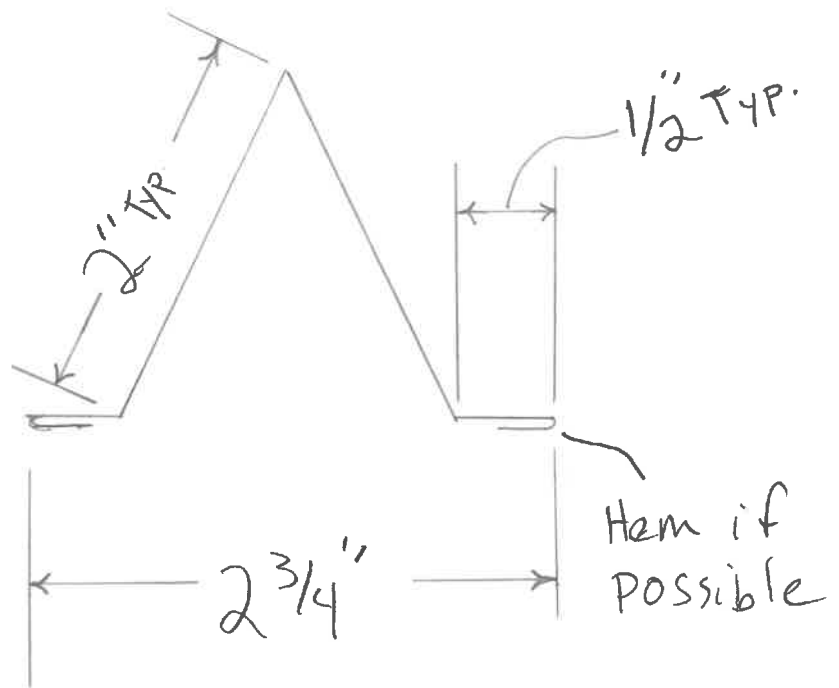
Revision		
No.	Date	Description

ENLARGED ELEVATIONS - CURTAIN WALLS

A304

PROJECT: 41152.00
 DATE: 21 SEPTEMBER 2016

80 Linear Feet: 4 Beams @ 17 LF. Stock material comes in 10' sections



1" = 1"

200" long not counting overlap

Black

4 Beams covers total

Change Order Request # 34

BUILDING ON A GREAT TRADITION

10.23.17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #34

Dear Mr. Moody

This Change Order Request is for the additional work to add aluminum caps to the tops and bottoms of the metal panel tubes. Samples have already been given to Mike Reisman and an additional third anchor attachment is provided along with a week hole for the bottom caps.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
American South General Contractors	1	LS	6,584.50	6,584.50
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			6,584.50	6,584.50
OH&P			10.00%	658.45
SUBTOTAL				<hr/> 7,242.95
BOND			1.0%	72.43
TOTAL CHANGE ORDER PROPOSAL				<hr/> 7,315.38

We have included appropriate backup documentation provided by our supplier.

Upon approval, please issue a Change Order for the total lump sum amount of **\$7,315.38**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: General Contractor
Contractor: American South General Contractors Inc.

Proposal #: 34
Project # 09-163
Date: _____

Description of Change: Price to add aluminum caps to the bottom and tops of the metal panel tubes.
This includes labor and material and assumes three (3) screws per cap only. The OH&P and Bond have
been omitted from this sheet as it will be carried on the overall COR form.

Material:	Screws & Caps	Subtotals
1. Total Cost of Materials (Attach Backup)	\$3,899.36	
2. Sales Tax	7% <u>\$272.96</u>	
3. Overhead & Profit	10% <u> </u>	<u>\$4,172.32</u>
Labor:	268 Pieces: 6 Pieces Per Hour = 44.67 Hours x 2 Men = 89.34 Man Hours @ \$20.00/HR	
4. Total Man Hour Cost (Attach Backup)	89.34 MH <u>\$1,786.80</u>	
5. Payroll Burden (Enter Percentage)	35% <u>\$625.38</u>	
6. Overhead & Profit	10% <u> </u>	<u>\$2,412.18</u>
Equipment:		
7. Equipment / Equipment Rental (Attach Backup)	<u> </u>	
8. Overhead & Profit	10% <u>\$0.00</u>	<u>\$0.00</u>
Unit Prices:		
9. Unit Price (If Applicable)	<u> </u>	<u>\$0.00</u>
Subcontractors:		
10. Subcontractor (Attach Subcontractor Backup)	<u> </u>	
11. Overhead & Profit	10% <u>\$0.00</u>	<u>\$0.00</u>
	Subtotal of Proposal	<u>\$6,584.50</u>
12. Payment & Performance Bond	<u> </u>	<u>\$0.00</u>
(Enter Bond Rate Percentage)		
	Total of Change Order Proposal	<u>\$6,584.50</u>

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: _____ Date: _____



Thursday, October 19, 2017

To: American South Contractors
Attn: Seth Chambers
Job: Asheville Airport Parking Deck Revision #1
Items: Quote for quantity of 268 aluminum caps

Material for caps: 1/8" thick Aluminum
Quantity of four top caps to fit over 2" x 6" aluminum tubing
Quantity of 130 top caps to fit over 4 3/4" x 6" aluminum tubing
Quantity of four bottom caps same as 2" x 6" top cap with additional weep hole centered
Quantity of 130 bottom caps same as 4 3/4" x 6" top cap with additional weep hole centered

Cap description:

- 3/4" bends on 3 sides
- All 3/4" bends will have one 0.201 clearance hole (no counter sink) centered in length and height of bend
- 134 bottom caps will have 0.201 diameter weep hole centered in profile
- folded corners at connecting legs will be tight with smooth finish (see sample)
- corners will not be welded
- caps will be non-painted and neat in appearance
- mounting screws or bolts by others
- proto type supplied in advance

Price: \$14.30 EACH x 268 = \$3,832.40

Caps will be available for pick 10 days after receipt of order.

Any questions contact Eric Henken or Jeff Schandavel at 82-645-4821
Or Email: eric@karpensteel.com
jeff@karpensteel.com

Thank you!
Jeff M Schandavel
Engineering Department
Karpén Steel Custom Doors & Frames



181 Reems Creek Road ▪ Weaverville, NC 28787
828▪645▪4821 Phone ▪ 800▪851▪2131 Fax
www.karpensteel.com



181 Reems Creek Road ▪ Weaverville, NC 28787
828▪645▪4821 Phone ▪ 800▪851▪2131 Fax
www.karpénsteel.com

START ON YOUR DREAM KITCHEN WITH LOWE'S TODAY >



Open until 10PM!
Weaverville Lowe's

Prices, promotions, styles, and availability may vary. Our local stores do not honor online pricing. Prices and availability of products and services are subject to change without notice. Errors will be corrected where discovered, and Lowe's reserves the right to revoke any stated offer and to correct any errors, inaccuracies or omissions including after an order has been submitted.

12 Bags Required

Hillman 75 Count #8 x 1-in Zinc-Plated Self-Drilling Phillips-Drive Interior/Exterior Standard (SAE) Sheet Metal Screws

Item # 121970 Model # 35265

★★★★★ (1 Review)



\$5.58

Screw Length (Inches)

1.0

Package Quantity

75

- 1 + ADD TO CART

SAVE SHARE

FREE Store Pickup

13 available today at Weaverville Lowe's!

Shipping & Delivery

Available!

Aisle 16 , Bay 7

In-Stor CHAT WITH LOWE'S

Change Order Request # 35

BUILDING ON A GREAT TRADITION

24-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #35

Dear Mr. Moody

This Change Order Request is for the additional work associated with the added conduit and wire called for by the new TIBA layout drawings. What was on the original bid documents is shown in green on the attached drawings and all of the new work is shown in red.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Fountain Electric Services (All Associated Electrical Work)	1	LS	24,596.66	24,596.66
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			24,596.66	24,596.66
OH&P			10.00%	2,459.67
SUBTOTAL				<hr/> 27,056.33
BOND			1.0%	270.56
TOTAL CHANGE ORDER PROPOSAL				<hr/> 27,326.89

We have included the new TIBA Layout drawings as well as the appropriate backup documentation received from our subcontractor.

Upon approval, please issue a Change Order for the total lump sum amount of **\$27,326.89**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: <u>Asheville Regional Airport Parking Deck</u>	Proposal #: <u>13</u>
Bid Package: <u>Electrical</u>	Project #: <u>09-163</u>
Contractor: <u>Fountain Services, LLC</u>	Date: _____

Description of Change: Add conduits and wire for additional requirements for PARCS system changes.

Material:						Subtotals
	1. Total Cost of Materials (Attach Backup)			\$3,432.59		
	2. Sales Tax	7%		\$240.28		
	3. Overhead & Profit	10%		\$367.29		<u>\$4,040.16</u>
Labor:						
	4. Total Man Hour Cost (Attach Backup)			\$6,186.34		
	5. Payroll Burden (Enter Percentage)	0%		\$0.00		
	6. Overhead & Profit	10%		\$618.63		<u>\$6,804.97</u>
Equipment:						
	7. Equipment / Equipment Rental (Attach Backup)			\$1,330.00		
	8. Overhead & Profit	10%		\$133.00		<u>\$1,463.00</u>
Unit Prices:						
	9. Unit Price (If Applicable)					<u>\$0.00</u>
Subcontractors:						
	10. Subcontractor (Attach Subcontractor Backup)			\$10,950.00		
	11. Overhead & Profit	10%		\$1,095.00		<u>\$12,045.00</u>
				Subtotal of Proposal		\$24,353.13
	12. Payment & Performance Bond			1%		<u>\$243.53</u>
	(Enter Bond Rate Percentage)					
				Total of Change Order Proposal		<u>\$24,596.66</u>

Time Extension Request: _____ Days
Schedule Activity # Affected _____ Does this effect the critical path? _____

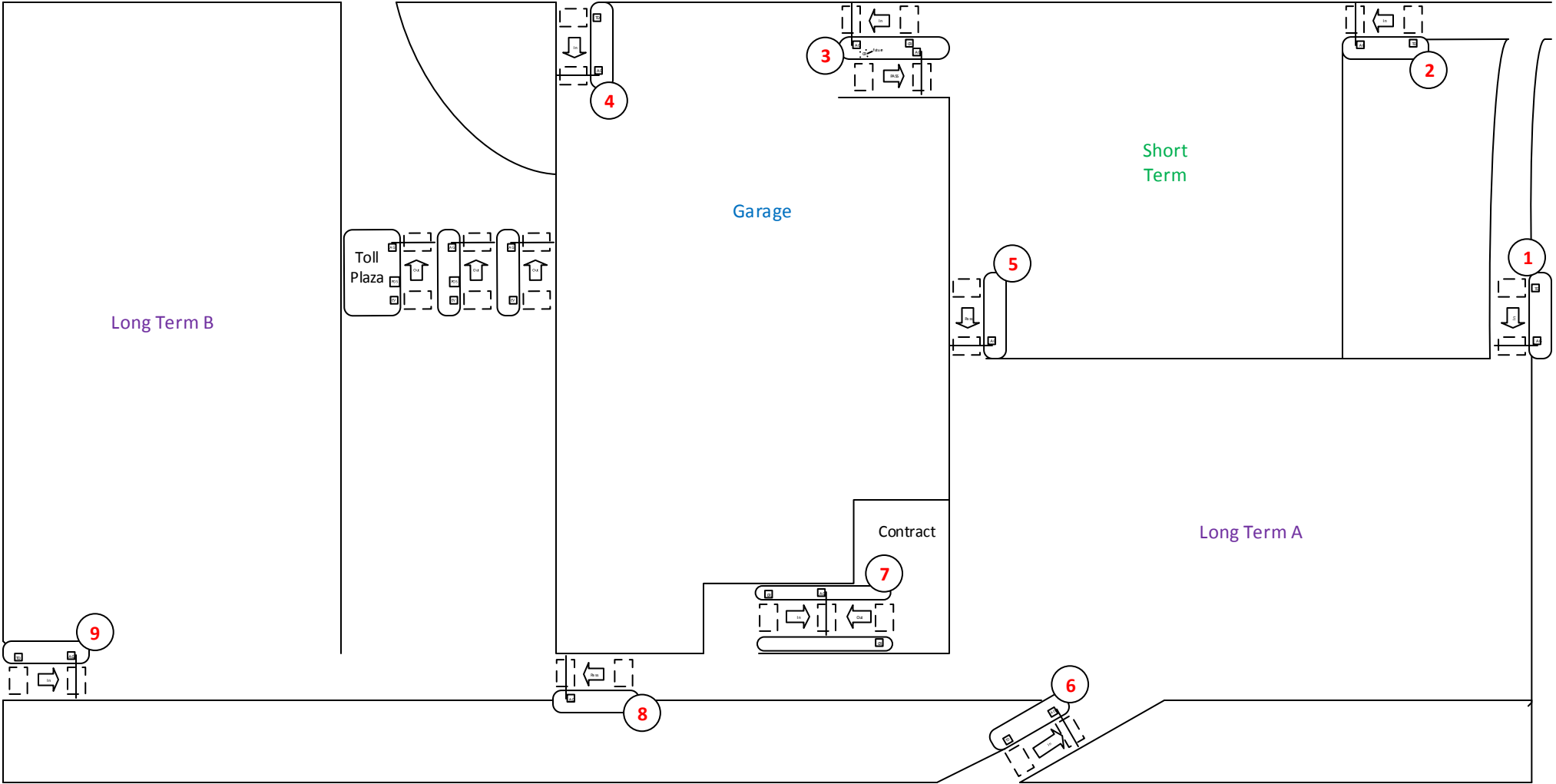
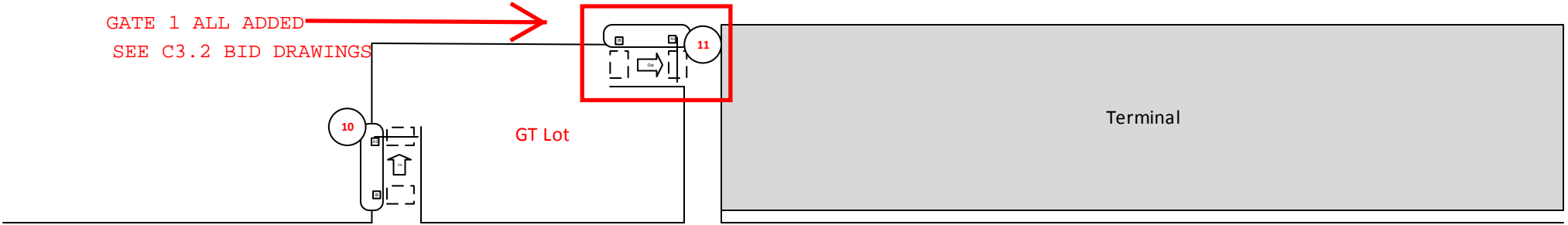
This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

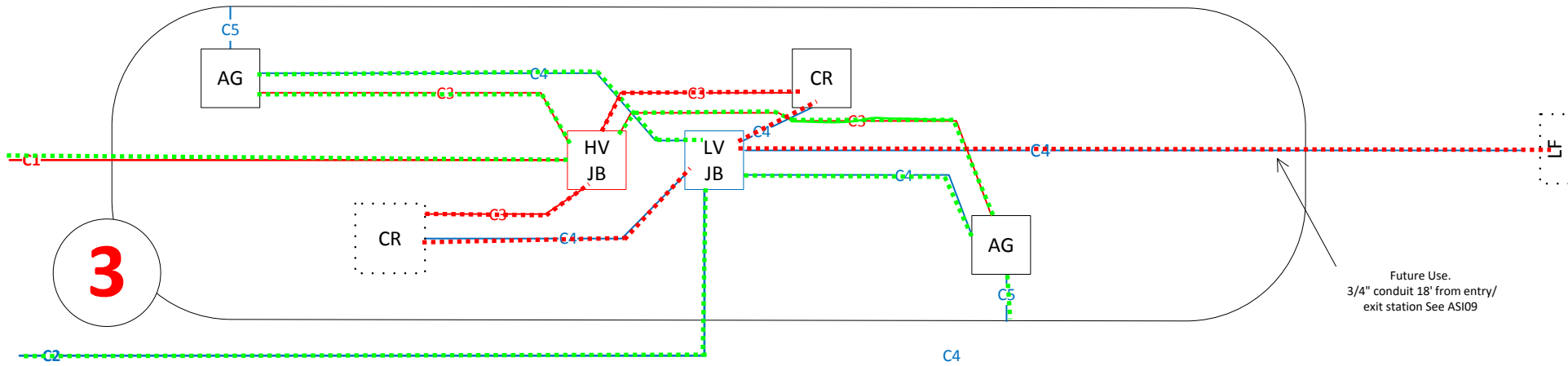
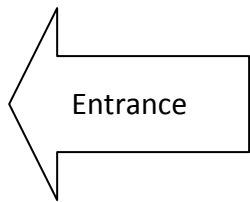
Construction Manager: _____ Date: _____
 Subcontractor: _____ Date: _____

electrical mechanic	47	114.62	5387.14
foreman	58	11.4	661.2
project manager	69	2	138

6186.34

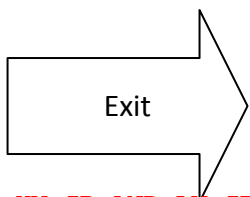
GATE 1 ALL ADDED
SEE C3.2 BID DRAWINGS





LINES IN GREEN IN BID

LINES IN RED NEW



C3 = 20' OF 1"

C4 = 20' OF 1"

2 = HINGED JUNCTION BOXES HV JB AND LV JB

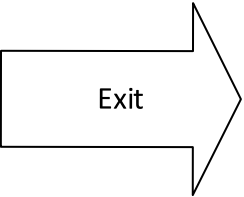
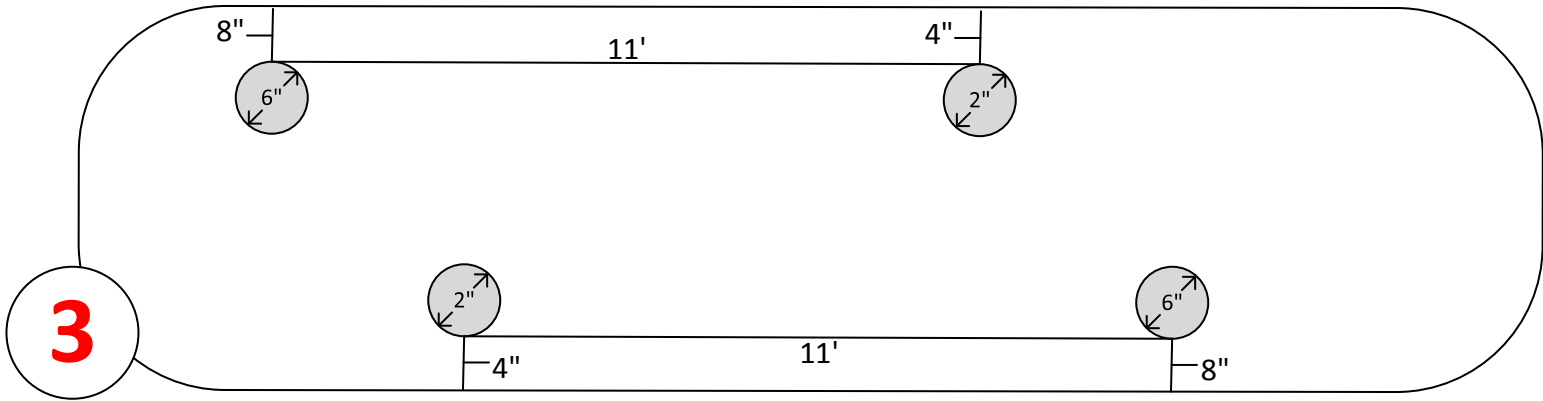
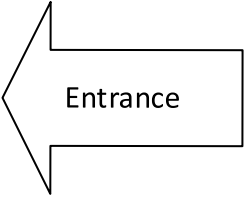
3 RUNS OF #8 AT 25' EACH TOTAL OF 75'


8 = rigid 90's

1= plug

BID DRAWINGS E-401 DETAIL 2

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (Future)
LPR – License Recognition
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 - 2" – Low voltage from source
C3 - 1" – 120VAC 20A
C4 - 1" – Low voltage data
C5 – ½" - To curb face at grade
C6 - ¾" - Low voltage data



Legend	
	- Conduit Area

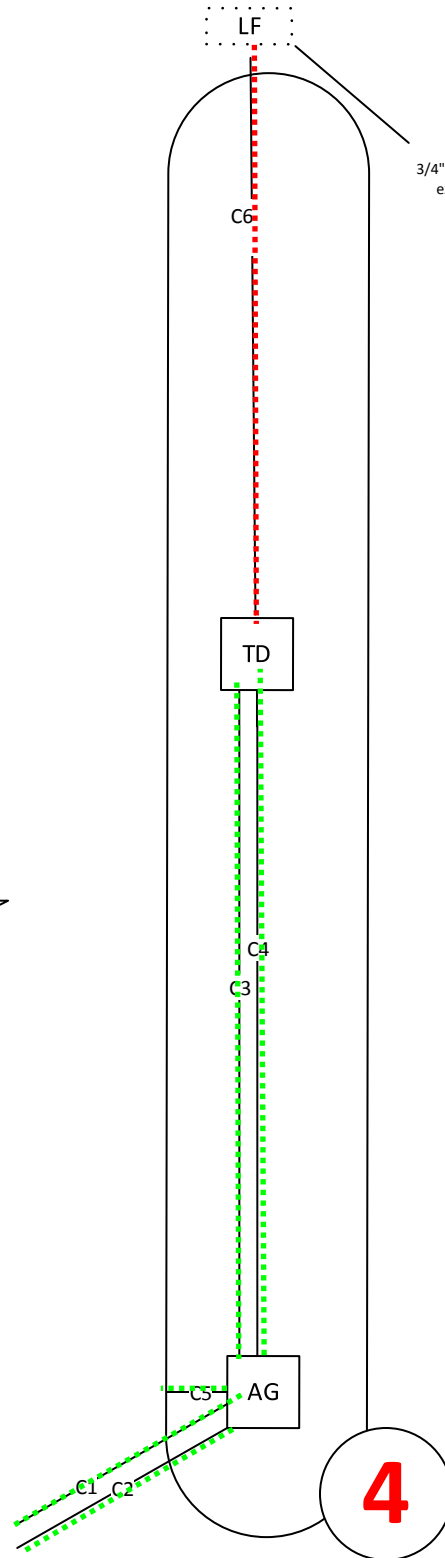
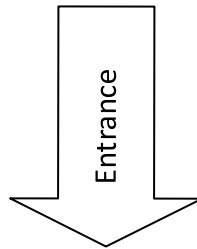
20' = 3/4" CONDUIT

2 = RIGID 90'S

Future Use.
3/4" conduit 18' from entry/
exit station See ASI09

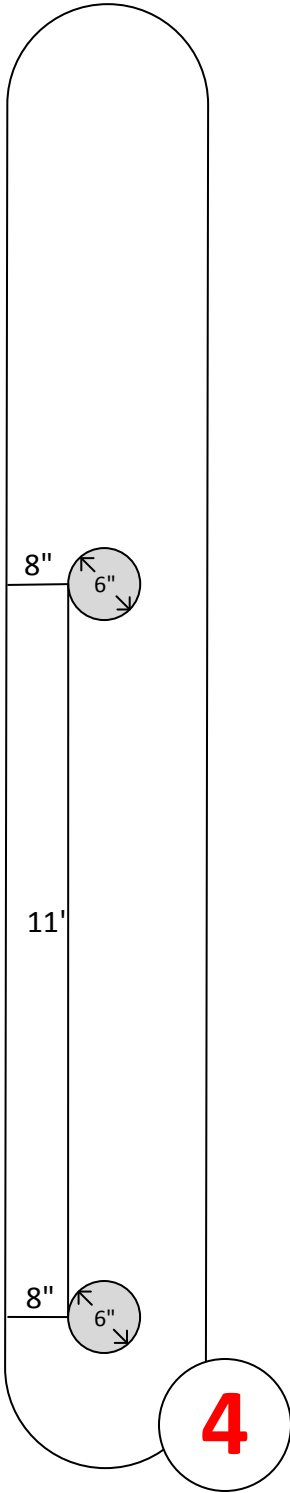
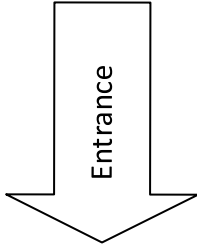
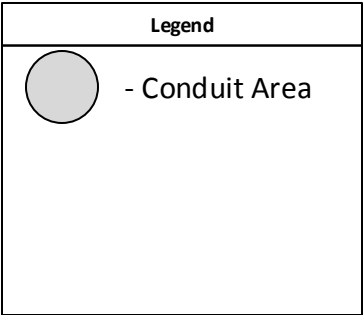
LINES IN GREEN IN BID

LINES IN RED NEW



BID DRAWING E-401 DETAIL 1

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 – 2" – Low voltage from source
C3 – 1" – 120VAC 20A
C4 – 1" – Low voltage data
C5 – ½" – To curb face at grade
C6 – 3/4" – Low Voltage Cable



2= QUAZITE BOXES 1 POWER AND 1 DATA

C4 = 15' TRENCH 1"

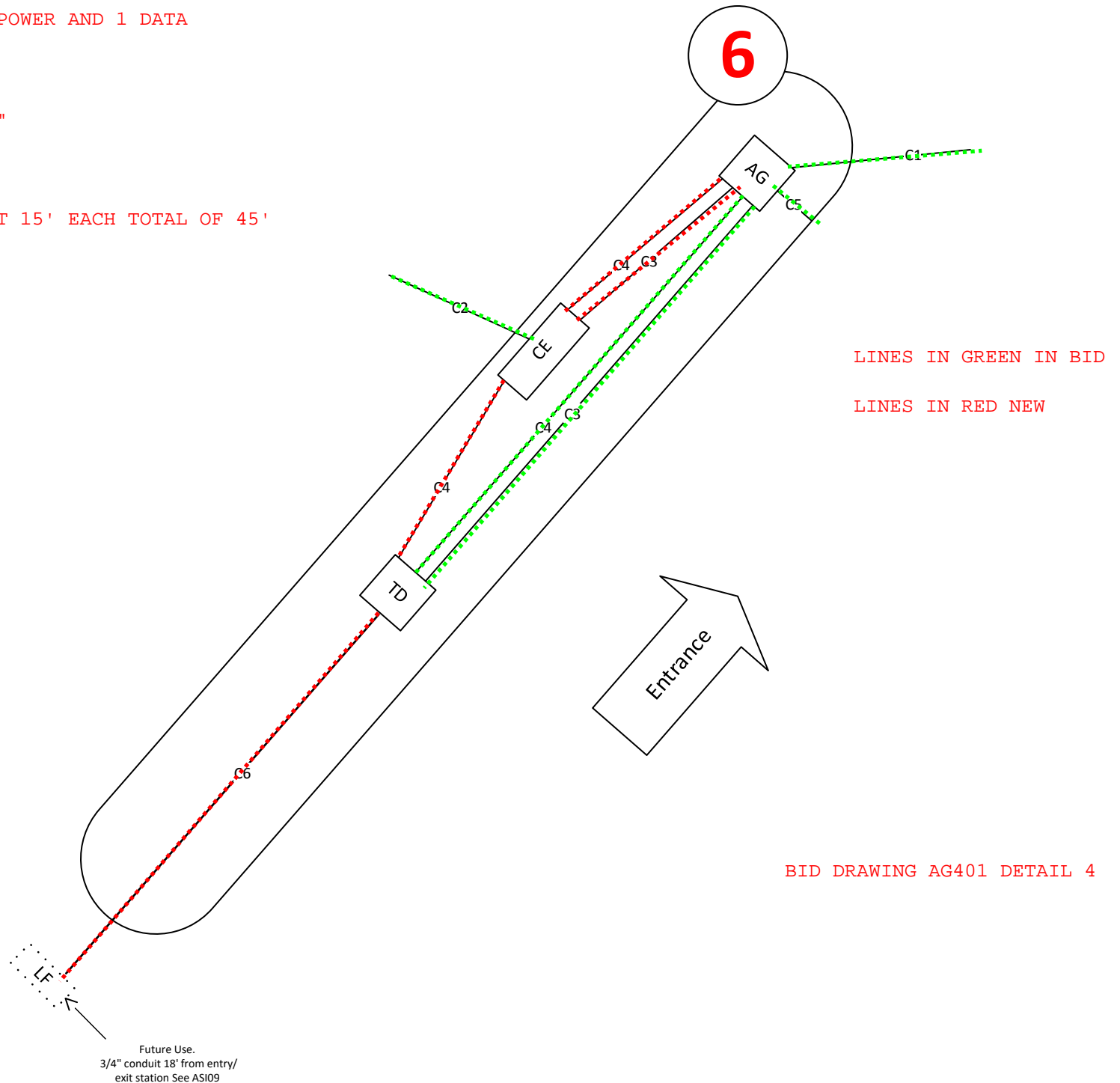
C6 = 20' TRENCH 3/4"

C3 10" TRENCH 1"

3 RUNS OF #6 WIRE AT 15' EACH TOTAL OF 45'

8 = RIGID 90'S

1 = PLUG

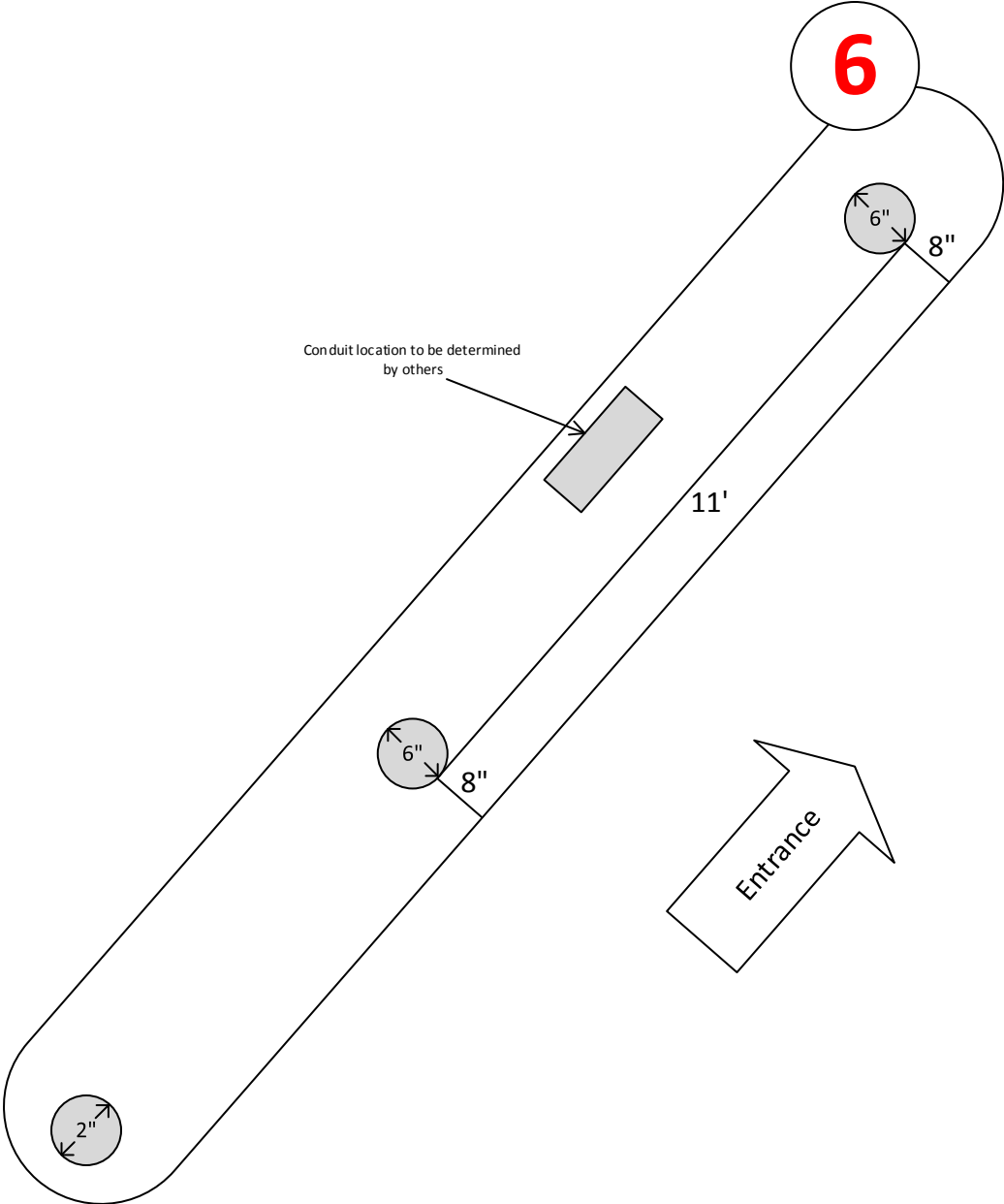



LINES IN GREEN IN BID

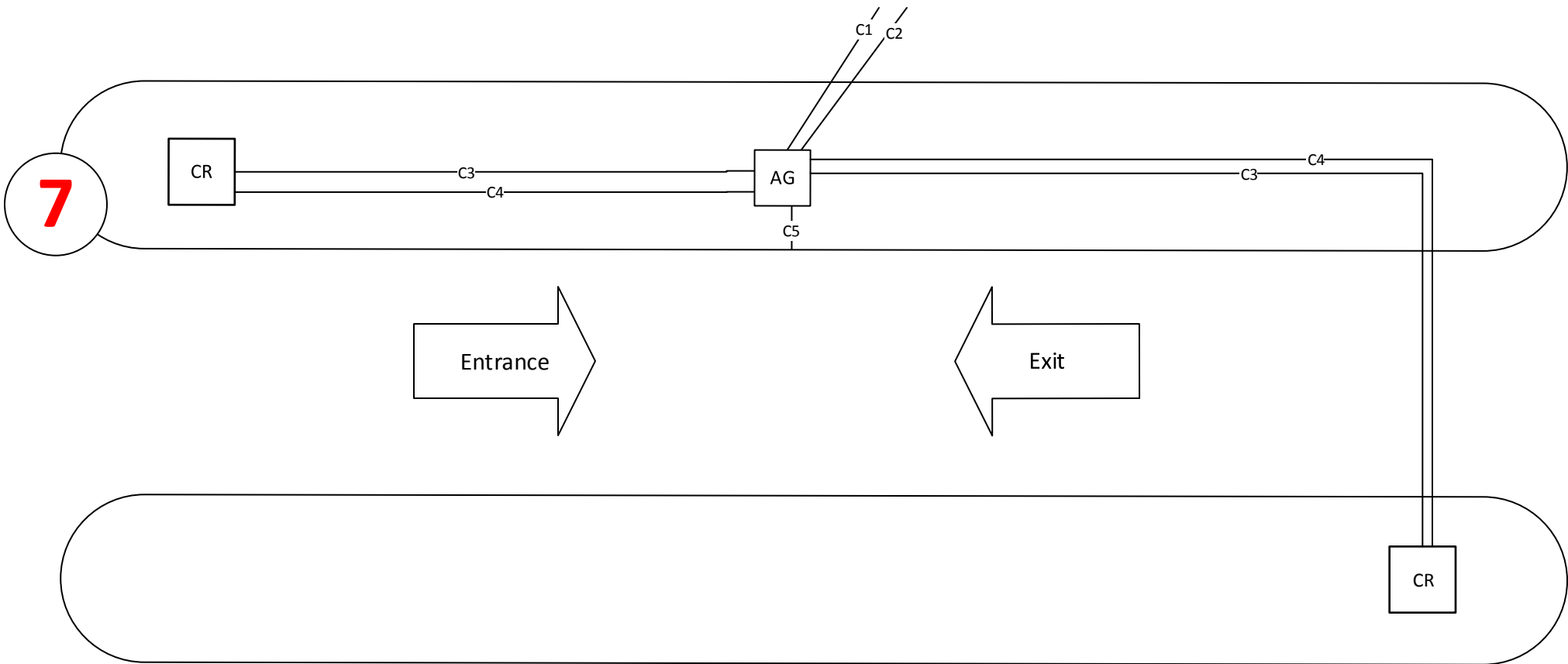
LINES IN RED NEW

BID DRAWING AG401 DETAIL 4

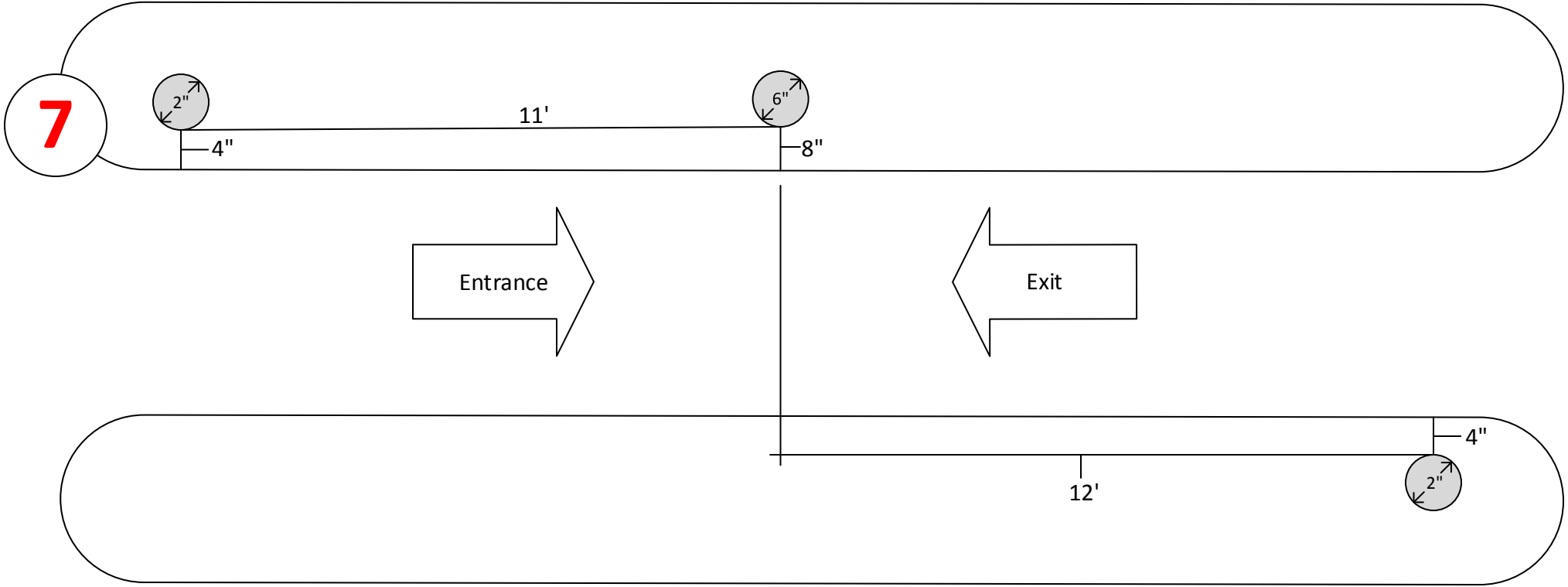
Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 – 2" – Low voltage from source
C3 – 1" – 120VAC 20A
C4 – 1" – Low voltage data
C5 – ½" – To curb face at grade
C6 – ¾" – Low Voltage Cable




Legend	
	- Conduit Area



Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 – 2" – Low voltage from source
C3 – 1" – 120VAC 20A
C4 – 1" – Low voltage data
C5 – ½" – To curb face at grade
C6 – ¾" – Low Voltage Cable



Legend	
	- Conduit Area

2 = QUAZITE BOXES 1 POWER 1 DATA

LINES IN GREEN IN BID

C4 = 30' TRENCH 1"

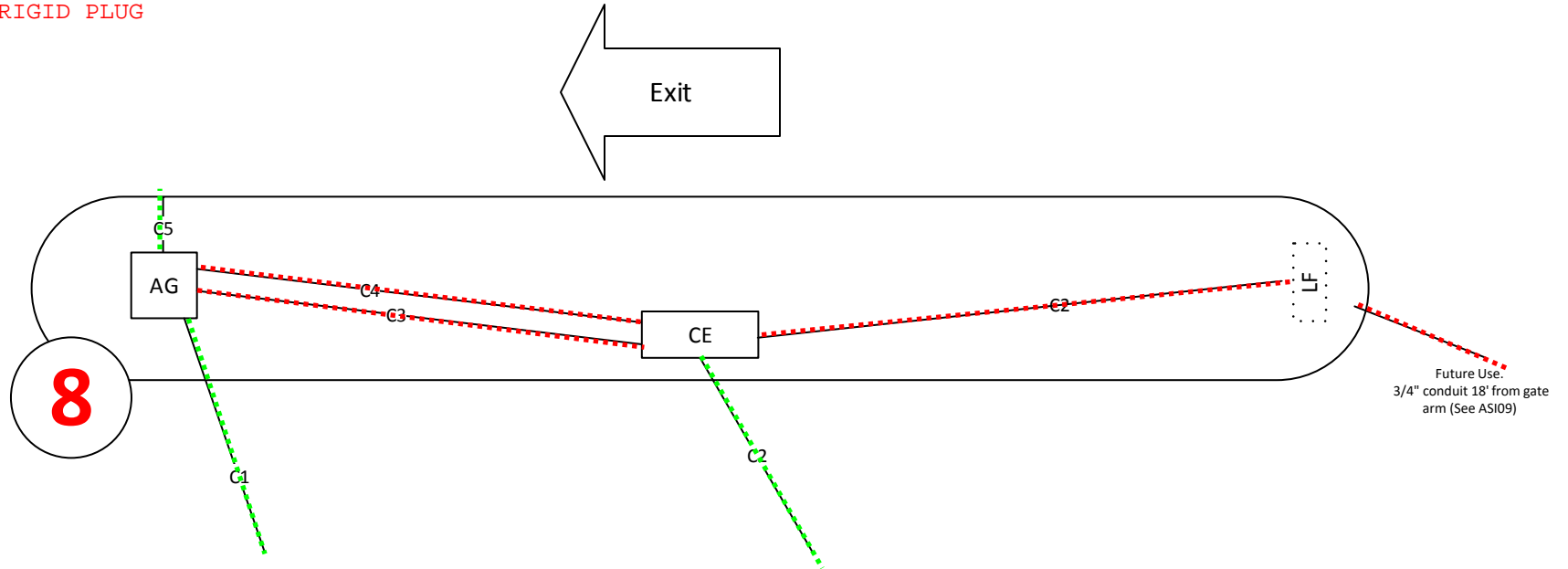
LINES IN RED NEW

C3 = 10' TRENCH 1"

3 RUNS OF #6 WIRE AT 15' EACH TOTAL OF 45'

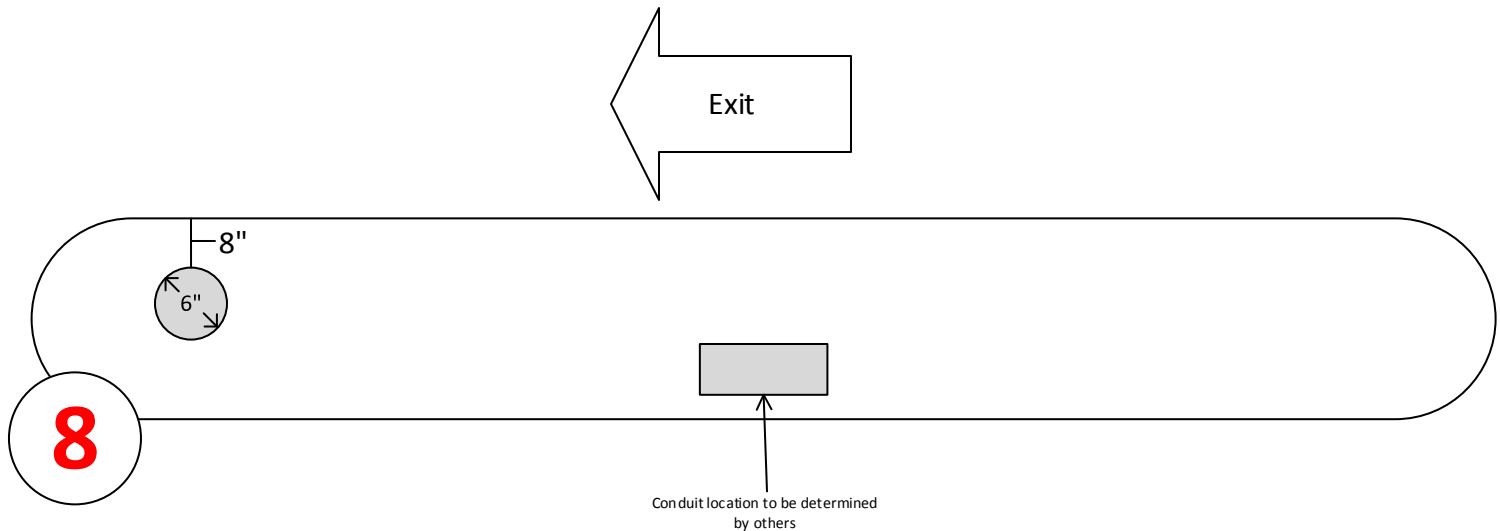
8 = RIGID 90'S


1 = RIGID PLUG



BID DRAWINGS AG401 DETAIL 6

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 - 2" – Low voltage from source
C3 - 1" – 120VAC 20A
C4 - 1" – Low voltage data
C5 – ½" - To curb face at grade
C6 - 3/4" - Low Voltage Cable



Legend	
	- Conduit Area

C3 = 10' TRENCH 1"

LINES IN GREEN IN BID

C4 = 35' TRENCH 1"

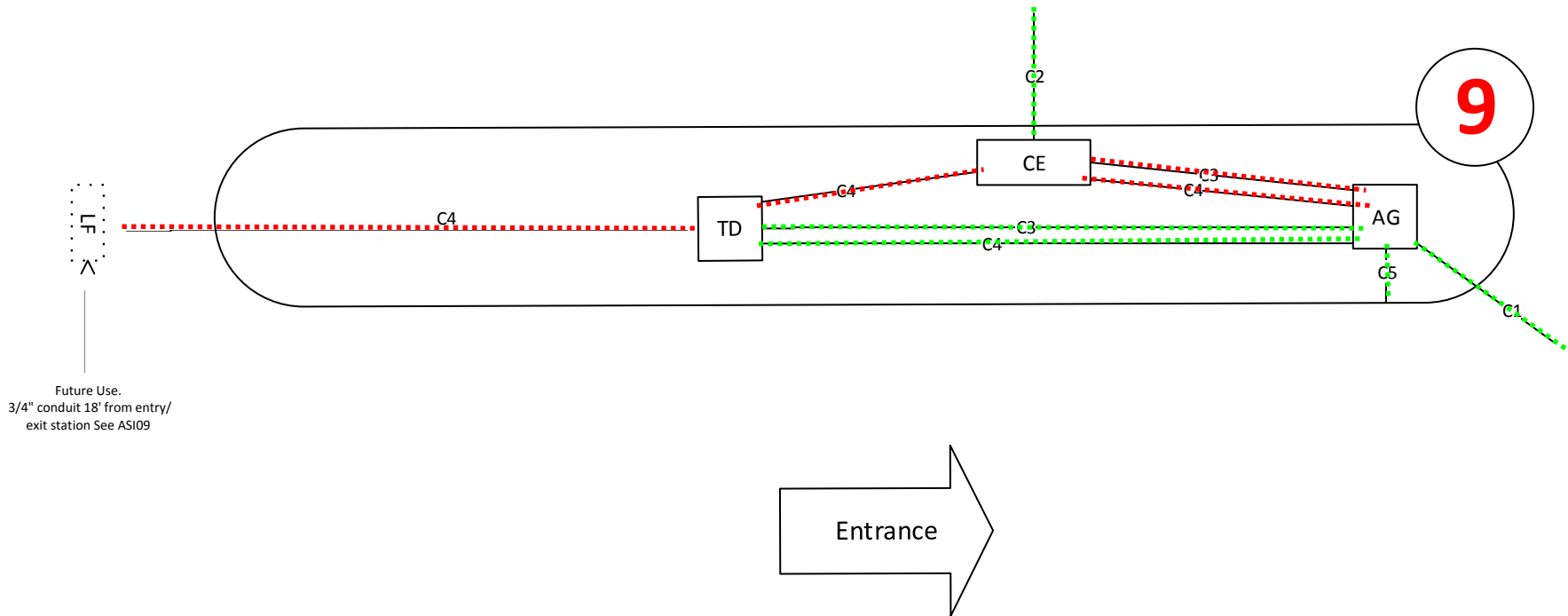
LINES IN RED NEW

8 = RIGID 90'S

1 = PLUG

3 RUNS OF #6 WIRE AT 15' EACH TOTAL OF 45'

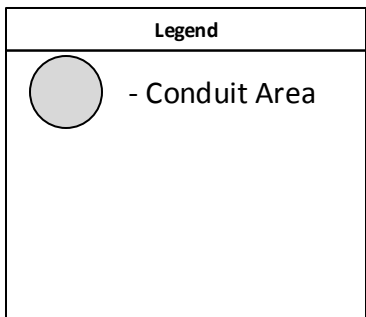
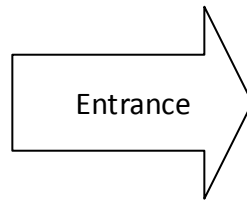
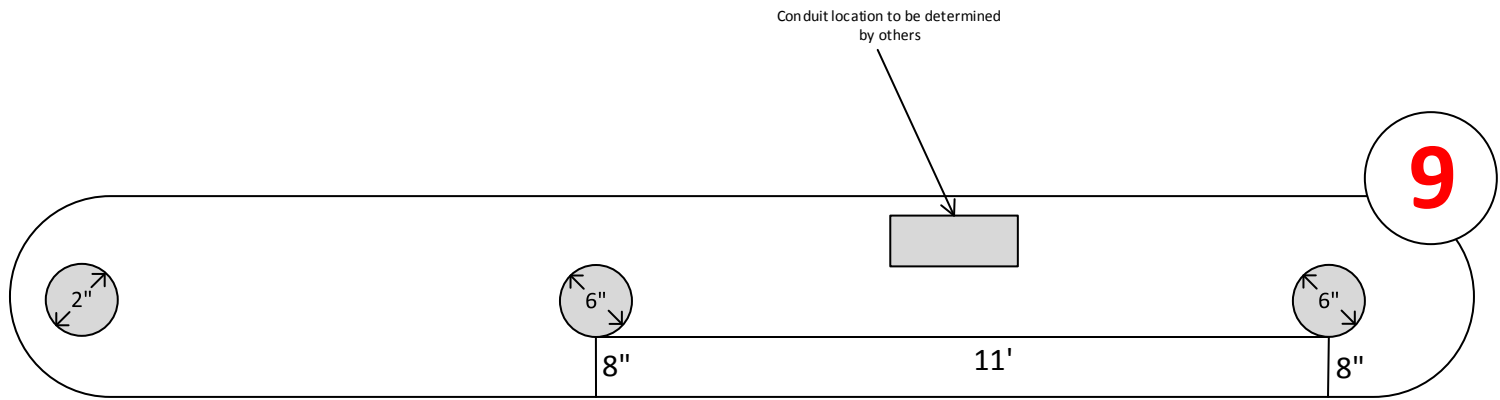
2 = quazite box



Future Use.
3/4" conduit 18' from entry/
exit station See ASI09

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 – 2" – Low voltage from source
C3 – 1" – 120VAC 20A
C4 – 1" – Low voltage data
C5 – 1/2" – To curb face at grade
C6 – 3/4" – Low Voltage Cable

BID DRAWINGS AG401 DETAIL 7



C1 = 65' BORE 1"

C2 = NO CHANGE

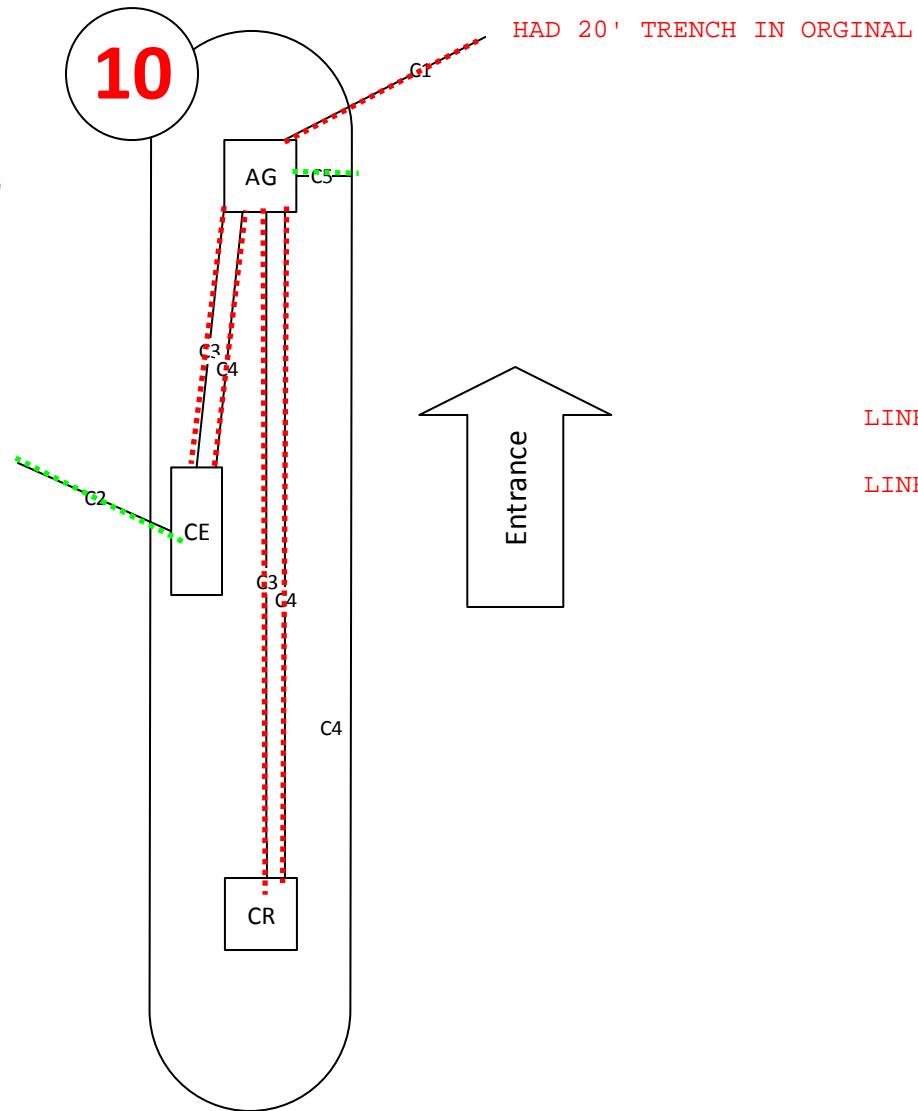
C3= 20' TRENCH 1"

C4 = 20' TRENCH 1"

1 = QUAZITE BOX FOR POWER

3 RUNS OF #6 AT 95' EACH TOTAL OF 285'

8 = RIGID 90'S

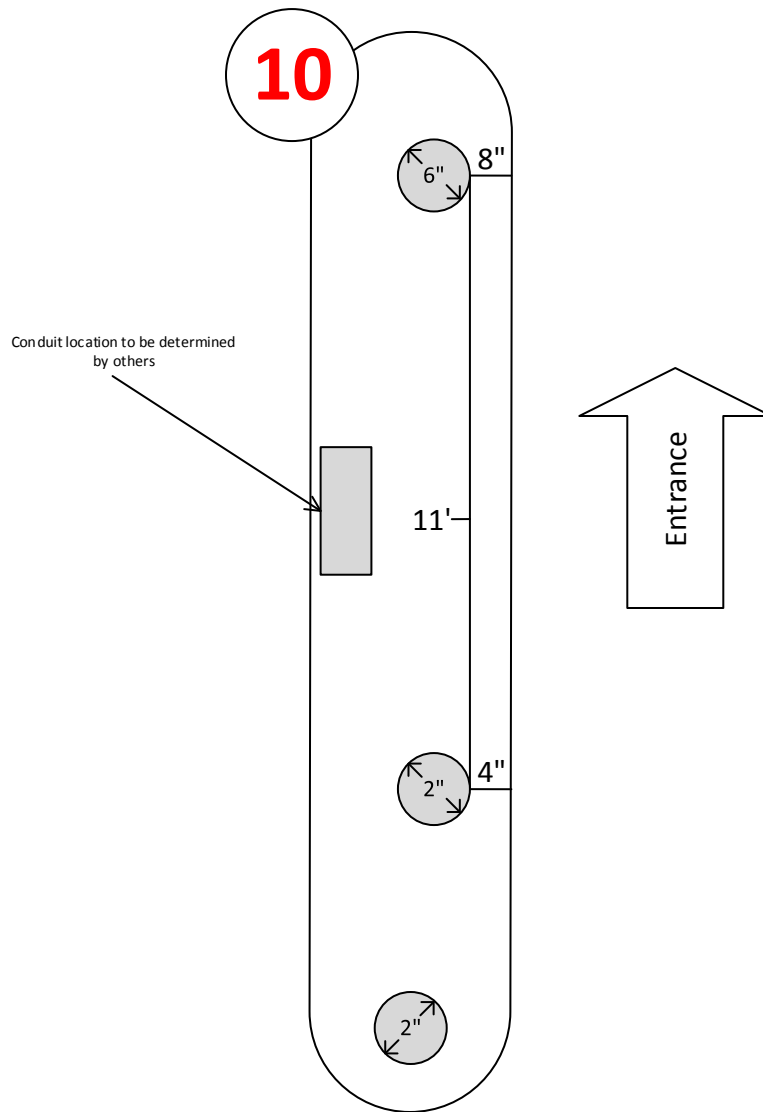
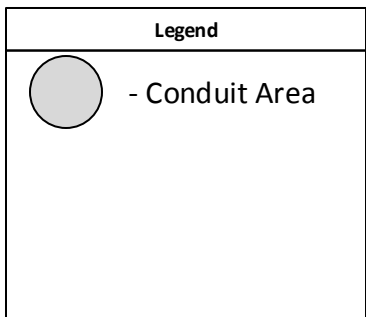


LINES IN GREEN IN BID

LINES IN RED NEW

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 - 2" – Low voltage from source
C3 - 1" – 120VAC 20A
C4 - 1" – Low voltage data
C5 – ½" - To curb face at grade
C6 - ¾" - Low Voltage Cable

BID DRAWING AG401 DETAIL 8



C2= 300' BORE 2" PVC

C1= 300' BORE 1"

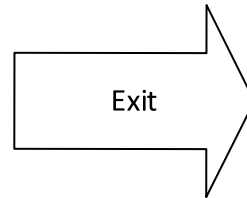
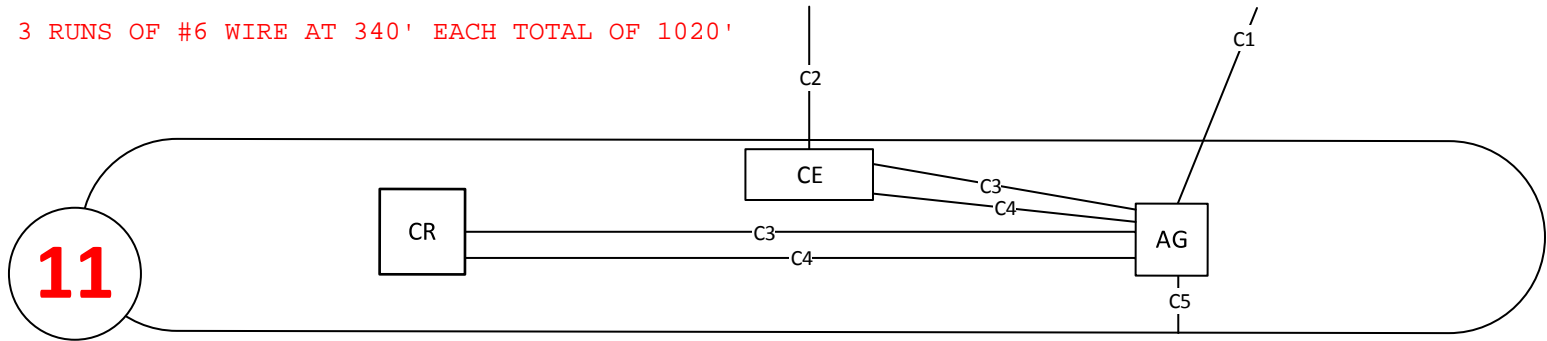
C3= 20' TRENCH 1"

C4 = 20' TRENCH 1"

4 = QUAZITE BOXES NEEDED

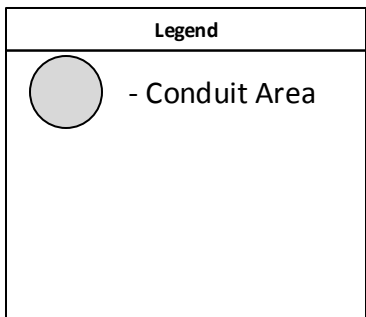
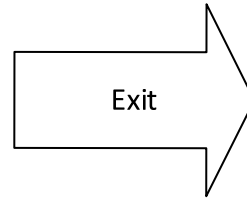
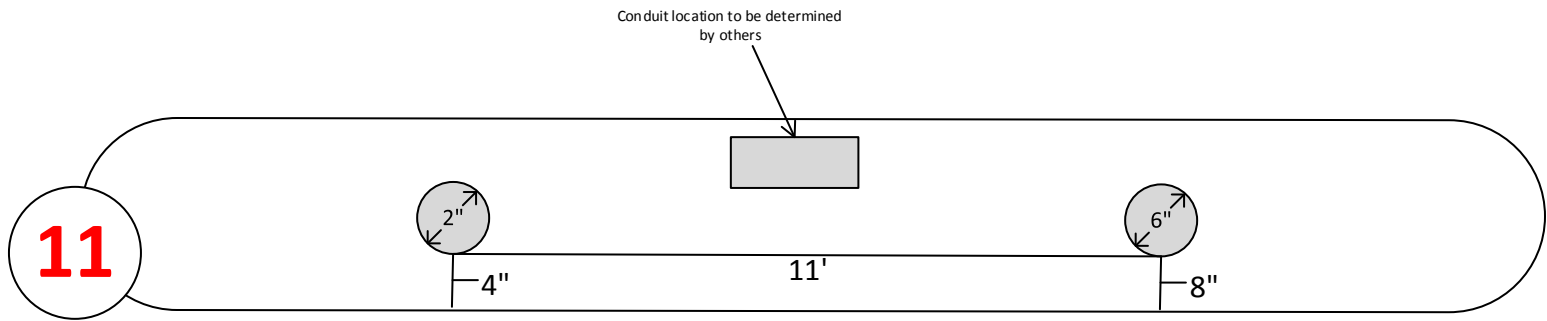
10 = RIGID 1" 90'S

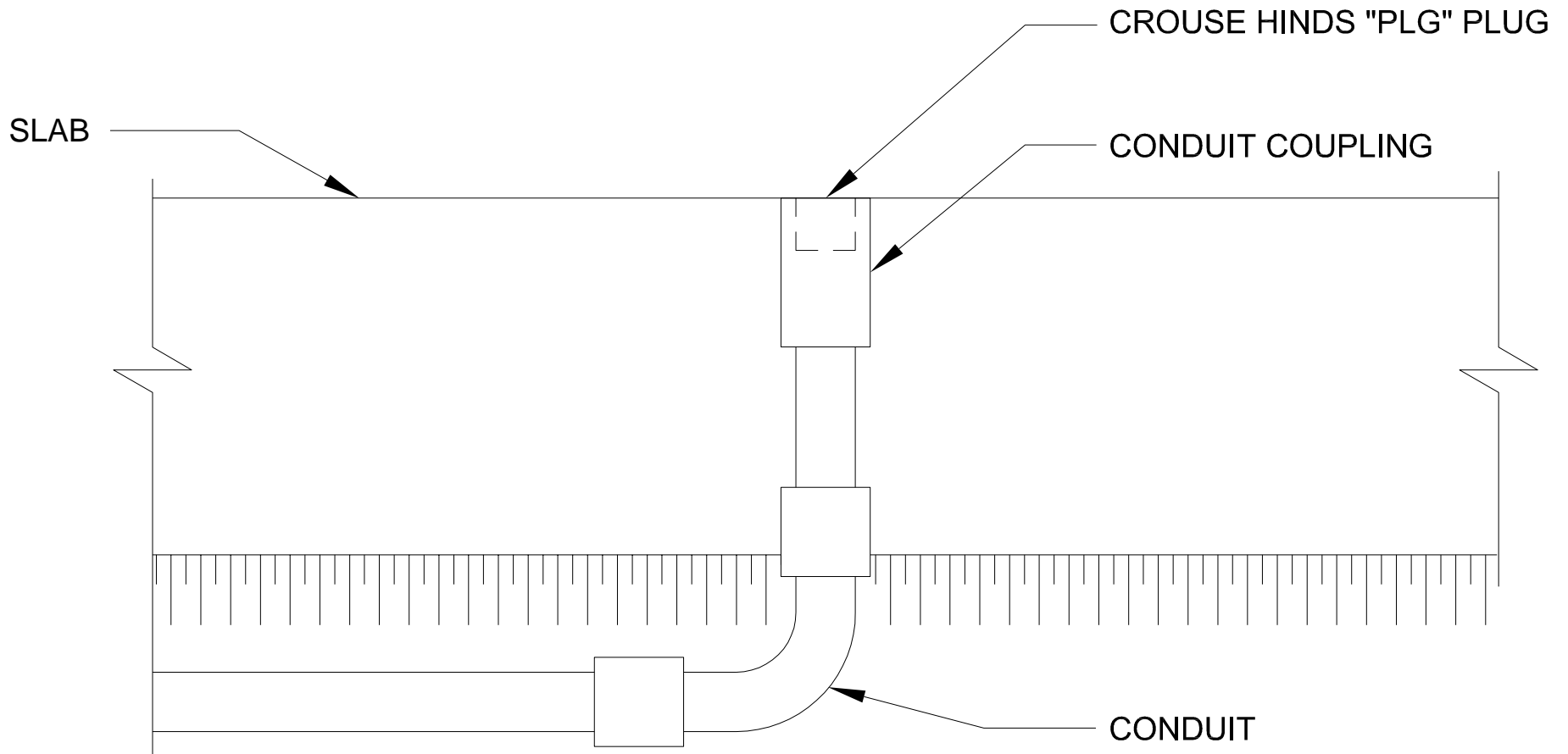
3 RUNS OF #6 WIRE AT 340' EACH TOTAL OF 1020'



BID DRAWING C3.3 NOT GATE SHOW

Legend
AG – Automatic Gate
CE – Communication Enclosure
CR – Card Reader
LF – Lot Full Sign or LPR (future)
TD – Ticket Dispenser
C1 – 1" – 120VAC 20A from source
C2 – 2" – Low voltage from source
C3 – 1" – 120VAC 20A
C4 – 1" – Low voltage data
C5 – 1/2" – To curb face at grade
C6 – 3/4" – Low Voltage Cable





ASI 09

CONDUIT TERMINATION DETAIL

Appling Boring Company, Inc.

426 Baxter Cemetery Road
Forest City NC 28043

Phone 828-657-6397
Fax 828-657-4537

Proposal - Acceptance

SUBMITTED TO Fountain Services LLC	DATE 25-Oct-17
Attn: Carl Cope	JOB NAME
CITY, STATE, ZIP	JOB LOCATION
PHONE	EMAIL ccope@fountainervicesllc.com

WE HEREBY SUBMIT SPECIFICATIONS AND ESTIMATES FOR:

2" Bore with 1 Conduit

\$16.50 Per foot

Appling Boring will notify NC811 to mark all utilities in the area. Fountain Services, LLC will be responsible for notifying any utilities not a member of NC811.

All material is guaranteed to be as specified. All work shall be completed in a workmanlike manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the amount set forth above. We shall not be responsible for delays caused by strikes, accidents, or other contingencies beyond our control. Owner to carry fire, tornado and other necessary insurance. Our workers are fully covered by Workmen's Compensation Insurance.

Note: this proposal may be withdrawn by us if not accepted with 30 days.

AUTHORIZED SIGNATURE

ACCEPTANCE OF PROPOSAL

AUTHORIZED SIGNATURE

The above prices, specifications, terms and conditions are satisfactory and are hereby accepted. You are authorized to do work as specified. Payment will be made as outlined above.

Date of Acceptance

Thank you for allowing Appling Boring Co., Inc. bid this work

Job ID: 160706 ASHEVILLE REGIONAL PARKING GARAGE
Project: 160706 ASHEVILLE REGIONAL PARKING GARAGE



CO: CO-0012: Tiba Layouts

Summary by Item Number

3 Oct 2017 11:27:43

Item #	Size	Description	Q/M	Quantity	U/M	Mat Unit	Mat Result	Labor Unit	Lab Result
10176	1	PVC SCH 40 10' LAID IN TRENCH	M	20	FT	0.3575	7.15	0.0429	0.86
10186	3/4	PVC SCH 40 20' LAID IN TRENCH	M	20	FT	0.4030	8.06	0.0380	0.76
10187	1	PVC SCH 40 20' LAID IN TRENCH	M	175	FT	0.6406	112.11	0.0396	6.93
10198	3/4	PVC SCH 80	M	20	FT	0.9186	18.37	0.0545	1.09
10199	1	PVC SCH 80	M	40	FT	0.4492	17.97	0.0644	2.57
20022	3/4	GRC 90-DEG ELBOW	M	4	EA	2.7779	11.11	0.4620	1.85
20023	1	GRC 90-DEG ELBOW	M	50	EA	4.2594	212.97	0.5775	28.88
30001	3/4	GRC/IMC COUPLING	M	2	EA	0.8060	1.61	0.0198	0.04
30002	1	GRC/IMC COUPLING	M	42	EA	1.1937	50.14	0.0247	1.04
30145	3/4	GRC CONDUIT CAP-OFF	M	1	EA	16.7946	16.79	0.1386	0.14
30146	1	GRC CONDUIT CAP-OFF	M	4	EA	87.2463	348.99	0.1617	0.65
31320	3/4	PVC FEMALE ADAPTER	M	2	EA	0.2154	0.43	0.1650	0.33
31321	1	PVC FEMALE ADAPTER	M	50	EA	0.2910	14.55	0.2063	10.31
70035	8	THHN/THWN CU (STR)	M	75	FT	0.2940	22.05	0.0104	0.78
70036	6.	THHN/THWN CU (STR)	M	1,440	FT	0.4523	651.33	0.0127	18.30
160779	1/4-20 x 1"	PLTD MACHINE SCREWS	M	8	EA	0.0416	0.33	0.0924	0.74
240007	1 5/8" x 1 5/8"H	12G STRUT CHNL 7/8"KO GALV	M	20	FT	13.9097	278.19	0.1320	2.64
240302	1/4-20	STRUT CHNL TWIRL-NUT-ZN	M	8	EA	4.6496	37.20	0.0825	0.66
240378	P1067EG	STRUT FLAT PLATE 4 HOLE	M	4	EA	6.3456	25.38	0.0330	0.13
390478	1"	ROCKY HORIZONTAL BORE	E	365	FT	0.0000	0.00	0.0248	9.03
390479	2"	ROCKY HORIZONTAL BORE	E	300	FT	0.0000	0.00	0.0262	7.87
400085	13"x 24"x 12"	HANDHOLE STR-WALL OPN-BOTM 5K	M	11	EA	136.3800	1,500.18	1.4570	16.03
710403	12x8x4	PULL BOX SCREW CVR NEMA 3R NO/KO	M	2	EA	48.8400	97.68	1.5015	3.00
Phase/Group totals:						3,432.59		114.62	
Job totals:						3,432.59		114.62	

Appling Boring Co., Inc.

Invoice

426 Baxter Cemetery Rd
Forest City, NC 28043

Date	Invoice #
5/12/2017	8169

Bill To

Fountain Electric Services LLC
244 Callahan-Koon Road
Spindale NC 28160

P.O. No.	Terms	Rep	Project
8169		JL	

Description	Qty	Rate	Amount
May 8 & 9 (1) 60' Bore (3- (1-1/2") Conduit)	60	30.00	1,800.00
May 8 & 9 (1) 60' Bore (3- (1") Conduit)	60	30.00	1,800.00
May 10 (2) 100' 4"Bores	200	30.00	6,000.00
May 11 (1) 60' Bore (3- (1") Conduit)	60	30.00	1,800.00
Work performed at Asheville Airport			
Total			\$11,400.00
Payments/Credits			\$0.00
Balance Due			\$11,400.00

Phone #	Fax #	E-mail
828-657-6397	828-657-4537	caycee.lovingood@gmail.com



Rental Rates

Equipment pricing may vary based on availability and market conditions.

\$500

4 Hour

\$500

Daily

\$1,330

Weekly

\$3,545

4 Week



Add to Cart

Change Order Request # 36

BUILDING ON A GREAT TRADITION



24-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226

RE: Asheville Regional Airport Parking Garage Change Order Request #36

Dear Mr. Moody

This Change Order Request is for the additional three (3) stop signs requested by the Owner per the email sent on 10/9/17.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
New Dimensions	1	LS	1,000.56	1,000.56
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			1,000.56	1,000.56
OH&P			10.00%	100.06
<hr/>				
SUBTOTAL				1,100.62
BOND			1.0%	11.01
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				1,111.62

We have included appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$1,111.62**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: <u>Asheville Regional Airport Parking Deck</u>	Proposal #: <u>17605 ND #1</u>
Bid Package: <u>Striping</u>	Project # <u>09-163</u>
Contractor: <u>New Dimensions</u>	Date: <u>10/5/2017</u>

Description of Change: Three additional stop signs installed

Material:					Subtotals
	1. Total Cost of Materials (Attach Backup)		\$612.00		
	2. Sales Tax	7%	\$42.84		
	3. Overhead & Profit	10%	\$65.48		\$720.32

Labor:					
	4. Total Man Hour Cost (Attach Backup)		\$132.50		
	5. Payroll Burden (Enter Percentage)	0.1	\$13.25		
	6. Overhead & Profit	10%	\$14.58		\$160.33

Equipment:					
	7. Equipment / Equipment Rental (Attach Backup)		\$100.00		
	8. Overhead & Profit	10%	\$10.00		\$110.00

Unit Prices:					
	9. Unit Price (If Applicable)				\$0.00

Subcontractors:					
	10. Subcontractor (Attach Subcontractor Backup)				
	11. Overhead & Profit	10%	\$0.00		\$0.00

			Subtotal of Proposal		\$990.65
	12. Payment & Performance Bond (Enter Bond Rate Percentage)	1%	\$9.91		
			Total of Change Order Proposal		\$1,000.56

Time Extension Request: 0 Days

Schedule Activity # Affected: _____ **Does this effect the critical path?** _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____ Date: _____

Subcontractor: Chris Miller Date: 10-5-17

New Dimensions in Pavement Markings

231 Rutledge Road
 Fletcher, NC 28732
 Voice: 828-654-8181
 Fax: 828-654-7590

PROPOSAL

Proposal Number: 17605
 Proposal Date: Oct 5, 2017

To:
American South General Contractors Inc. 60 N. Merrimon Ave. Unit 101 Asheville, NC 28804

Location:
Stop Sign Change Order

Payment Terms
Net 30 Days

Quantity	Item	Description	Unit Price	Amount
3.00	502	Stop signs on 2 1/2" x 2 1/2" Aluminum Post	333.52	1,000.56

Subtotal	1,000.56
Sales Tax	
Freight	0.00
TOTAL PROPOSAL AMOUNT	1,000.56

This proposal is based on the work as specified to our company via blueprint, email, meeting, or on-site review. Any and all work required above and beyond the above referenced items will be invoiced at a per item price. Unless otherwise noted, this proposal covers ONE mobilization to complete this work. This proposal will be valid for 60 days beginning with the proposal date. If it is not accepted prior to then the pricing will be subject to re-evaluation as necessary.

I have read and agree to the terms of this proposal and the work quoted herein

Signed _____

Date _____

Change Order Request # 37

BUILDING ON A GREAT TRADITION

24-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #37

Dear Mr. Moody

This Change Order Request is for the additional work associated with painting Stair Towers B and C as well as the east walls of the Communication Rooms on 2nd and 4th floors and the south wall of the Electrical Room on the Ground Floor.

<u>Description of Subcontractor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Southern Painting	1	LS	16,404.42	16,404.42
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			16,404.42	16,404.42
OH&P			10.00%	1,640.44
<hr/>				
SUBTOTAL				18,044.86
BOND			1.0%	180.45
<hr/>				
TOTAL CHANGE ORDER PROPOSAL				18,225.31

We have included the appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$18,225.31**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Painting 9900
Contractor: Southern Painting

Proposal #: 2R
Project #: 09-163
Date: 10/25/2017

Description of Change: Painting Stairwells B&C & Exposed Faces of the Communication Rooms @ Levels 2 and 4 (east face) and the Electrical Room on the Ground Floor (south face)

Material:

1. Total Cost of Materials (Attach Backup)		<u>\$3,198.80</u>	
2. Sales Tax	7%	<u>\$223.92</u>	
3. Overhead & Profit	10%	<u>\$342.27</u>	<u>\$3,764.99</u>

Subtotals

Labor:

4. Total Man Hour Cost (Attach Backup)		<u>\$9,800.00</u>	
5. Payroll Burden (Enter Percentage)	15%	<u>\$1,470.00</u>	
6. Overhead & Profit	10%	<u>\$1,127.00</u>	<u>\$12,397.00</u>

Equipment:

7. Equipment / Equipment Rental (Attach Backup)			
8. Overhead & Profit	10%	<u>\$0.00</u>	<u>\$0.00</u>

Unit Prices:

9. Unit Price (If Applicable)			<u>\$0.00</u>
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Subcontractors:

10. Subcontractor (Attach Subcontractor Backup)			
11. Overhead & Profit	10%	<u>\$0.00</u>	<u>\$0.00</u>

		Subtotal of Proposal	\$16,161.99
12. Payment & Performance Bond (Enter Bond Rate Percentage)	2%	<u>\$242.43</u>	<u>\$242.43</u>
Total of Change Order Proposal			<u>\$16,404.42</u>

Time Extension Request: _____ Days

Schedule Activity # Affected _____

Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____

Date: _____

Subcontractor: Bob Maffett

Date: 10/17/2017



SHERWIN-WILLIAMS
1139 WHITE HORSE RD
GREENVILLE, SC 29605 4933
(864) 236-9116

10/17/2017
Quote #: 3917795

SOUTHERN PAINTING
PO BOX 8924
GREENVILLE SC 296048924

Re: Price Quote for Asheville Regional Airport - Stairwell B & C

Dear David Fields:

Thank you for considering Sherwin-Williams products for the Asheville Regional Airport - Stairwell B & C project. Included is the Sherwin-Williams price quote.

Should you require assistance or have any questions or concerns, please contact me at (864) 236-9116 or e-mail me at l.gregg.thompson@sherwin.com.

Sincerely,

Larry Thompson
Sherwin-Williams
Sales Representative



**SHERWIN
WILLIAMS®**

PRICE QUOTATION

Account #: 5310-7155-3

10/17/2017
Quote #: 3917795

SOUTHERN PAINTING
PO BOX 8924
GREENVILLE, SC 296048924
(864) 232-4010

Project:	ASHEVILLE REGIONAL AIRPORT - STAIRWELL B & C
Start Date:	10/17/2017
Completion Date:	12/31/2017
Purchase Type:	Single Purchase

We are pleased to quote you as follows:

SALES NUMBER	SIZE	PRODUCT/REX NUMBER	DESCRIPTION	QTY	PRICE	EXT. PRICE
6501-32657	5 GAL	A24W08300	Loxon® Concrete & Masonry Primer, Interior/Exterior Latex White	40	\$25.58	\$1,023.20
6403-89169	5 GAL	A06W00151	A-100® Exterior Latex Flat Extra White	60	\$28.60	\$1,716.00
7907-99993	GALLON	B54W00101	Industrial Enamel Pure White	12	\$38.30	\$459.60

TOTAL PRICE:	\$3,198.80
---------------------	-------------------

We thank you for your consideration of Sherwin-Williams products and look forward to supplying these products to you.

Note: All prices are per gallon/unit.

TERMS OF THE SALE

Quotation Expires: 12/31/2017
F.O.B. Location:
Freight Terms:
Terms: As Agreed

By: Larry Thompson

Store Address: 1139 WHITE HORSE RD
City: GREENVILLE State: SC Zip: 29605 4933
Store Number: 2735
Phone: (864) 236-9116
Territory #: 5726

NOTICE: Please take notice that the quotation set forth above is not a contract and is subject to and conditioned upon approval by SHERWIN-WILLIAMS. In the event such approval is not obtained, you will be provided with a revised quotation and the quotation set forth above shall be null, void and of no force or effect. The pricing and recommendations detailed in this proposal represent confidential information provided by SHERWIN-WILLIAMS. We request that it not to be copied or shared with others outside your firm.

Change Order Request # 38R

BUILDING ON A GREAT TRADITION

27-Oct-17
Delta Airport Consultants
11121 Carmel Commons Blvd., Ste 435
Charlotte, NC 28226



RE: Asheville Regional Airport Parking Garage Change Order Request #38-Revised

Dear Mr. Moody

This Change Order Request is for the additional to add herbicide and a weed barrier to the river rock bed around the generator. This has been revised to remove the mobilization fees.

<u>Description of Subcontracor's Used</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>SUBCONTRACT</u>	<u>TOTAL</u>
Snow Creek Landscaping	1	LS	258.32	258.32
	1	LS		0.00
	1	LS		0.00
	1	LS		0.00
<hr/>				
SUBTOTAL			258.32	258.32
OH&P			10.00%	25.83
SUBTOTAL				284.15
BOND			1.0%	2.84
TOTAL CHANGE ORDER PROPOSAL				286.99

We have included the appropriate backup documentation provided by our Subcontractors.

Upon approval, please issue a Change Order for the total lump sum amount of **\$286.99**

Please feel free to contact us should you need any additional information regarding this or any other matter.
Sincerely,

AMERICAN SOUTH GENERAL CONTRACTORS, INC.

Seth Chambers

Seth Chambers
Project Manager

Change Order Proposal Worksheet

Project: Asheville Regional Airport Parking Deck
Bid Package: Landscaping
Contractor: Snow Creek Landscaping, LLC

Proposal #: 736369 2/R
Project # 09-163
Date: 10/25/2017

Description of Change: Herbicide & fabric

Material:			Subtotals
1. Total Cost of Materials (Attach Backup)		<u>\$127.61</u>	
2. Sales Tax	<u>7%</u>	<u>\$8.93</u>	
3. Overhead & Profit	<u>10%</u>	<u>\$13.65</u>	<u>\$150.20</u>
Labor:			
4. Total Man Hour Cost (Attach Backup)		<u>\$87.24</u>	
5. Payroll Burden (Enter Percentage)	<u>10%</u>	<u>\$8.72</u>	
6. Overhead & Profit	<u>10%</u>	<u>\$9.60</u>	<u>\$105.57</u>
Equipment:			
7. Equipment / Equipment Rental (Attach Backup)		<u>\$0.00</u>	
8. Overhead & Profit	<u>10%</u>	<u>\$0.00</u>	<u>\$0.00</u>
Unit Prices:			
9. Unit Price (If Applicable)			<u>\$0.00</u>
Subcontractors:			
10. Subcontractor (Attach Subcontractor Backup)			
11. Overhead & Profit	<u>10%</u>	<u>\$0.00</u>	<u>\$0.00</u>
Subtotal of Proposal			<u>\$255.76</u>
12. Payment & Performance Bond (Enter Bond Rate Percentage)		<u>1%</u>	<u>\$2.56</u>
Total of Change Order Proposal			<u>\$258.32</u>

Time Extension Request: _____ Days

Schedule Activity # Affected _____

Does this effect the critical path? _____

This Contractor agrees to perform the work outlined in this change proposal for the amount specified above and in accordance with the contract documents.

Construction Manager: _____

Date: _____

Subcontractor: U.K. [Signature]

Date: 10/27/17



MEMORANDUM

TO: Members of the Airport Authority
FROM: Lew Bleiweis, A.A.E., Executive Director
DATE: November 9, 2017

ITEM DESCRIPTION – Information Section Item A

September, 2017 Traffic Report – Asheville Regional Airport

SUMMARY

September, 2017 overall passenger traffic numbers were up 2.6% compared to the same period last year. Passenger traffic numbers reflect a 3.8% increase in passenger enplanements from September, 2016. Enplanements for Fiscal Year to Date total 142,441 which is an 11.3% increase over the same period last year.

AIRLINE PERFORMANCE

Allegiant Airlines: Year over Year passenger enplanements for Allegiant in September 2017 were up by 1.5%. There were no flight cancellations for the month.

American Airlines: American's September 2017 passenger enplanements represent a 4.0% decrease over the same period last year. There were fifteen (15) flight cancellations for the month.

Delta Airlines: Delta's September 2017 enplanements decreased by 7.2% compared to September 2016. There were no flight cancellations for the month.

United Airlines: In September 2017, United Airlines saw an increase in enplanements by 42.1% over the same period last year. There were no flight cancellations for the month.

Monthly Traffic Report

Asheville Regional Airport

September 2017



Category	Sep 2017	Sep 2016	Percentage Change	*CYTD-2017	*CYTD-2016	Percentage Change	*MOV12-2017	*MOV12-2016	Percentage Change
Passenger Traffic									
Enplaned	40,051	38,597	3.8%	342,782	301,905	13.5%	455,483	399,045	14.1%
Deplaned	<u>39,427</u>	<u>38,831</u>	1.5%	<u>341,375</u>	<u>299,351</u>	14.0%	<u>454,066</u>	<u>397,035</u>	14.4%
Total	79,478	77,428	2.6%	684,157	601,256	13.8%	909,549	796,080	14.3%
Aircraft Operations									
Airlines	603	612	-1.5%	5,726	4,753	20.5%	7,503	6,596	13.8%
Commuter /Air Taxi	<u>1,267</u>	<u>1,001</u>	26.6%	8,050	7,660	5.1%	10,234	10,029	2.0%
Subtotal	<u>1,870</u>	<u>1,613</u>	15.9%	<u>13,776</u>	<u>12,413</u>	11.0%	<u>17,737</u>	<u>16,625</u>	6.7%
General Aviation	4,051	3,878	4.5%	31,507	32,910	-4.3%	41,986	43,636	-3.8%
Military	<u>564</u>	<u>413</u>	36.6%	<u>3,523</u>	<u>3,182</u>	10.7%	<u>4,555</u>	<u>4,427</u>	2.9%
Subtotal	<u>4,615</u>	<u>4,291</u>	7.6%	<u>35,030</u>	<u>36,092</u>	-2.9%	<u>46,541</u>	<u>48,063</u>	-3.2%
Total	6,485	5,904	9.8%	48,806	48,505	0.6%	64,278	64,688	-0.6%
Fuel Gallons									
100LL	16,748	15,736	6.4%	122,001	129,471	-5.8%	163,690	172,140	-4.9%
Jet A (GA)	171,193	124,704	37.3%	1,084,989	939,754	15.5%	1,447,047	1,214,763	19.1%
Subtotal	<u>187,941</u>	<u>140,440</u>	33.8%	<u>1,206,990</u>	<u>1,069,225</u>	12.9%	<u>1,610,737</u>	<u>1,386,903</u>	16.1%
Jet A (A/L)	<u>329,930</u>	<u>282,636</u>	16.7%	<u>2,547,344</u>	<u>1,894,224</u>	34.5%	<u>3,279,618</u>	<u>2,637,457</u>	24.3%
Total	517,871	423,076	22.4%	3,754,334	2,963,449	26.7%	4,890,355	4,024,360	21.5%

*CYTD = Calendar Year to Date and *Mov12 = Moving Twelve Months.

Thursday, October 26, 2017

Airline Enplanements, Seats, and Load Factors

Asheville Regional Airport

September 2017



	Sep 2017	Sep 2016	Percentage Change	*CYTD-2017	*CYTD-2016	Percentage Change
Allegiant Air						
Enplanements	10,189	10,043	1.5%	103,234	90,692	13.8%
Seats	14,107	11,762	19.9%	122,169	106,085	15.2%
Load Factor	72.2%	85.4%	-15.4%	84.5%	85.5%	-1.2%
American Airlines						
Enplanements	10,242	10,673	-4.0%	76,911	73,477	4.7%
Seats	12,327	12,398	-0.6%	103,464	94,224	9.8%
Load Factor	83.1%	86.1%	-3.5%	74.3%	78.0%	-4.7%
Delta Air Lines						
Enplanements	11,319	12,191	-7.2%	102,698	104,572	-1.8%
Seats	13,037	14,269	-8.6%	127,618	129,680	-1.6%
Load Factor	86.8%	85.4%	1.6%	80.5%	80.6%	-0.2%
Elite Airways						
Enplanements	218	0	#Div/0!	998	0	#Div/0!
Seats	520	0	#Div/0!	1,970	0	#Div/0!
Load Factor	41.9%	#Num!	#Type!	50.7%	#Num!	#Type!
United Airlines						
Enplanements	8,083	5,690	42.1%	58,941	33,164	77.7%
Seats	9,752	6,750	44.5%	72,354	37,864	91.1%
Load Factor	82.9%	84.3%	-1.7%	81.5%	87.6%	-7.0%
Totals						
Enplanements	40,051	38,597	3.8%	342,782	301,905	13.5%
Seats	49,743	45,179	10.1%	427,575	367,853	16.2%
Load Factor	80.5%	85.4%	-5.8%	80.2%	82.1%	-2.3%

Thursday, October 26, 2017

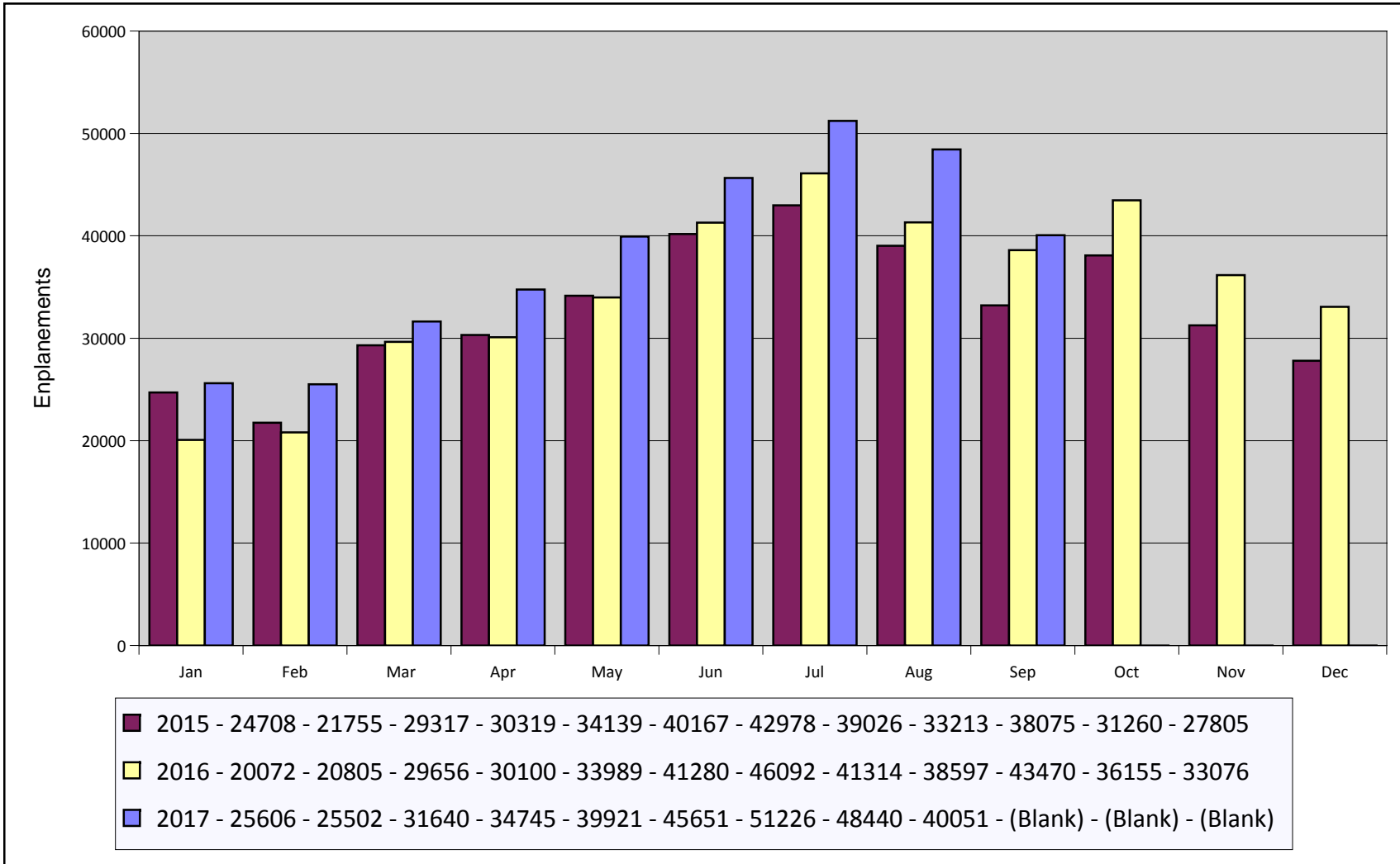
*CTYD = Calendar Year to Date and *Mov12 = Moving Twelve Months.

Airline Flight Completions Asheville Regional Airport September 2017

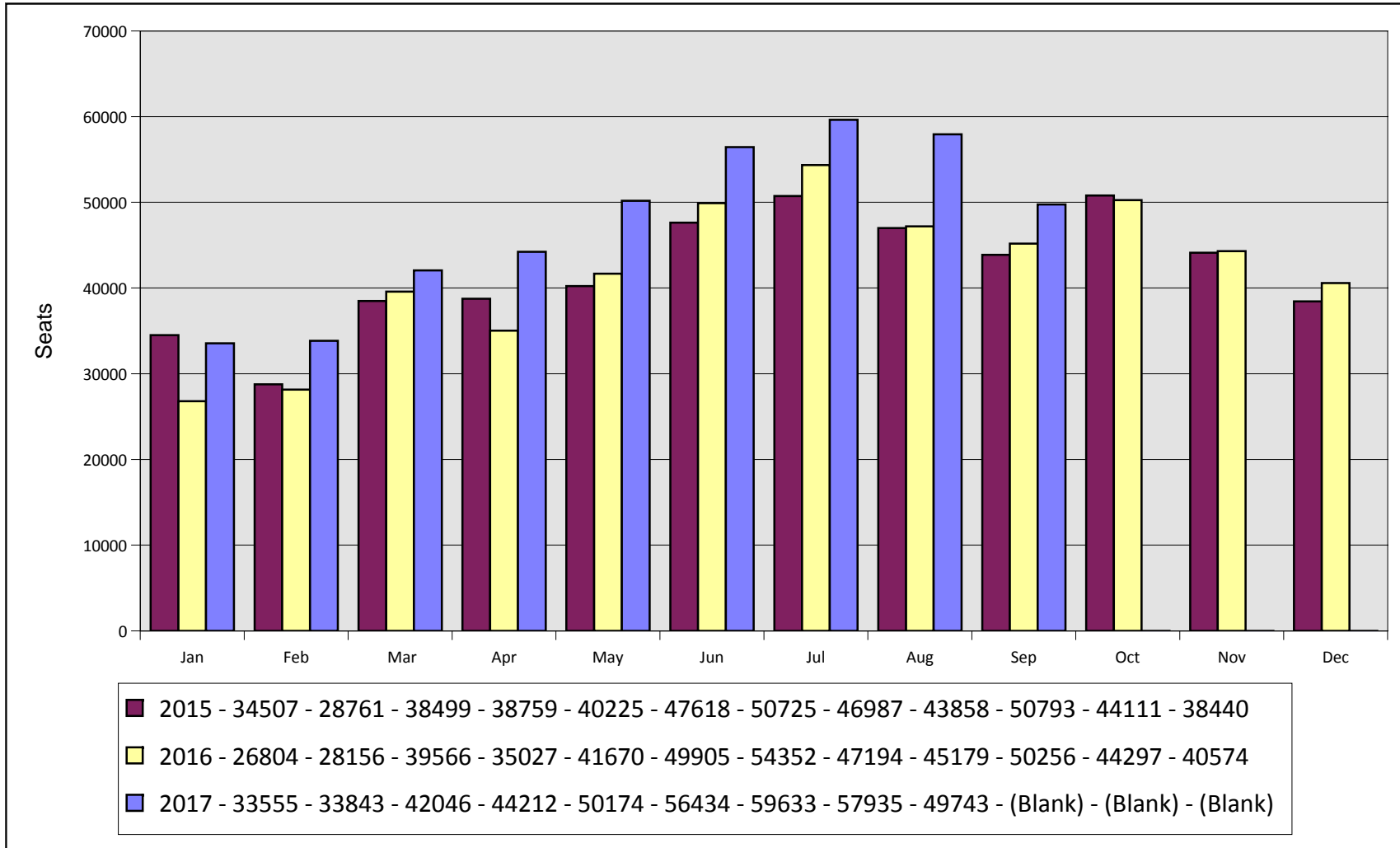


Airline	Scheduled Flights	Field	Cancellations Due To			Total Cancellations	Percentage of Completed
			Mechanical	Weather	Other		
Allegiant Air	82	0	0	0	0	0	100.0%
American Airlines	251	0	13	2	0	15	94.0%
Delta Air Lines	207	0	0	0	0	0	100.0%
Elite Airways	10	0	0	0	0	0	100.0%
United Airlines	149	0	0	0	0	0	100.0%
Total	699	0	13	2	0	15	97.9%

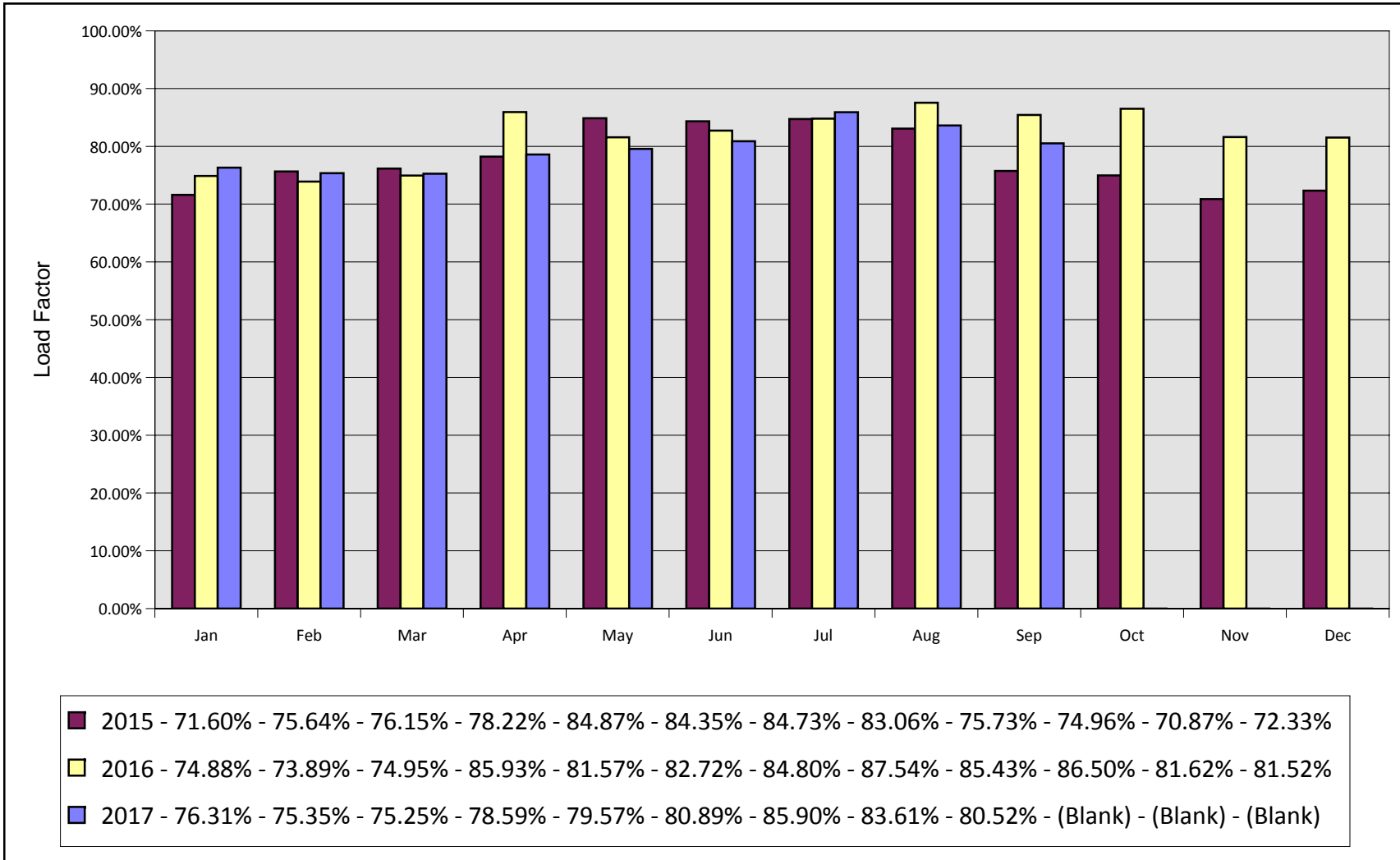
Monthly Enplanements By Year Asheville Regional Airport



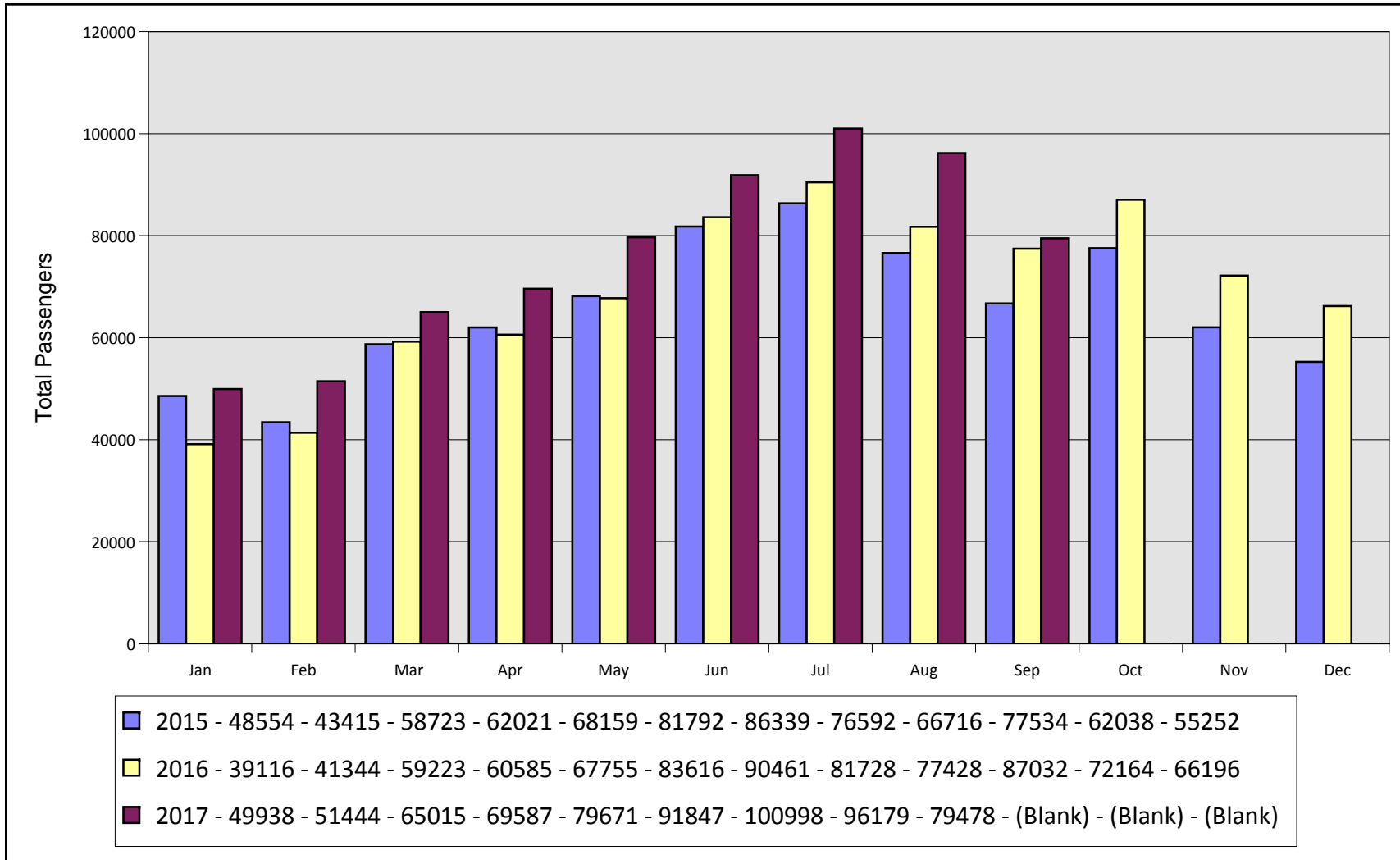
Monthly Seats By Year Asheville Regional Airport



Monthly Load Factors By Year Asheville Regional Airport



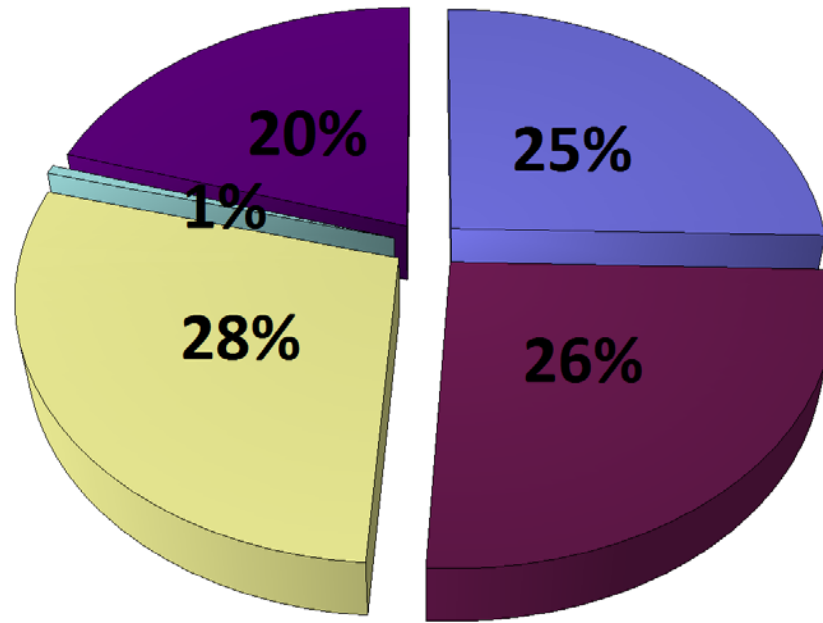
Total Monthly Passengers By Year Asheville Regional Airport



Airline Market Share Analysis (Enplanements) Asheville Regional Airport



Report Period From September 2017 Through September 2017



Travel Period			Nov 2017		Nov 2016		Diff		Percent Diff	
Mkt AI	Orig	Dest	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats
AA	AVL	CLT	52	2,900	46	2,691	6	209	13.0%	7.8%
AA	CLT	AVL	52	2,900	46	2,691	6	209	13.0%	7.8%
DL	ATL	AVL	52	3,320	46	3,742	6	(422)	13.0%	(11.3%)
DL	AVL	ATL	52	3,380	46	3,682	6	(302)	13.0%	(8.2%)
G4	AVL	BWI	1	177	1	166	0	11	0.0%	6.6%
G4	AVL	EWR	2	354	3	498	(1)	(144)	(33.3%)	(28.9%)
G4	AVL	FLL	7	1,239	7	1,173	0	66	0.0%	5.6%
G4	AVL	PGD	2	354	3	498	(1)	(144)	(33.3%)	(28.9%)
G4	AVL	PIE	4	708	4	675	0	33	0.0%	4.9%
G4	AVL	SFB	4	708	3	498	1	210	33.3%	42.2%
G4	BWI	AVL	1	177	1	166	0	11	0.0%	6.6%
G4	EWR	AVL	2	354	3	498	(1)	(144)	(33.3%)	(28.9%)
G4	FLL	AVL	7	1,239	7	1,173	0	66	0.0%	5.6%
G4	PGD	AVL	2	354	3	498	(1)	(144)	(33.3%)	(28.9%)
G4	PIE	AVL	4	708	4	675	0	33	0.0%	4.9%
G4	SFB	AVL	4	708	3	498	1	210	33.3%	42.2%
UA	AVL	EWR	7	836	7	350	0	486	0.0%	138.9%
UA	AVL	ORD	21	1,050	11	550	10	500	90.9%	90.9%
UA	EWR	AVL	7	836	7	350	0	486	0.0%	138.9%
UA	ORD	AVL	21	1,050	11	550	10	500	90.9%	90.9%
TOTAL			304	23,352	262	21,622	42	1,730	16.0%	8.0%

Travel Period			Dec 2017		Dec 2016		Diff		Percent Diff	
Mkt AI	Orig	Dest	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats
AA	AVL	CLT	48	2,825	42	2,370	6	455	14.3%	19.2%
AA	CLT	AVL	48	2,838	42	2,357	6	481	14.3%	20.4%
DL	ATL	AVL	44	2,980	41	2,975	3	5	7.3%	0.2%
DL	AVL	ATL	44	2,980	41	2,975	3	5	7.3%	0.2%
G4	AVL	BWI	2	354	2	332	0	22	0.0%	6.6%
G4	AVL	EWR	2	354	4	664	(2)	(310)	(50.0%)	(46.7%)
G4	AVL	FLL	6	1,062	5	841	1	221	20.0%	26.3%
G4	AVL	PGD	3	531	2	332	1	199	50.0%	59.9%
G4	AVL	PIE	4	708	2	332	2	376	100.0%	113.3%
G4	AVL	SFB	3	531	2	332	1	199	50.0%	59.9%
G4	BWI	AVL	2	354	2	332	0	22	0.0%	6.6%
G4	EWR	AVL	2	354	4	664	(2)	(310)	(50.0%)	(46.7%)
G4	FLL	AVL	6	1,062	5	841	1	221	20.0%	26.3%
G4	PGD	AVL	3	531	2	332	1	199	50.0%	59.9%
G4	PIE	AVL	4	708	2	332	2	376	100.0%	113.3%
G4	SFB	AVL	3	531	2	332	1	199	50.0%	59.9%
UA	AVL	EWR	7	826	7	350	0	476	0.0%	136.0%
UA	AVL	ORD	21	1,050	11	550	10	500	90.9%	90.9%
UA	EWR	AVL	7	826	7	350	0	476	0.0%	136.0%
UA	ORD	AVL	21	1,050	11	550	10	500	90.9%	90.9%
TOTAL			280	22,455	236	18,143	44	4,312	18.6%	23.8%

Travel Period			Jan 2018		Jan 2017		Diff		Percent Diff	
Mkt AI	Orig	Dest	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats	Ops/Week	Seats
AA	AVL	CLT	33	2,261	38	2,122	(5)	139	(13.2%)	6.6%
AA	CLT	AVL	33	2,261	38	2,148	(5)	113	(13.2%)	5.3%
DL	ATL	AVL	39	2,816	38	2,843	1	(27)	2.6%	(0.9%)
DL	AVL	ATL	39	2,816	38	2,843	1	(27)	2.6%	(0.9%)
G4	AVL	EWR	2	354	4	664	(2)	(310)	(50.0%)	(46.7%)
G4	AVL	FLL	7	1,239	4	675	3	564	75.0%	83.6%
G4	AVL	PGD	2	354	2	332	0	22	0.0%	6.6%
G4	AVL	PIE	4	708	2	332	2	376	100.0%	113.3%
G4	AVL	SFB	2	354	2	332	0	22	0.0%	6.6%
G4	EWR	AVL	2	354	4	664	(2)	(310)	(50.0%)	(46.7%)
G4	FLL	AVL	7	1,239	4	675	3	564	75.0%	83.6%
G4	PGD	AVL	2	354	2	332	0	22	0.0%	6.6%
G4	PIE	AVL	4	708	2	332	2	376	100.0%	113.3%
G4	SFB	AVL	2	354	2	332	0	22	0.0%	6.6%
UA	AVL	EWR	6	768	0	0	6	768	0.0%	0.0%
UA	AVL	ORD	14	700	14	700	0	0	0.0%	0.0%
UA	EWR	AVL	6	768	0	0	6	768	0.0%	0.0%
UA	ORD	AVL	14	700	14	700	0	0	0.0%	0.0%
TOTAL			218	19,108	208	16,026	10	3,082	4.8%	19.2%



MEMORANDUM

TO: Members of the Airport Authority
FROM: Janet Burnette, Director of Finance & Accounting
DATE: November 9, 2017

ITEM DESCRIPTION – Information Section Item B

Greater Asheville Regional Airport – Explanation of Extraordinary Variances
Month of September 2017 (Month 3 of FY2018)

SUMMARY

Operating Revenues for the month of September were \$1,052,878, 16.98% over budget. Operating Expenses for the month were \$1,008,520, 15.66% over budget. As a result, Net Operating Revenues before Depreciation were \$44,358, 58.27% over budget. Net Non-Operating Revenues were \$279,305, 14.57% over budget.

Year-to-date Operating Revenues were \$3,049,060, 10.34% over budget. Year-to-date Operating Expenses were \$2,136,765, 1.12% under budget. Year-to-date Net Operating Revenues before Depreciation were \$912,295, 51.41% over budget. Net Non-Operating Revenues for the year were \$912,565, 8.34% over budget.

REVENUES

Significant variations to budget for September were:

Terminal Rental-Airlines	(\$19,305)	(12.62%)	Refunds of prior year non-signatory fees
Concessions	\$10,469	29.64%	Enplanements over budget
Auto Parking	\$17,207	6.20%	Enplanements over budget
Rental Car – Rentals	\$194,683	148.40%	Recorded revenue after annual true-up
Landing Fees	(\$55,149)	(73.77%)	Refunds of prior year non-signatory fees

Information Section – Item B



EXPENSES

Significant variations to budget for September were:

Contractual Services	\$98,667	95.89%	Payment for current and prior month parking management
Professional Services	\$20,088	112.27%	Legal fees and airline rate consulting fees
Utility Services	(\$13,719)	(35.07%)	Electricity and natural gas use remain low

STATEMENT OF NET ASSETS

Significant variations to prior month were:

Cash and Cash Equivalents – Cash and Cash Equivalents increased by \$1,422k mainly due to receipt of AIP grant funds.

Grants Receivable – Grants Receivable decreased by \$3,191k due to receipt of grant funds.

Construction in Progress – Construction in Progress increased by \$1,389k mainly due to the Airfield Redevelopment project and the parking garage.

Property and Equipment, Net – Property and Equipment, Net decreased by \$389k due to depreciation.

Net Pension Asset – LGERS – Net Pension Asset decreased by \$854k due to reclassification to current year contributions and expense, per LGERS adjustments.

Contributions in Current Year – Contributions in Current Year (Pension) increased by \$722k due to reclassification of Net Pension Asset – LGERS (see above).

**ASHEVILLE REGIONAL AIRPORT
INVESTMENT AND INTEREST INCOME SUMMARY
As of September 30, 2017**

<u>Institution:</u>	<u>Interest Rate</u>	<u>Investment Amount</u>	<u>Monthly Interest</u>
Bank of America - Operating Account	0.20%	\$ 10,041,456	1,480
First Citizens - Money Market Account	0.05%	6,393,986	263
NC Capital Management Trust - Cash Portfolio		17,324	13
NC Capital Management Trust - Term Portfolio		3,054,916	2,883
Petty Cash		200	
 <u>Restricted Cash:</u>			
BNY Mellon		5,860,421	
Bank of America - PFC Revenue Account	0.20%	7,550,904	1,238
		425,667	
Total		<u><u>\$ 33,344,874</u></u>	<u><u>\$ 5,877</u></u>

Investment Diversification:

Banks	91%
NC Capital Management Trust	9%
Commercial Paper	0%
Federal Agencies	0%
US Treasuries	0%
	<u><u>100%</u></u>

ASHEVILLE REGIONAL AIRPORT
STATEMENT OF CHANGES IN FINANCIAL POSITION
For the Month Ended September 30, 2017

	Current Month	Prior Period
Cash and Investments Beginning of Period	<u>\$ 31,921,966</u>	<u>\$ 31,409,809</u>
Net Income/(Loss) Before Capital Contributions	(84,884)	520,343
Depreciation	389,296	389,298
Decrease/(Increase) in Receivables	4,105,942	(1,135,420)
Increase/(Decrease) in Payables	(1,334,962)	2,127,690
Decrease/(Increase) in Prepaid Expenses	29,395	4,118
Decrease/(Increase) in Fixed Assets	(1,389,520)	(4,691,445)
Principal Payments of Bond Maturities	(454,629)	38,528
Capital Contributions	162,270	3,259,045
Increase(Decrease) in Cash	<u>1,422,908</u>	<u>512,157</u>
Cash and Investments End of Period	<u><u>\$ 33,344,874</u></u>	<u><u>\$ 31,921,966</u></u>

Asheville Regional Airport
Detailed Statement of Revenue, Expenses and Changes in Net Assets
For the Month Ending September 30, 2017

	<u>Current Month Actual</u>	<u>Current Month Budget</u>	<u>Variance \$</u>	<u>Variance %</u>	<u>YTD Actual</u>	<u>YTD Budget</u>	<u>Variance \$</u>	<u>Variance %</u>	<u>Annual Budget</u>
Operating Revenue:									
Terminal Space Rentals - Non Airline	\$19,978	\$19,978	\$0	0.00%	\$59,935	\$59,934	\$1	0.00%	\$243,049
Terminal Space Rentals - Airline	133,640	152,945	(19,305)	(12.62%)	440,774	478,726	(37,952)	(7.93%)	1,571,724
Concessions	45,791	35,322	10,469	29.64%	147,190	104,531	42,659	40.81%	444,480
Auto Parking	294,957	277,750	17,207	6.20%	947,770	849,500	98,270	11.57%	3,270,000
Rental Car - Car Rentals	325,870	131,187	194,683	148.40%	602,740	395,561	207,179	52.38%	1,579,656
Rental Car - Facility Rent	53,174	52,378	796	1.52%	169,037	156,090	12,947	8.29%	627,526
Commercial Ground Transportation	9,248	2,500	6,748	269.92%	28,457	12,500	15,957	127.66%	75,000
Landing Fees	19,606	74,755	(55,149)	(73.77%)	183,713	241,783	(58,070)	(24.02%)	774,903
FBO'S	88,399	85,783	2,616	3.05%	264,363	260,349	4,014	1.54%	1,021,005
Building Leases	13,018	12,903	115	0.89%	38,903	38,709	194	0.50%	113,820
Land Leases	4,004	4,913	(909)	(18.50%)	13,919	14,739	(820)	(5.56%)	58,968
Other Leases/Fees	45,193	49,599	(4,406)	(8.88%)	152,259	151,033	1,226	0.81%	528,639
Total Operating Revenue	\$1,052,878	\$900,013	\$152,865	16.98%	\$3,049,060	\$2,763,455	\$285,605	10.34%	\$10,308,770
Operating Expenses:									
Personnel Services	\$568,115	\$578,536	(\$10,421)	(1.80%)	\$1,248,583	\$1,281,331	(\$32,748)	(2.56%)	\$5,400,329
Professional Services	37,981	17,893	20,088	112.27%	64,796	47,911	16,885	35.24%	262,250
Accounting & Auditing	8,650	3,000	5,650	188.33%	15,150	5,000	10,150	203.00%	16,200
Other Contractual Services	201,568	102,901	98,667	95.89%	337,742	327,140	10,602	3.24%	960,194
Travel & Training	23,640	18,021	5,619	31.18%	46,203	34,963	11,240	32.15%	209,650
Communications & Freight	5,542	8,825	(3,283)	(37.20%)	18,543	25,974	(7,431)	(28.61%)	103,900
Utility Services	25,396	39,115	(13,719)	(35.07%)	92,420	129,935	(37,515)	(28.87%)	520,817
Rentals & Leases	995	1,033	(38)	(3.68%)	8,070	2,699	5,371	199.00%	12,000
Insurance	16,679	21,058	(4,379)	(20.79%)	50,313	63,174	(12,861)	(20.36%)	252,700
Repairs & Maintenance	15,919	20,773	(4,854)	(23.37%)	64,313	65,934	(1,621)	(2.46%)	266,530
Advertising, Printing & Binding	11,848	13,139	(1,291)	(9.83%)	17,571	21,617	(4,046)	(18.72%)	257,790
Promotional Activities	486	7,525	(7,039)	(93.54%)	19,572	28,975	(9,403)	(32.45%)	108,950
Other Current Charges & Obligations	14,892	8,625	6,267	72.66%	29,253	25,200	4,053	16.08%	104,000
Office Supplies	493	750	(257)	(34.27%)	1,189	2,250	(1,061)	(47.16%)	9,000
Operating Supplies	25,067	29,076	(4,009)	(13.79%)	66,895	92,028	(25,133)	(27.31%)	476,793
Books, Publications, Subscriptions & Mem	3,053	1,717	1,336	77.81%	5,597	6,796	(1,199)	(17.64%)	59,870
Contingency	-	-	-	100.00%	-	-	-	100.00%	100,000
Emergency Repair	-	-	-	100.00%	-	-	-	100.00%	50,000
Business Development	48,196	-	48,196	100.00%	50,555	-	50,555	100.00%	300,000
Total Operating Expenses	\$1,008,520	\$871,987	\$136,533	15.66%	\$2,136,765	\$2,160,927	(\$24,162)	(1.12%)	\$9,470,973

Asheville Regional Airport
Detailed Statement of Revenue, Expenses and Changes in Net Assets

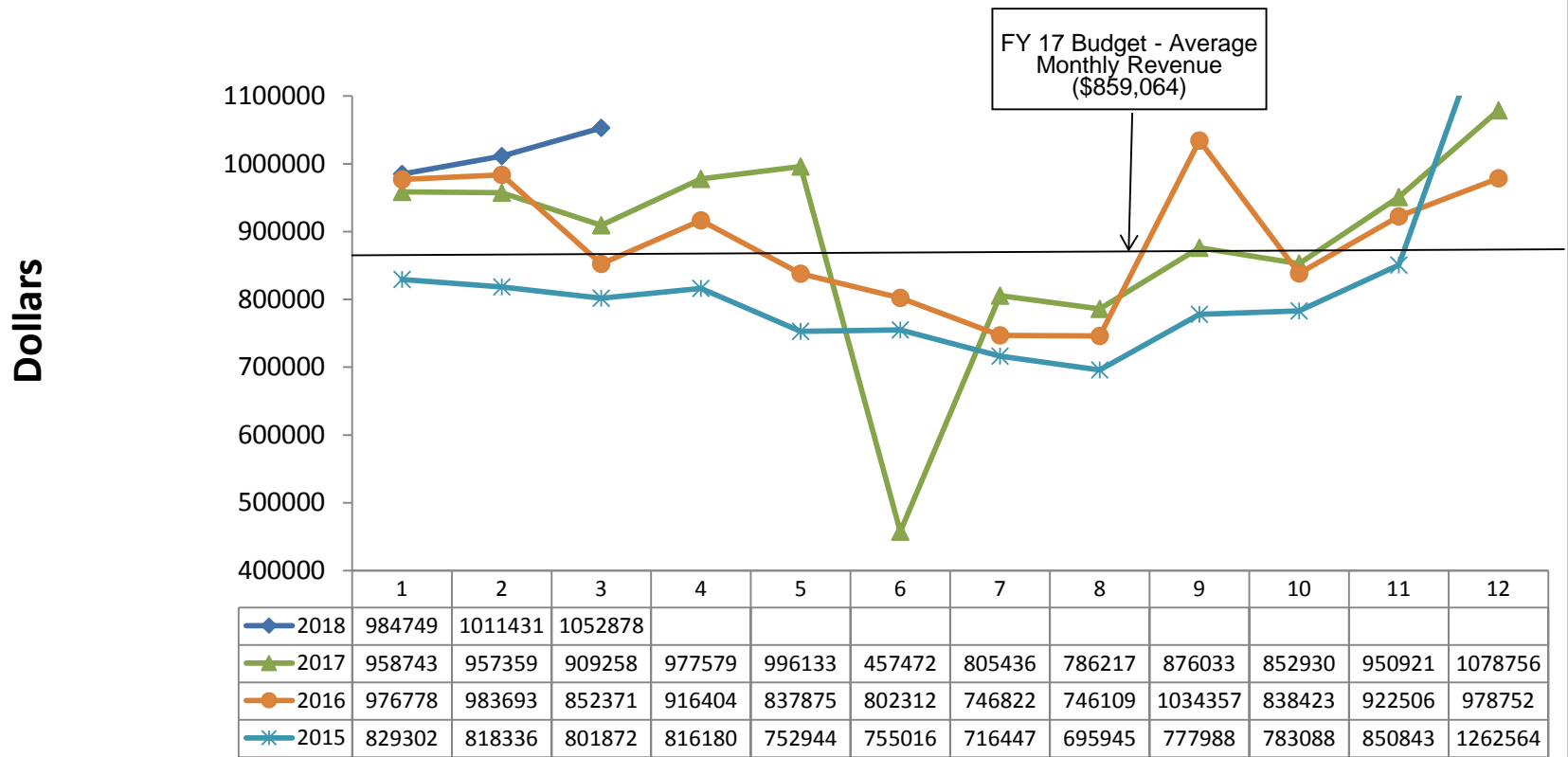
For the Month Ending September 30, 2017

	Current Month Actual	Current Month Budget	Variance \$	Variance %	YTD Actual	YTD Budget	Variance \$	Variance %	Annual Budget
Operating Revenue before Depreciation	\$44,358	\$28,026	\$16,332	58.27%	\$912,295	\$602,528	\$309,767	51.41%	\$837,797
Depreciation	389,296	-	389,296	100.00%	1,167,537	-	1,167,537	100.00%	-
Operating Income(Loss) Before Non-Operating Revenue and Expenses	(\$344,938)	\$28,026	(\$372,964)	(1,330.78%)	(\$255,242)	\$602,528	(\$857,770)	(142.36%)	\$837,797
Non-Operating Revenue and Expense									
Customer Facility Charges	\$119,803	\$121,500	(\$1,697)	(1.40%)	\$431,078	\$432,000	(\$922)	(0.21%)	\$ 1,350,000
Passenger Facility Charges	180,389	160,000	20,389	12.74%	572,182	523,500	48,682	9.30%	1,788,500
Interest Revenue	5,877	2,917	2,960	101.47%	17,346	8,750	8,596	98.24%	35,000
Interest Expense	(40,639)	(40,639)	-	0.00%	(121,916)	(121,916)	-	0.00%	(487,666)
Bond Expense	-	-	-	100.00%	-	-	-	100.00%	-
Reimbursable Cost Revenues	-	-	-	100.00%	-	-	-	100.00%	-
Reimbursable Cost Expenses	-	-	-	100.00%	-	-	-	100.00%	-
Gain/Loss on Disposal of Assets	13,875	0	13,875	0.00%	13,875	0	13,875	0.00%	-
Non-Operating Revenue-Net	\$279,305	\$243,778	\$35,527	14.57%	\$912,565	\$842,334	\$70,231	8.34%	\$2,685,834
Income (Loss) Before Capital Contributions	(\$65,633)	\$271,804	(\$337,437)	(124.15%)	\$657,323	\$1,444,862	(\$787,539)	(54.51%)	\$3,523,631
Capital Contributions	\$162,270	\$0	\$162,270	100.00%	\$3,507,334	\$0	\$3,507,334	100.00%	\$0
Increase in Net Assets	\$96,637	\$271,804	(\$175,167)	(64.45%)	\$4,164,657	\$1,444,862	\$2,719,795	188.24%	\$3,523,631

**ASHEVILLE REGIONAL AIRPORT
STATEMENT OF FINANCIAL POSITION
As of September 30, 2017**

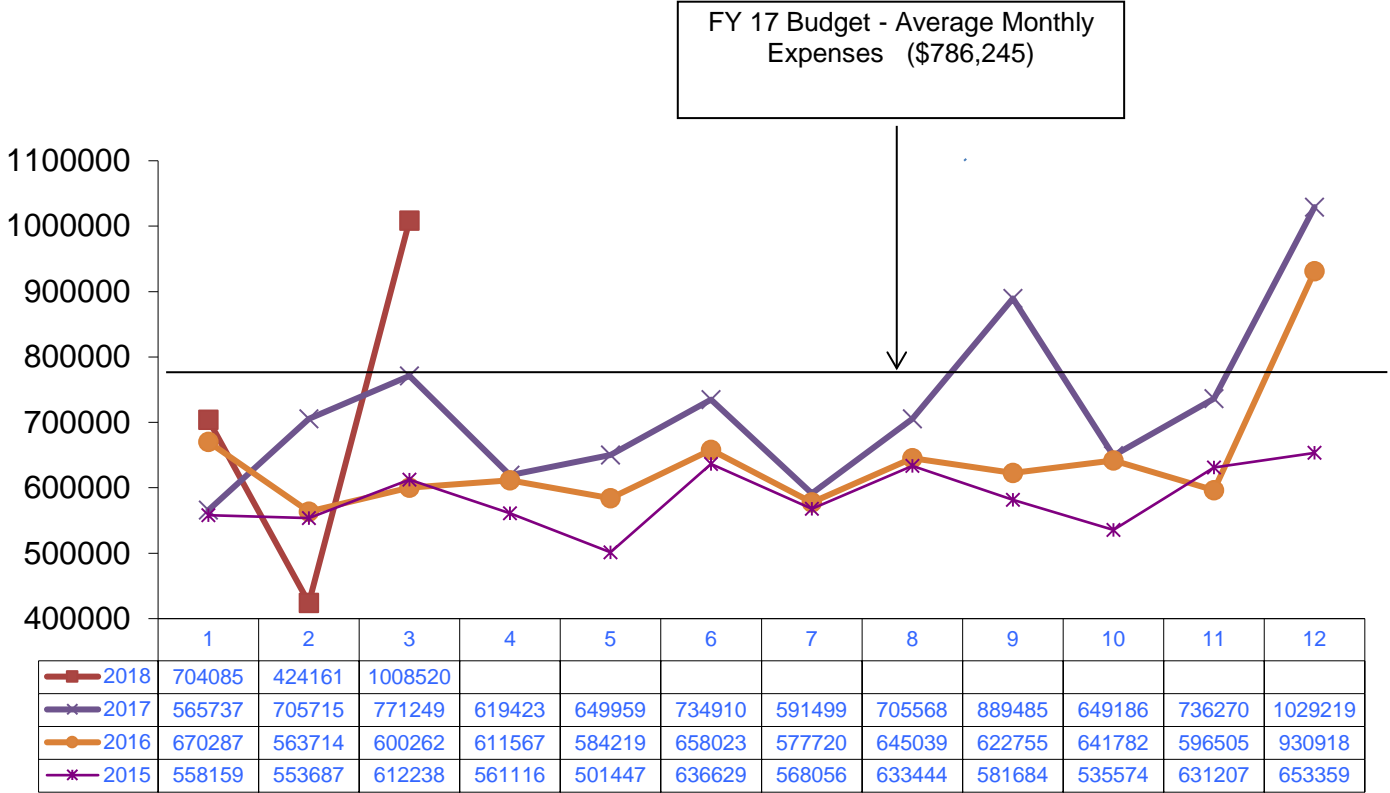
	Current Month	Last Month
<u>ASSETS</u>		
Current Assets:		
Unrestricted Net Assets:		
Cash and Cash Equivalents	\$19,507,882	\$15,476,541
Accounts Receivable	663,019	780,196
Passenger Facility Charges Receivable	350,000	300,000
Refundable Sales Tax Receivable	351,878	1,198,817
Grants Receivable	203,869	3,395,696
Prepaid Expenses	158,969	188,363
Total Unrestricted Assets	21,235,617	21,339,613
Restricted Assets:		
Cash and Cash Equivalents	13,836,992	16,445,425
Total Restricted Assets	13,836,992	16,445,425
Total Current Assets	35,072,609	37,785,038
Noncurrent Assets:		
Construction in Progress	68,655,846	67,266,327
Net Pension Asset - LGERS	(1,061,652)	(226,282)
Contributions in Current Year	944,231	222,035
Property and Equipment - Net	56,580,806	56,970,102
Total Noncurrent Assets	125,119,231	124,232,182
	\$160,191,840	\$162,017,220
<u>LIABILITIES AND NET ASSETS</u>		
Current Liabilities:		
Payable from Unrestricted Assets:		
Accounts Payable & Accrued Liabilities	\$2,553,446	\$3,633,281
Customer Deposits	17,210	17,210
Unearned Revenue	232,295	573,978
Construction Contract Retainages	2,080,038	2,508,003
Revenue Bond Payable - Current	1,215,000	1,215,000
Total Payable from Unrestricted Assets	6,097,989	7,947,472
Total Current Liabilities	6,097,989	7,947,472
Noncurrent Liabilities:		
Other Postemployment Benefits	1,209,372	1,202,917
Compensated Absences	350,006	350,006
Net Pension Obligation-LEO Special Separation Allowance	441,122	9,558
Revenue Bond Payable - Noncurrent	18,830,000	18,830,000
Total Noncurrent Liabilities	20,830,500	20,392,481
Total Liabilities	26,928,489	28,339,953
Net Assets:		
Invested in Capital Assets	105,191,652	104,191,429
Restricted	13,836,992	16,445,425
Unrestricted	14,234,707	13,040,413
Total Net Assets	133,263,351	133,677,267
	\$160,191,840	\$162,017,220

ASHEVILLE REGIONAL AIRPORT Annual Operating Revenue by Month September 2017



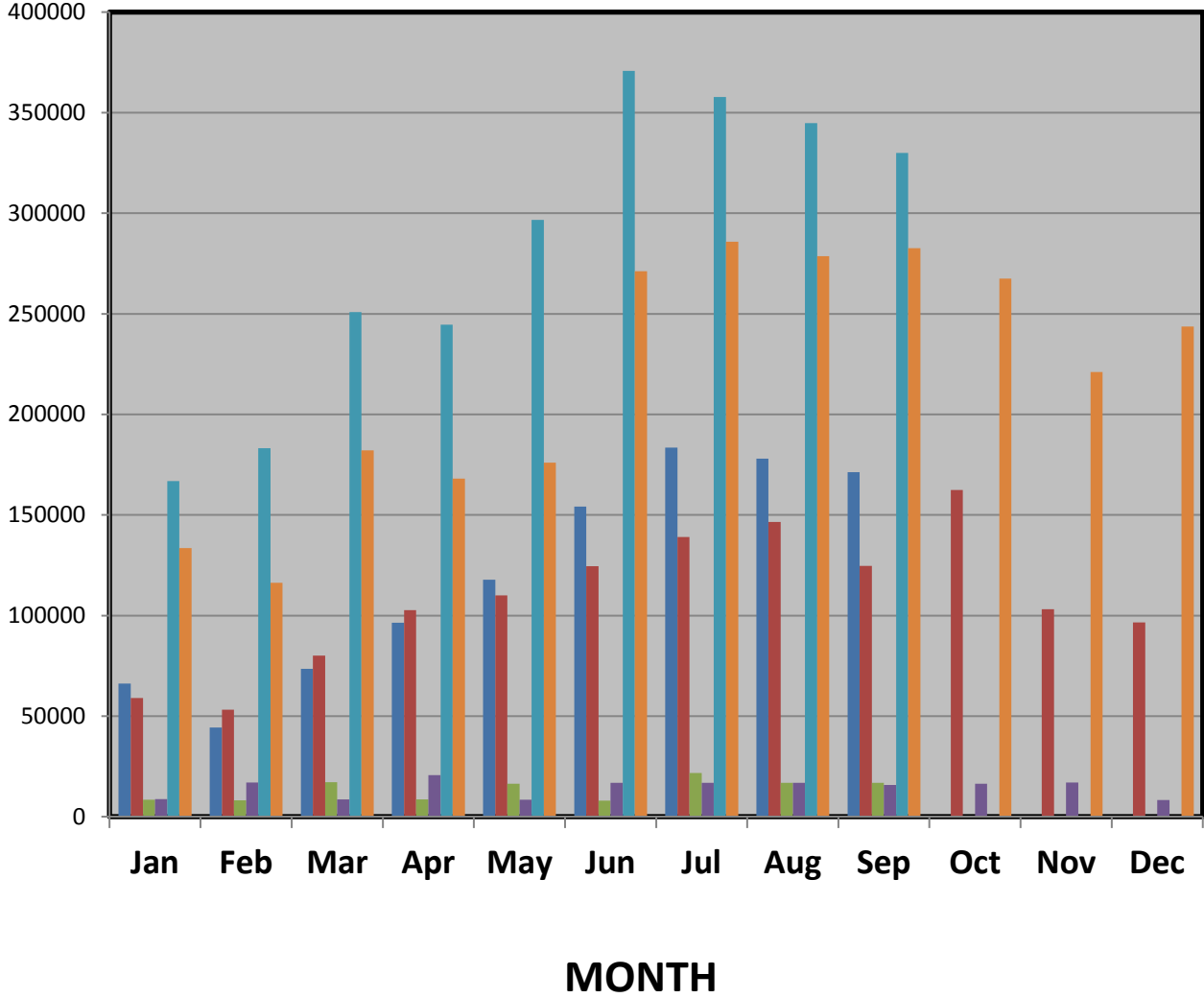
ASHEVILLE REGIONAL AIRPORT Annual Operating Expenses by Month September 2017

Dollars



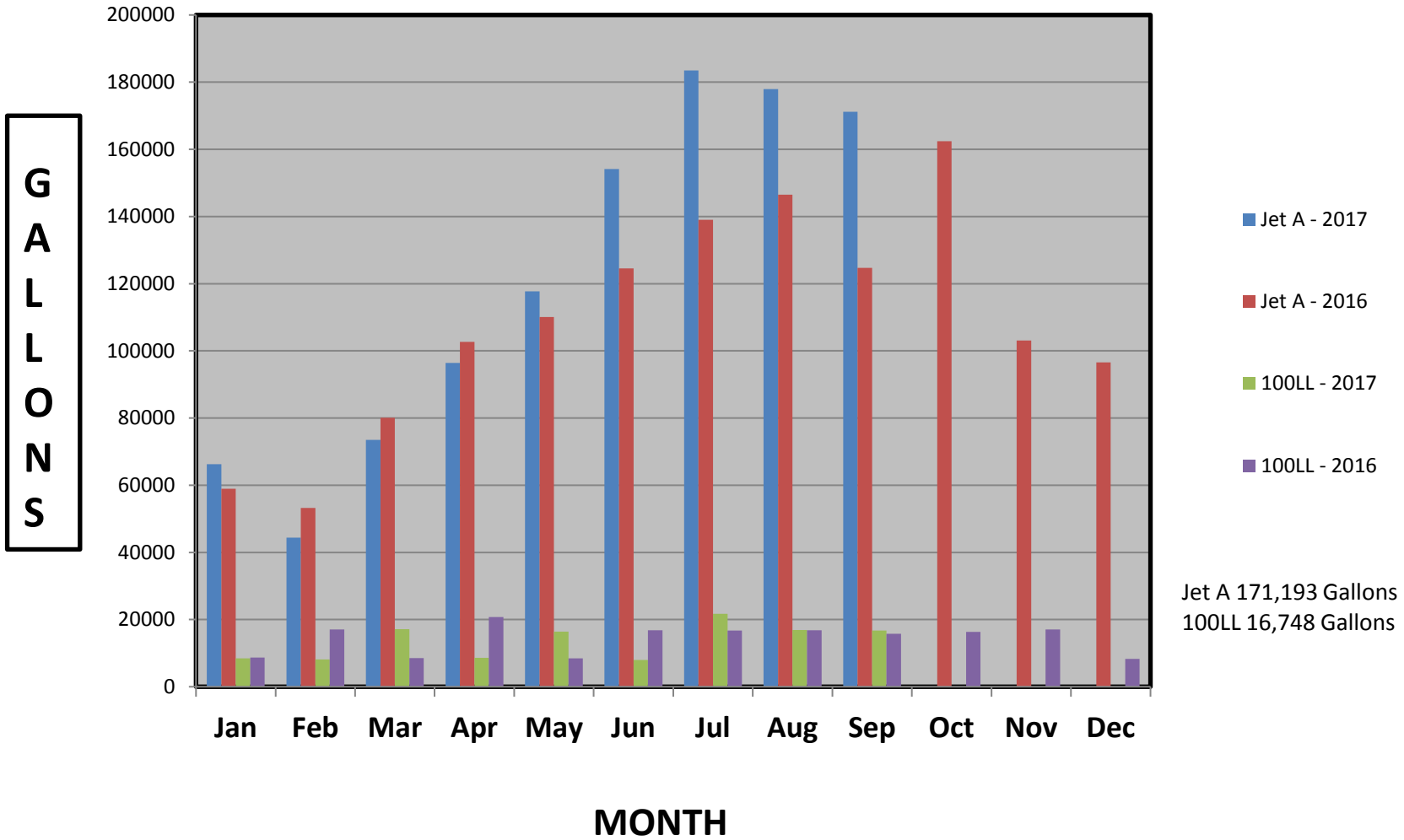
**ASHEVILLE REGIONAL AIRPORT
FUEL SALES - GALLONS
September 2017**

**G
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Jet A 171,193 Gallons
100LL 16,748 Gallons
Airline 329,930 Gallons

**ASHEVILLE REGIONAL AIRPORT
GENERAL AVIATION FUEL SALES - GALLONS
September 2017**



Design Phase														
Project Number	Project Name	Project Description	Professional Services Consultant	Professional Services Contract	General Contractor	Original Construction Contract	Change Orders (thru 11/01/2017)	Percent of Original Contract	Board Approved Project Cost	Percent Complete	Expensed to Date (thru 11/01/2017)	Start Date	End Date	Current Project Status (as of 11/01/2017)
1	Airfield Re-Development Project	Budget for the complete project				\$64,100,000.00	\$15,900,000.00		\$80,000,000.00	71.0%	\$56,730,284			All Engineer contracts, completed construction contracts and expenses will be inclusive of budget.
1A	Airfield Re-Development Project	Phase I - Design Services	RS&H	\$447,983.00	N/A	N/A	\$0.00	0.00%	(Overall total included in above number)	83%	\$372,161	Dec-12	Jun-16	Project Management work primarily complete.
1B	Airfield Re-Development Project	Phase II - Design Services and Project Management.	RS&H	\$1,842,318.00	N/A	N/A	\$0.00	0.00%	(Overall total included in above number)	95.0%	\$1,748,887	Jun-13	Dec-16	Project Management work continues, pending Contractor resolution.
1C	Airfield Re-Development Project	Phase III and IV - Design Services and Project Management.	RS&H	\$2,399,826.00	N/A	N/A	\$0.00	0.00%	(Overall total included in above number)	78.0%	\$1,870,921	Dec-14	May-18	Phase IV Project Management underway.
1D	Airfield Re-Development Project	New Runway Design	AVCON	\$1,967,476.00	N/A	N/A	\$0.00	0.00%	(Overall total included in above number)	94.0%	\$1,850,368	Mar-13	May-18	Phase IV Project Management underway.
1E	Airfield Re-Development Project	Miscellaneous and Administrative Expenses			N/A	N/A	\$0.00	0.00%	(Overall total included in above number)		\$3,766,649	Jan-13	Dec-17	Miscellaneous Administrative Expenses, Reimbursable Agreement and Land Acquisition
2	Parking Garage Project	Design and EA for approximately 1300 spaces of covered parking garage.	Delta Airport Consultants	\$1,627,575.00	N/A	N/A	\$0.00	0.00%	\$1,627,575.00	92.0%	\$1,497,901	Oct-15	Nov-17	Construction service and RPR Management continues.
3	Expand Air Carrier Apron	Design additional apron space to hold RON Aircraft.	Delta Airport Consultants	\$99,000.00	N/A	N/A	\$0.00	0.00%	\$99,000.00	97.0%	\$96,017	Feb-17	Jun-17	Project will go through new bid process early Spring 2018.

Construction Phase														
Project Number	Project Name	Project Description	Professional Services Consultant	Professional Services Contract	General Contractor	Original Construction Contract	Change Orders (thru 11/01/2017)	Percent of Original Contract	Board Approved Project Cost	Percent Complete	Expensed to Date (thru 11/01/2017)	Start Date	End Date	Current Project Status (as of 11/01/2017)
1	Parking Garage	Construct a 5 level parking garage for passenger/public parking.	Delta Airport Consultants	\$1,627,575.00	American South General Contractors	\$20,244,000.00	\$389,488.13	1.92%	\$21,938,700.00	82.0%	\$16,905,603	Sep-16	Nov-17	Interior finishes continue with electrical lighting, line striping, elevator installation, IT room conduit, drywall installation in stairwell A. Landscaping and sign installation beginning.
2	Permanent Runway 17-35 Construction, NAVAIDS and Taxiway Conversion	Construct new runway and convert temporary runway to a taxiway	RS&H and AVCON Inc.	Amount Included in Phase 3 Design Fees	Cedar Peaks Enterprises Inc.	\$33,703,095.70	\$0.00	0.00%	\$34,703,095.70	24.7%	\$8,343,589	Mar-17	Jun-18	Winter shutdown in process. Project to resume in Spring 2018.

(Construction and Administrative Costs included)

Airportsurvey.com



AVL

Airport Facilities Review For 3rd Quarter 2017

Welcome

- Welcome to the Airportsurvey.com Airport Facilities Review for the recent quarter, a complimentary data set provided to Airportsurvey.com participating airports
- The following slides provide non-weighted scores and ratings based on an independent survey of air travelers
- Note that passenger responses are based on perception, rather than objective assessment
- Value Added Services available from Canmark include:
 - Report analysis
 - Statistical testing
 - Air carrier responses
 - Non-facility responses
 - Tailored comparison sets
 - Passenger demographics
 - Sample size enhancement
 - Targeted and customized reporting
 - Custom survey questions and content

No representations are made as to the completeness or accuracy of information contained herein. Airport facility raw data is available upon request.

Proprietary and Confidential

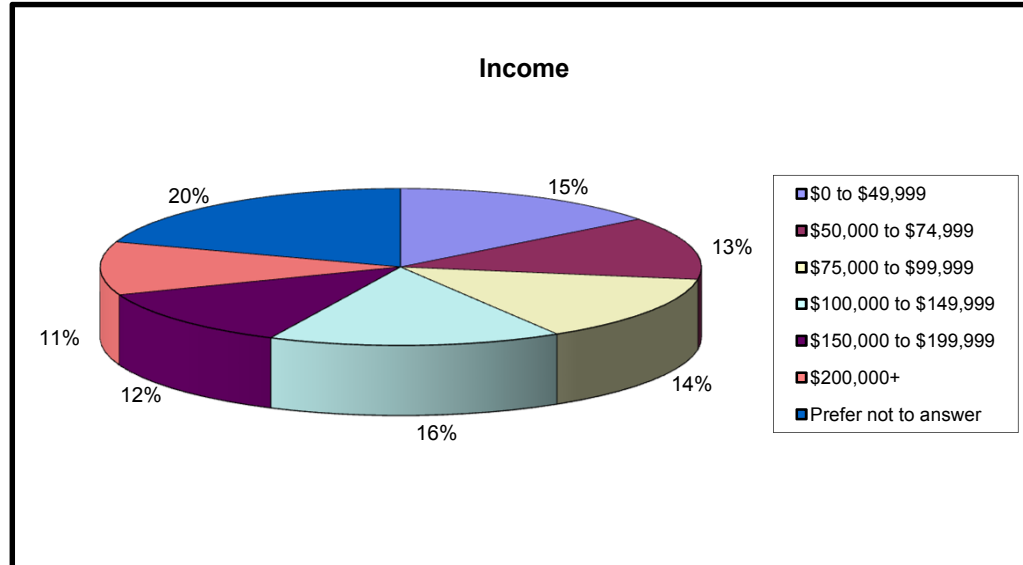
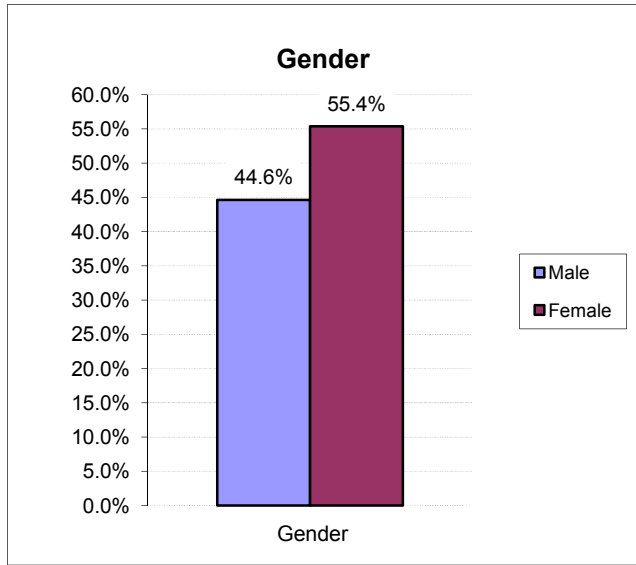
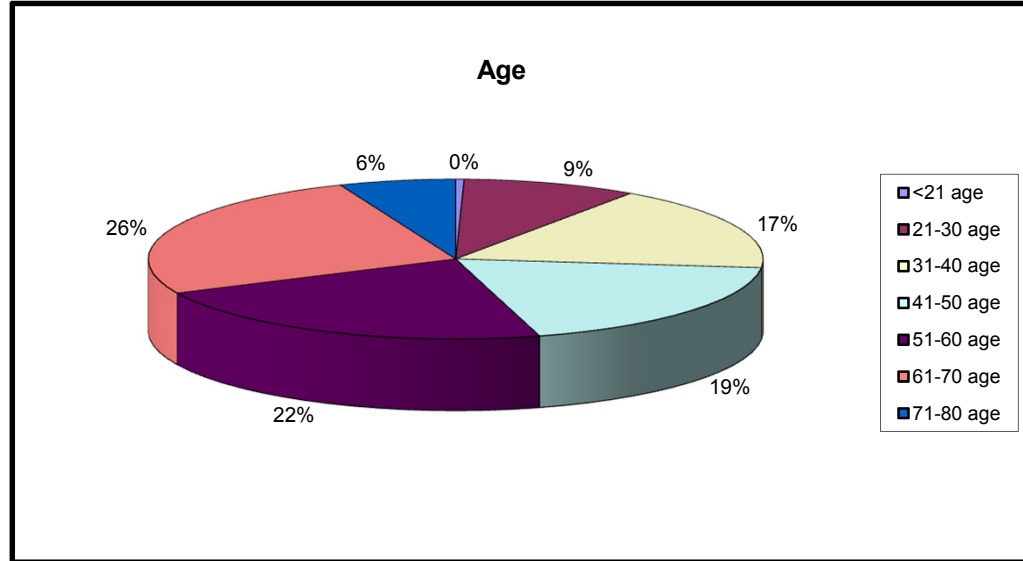
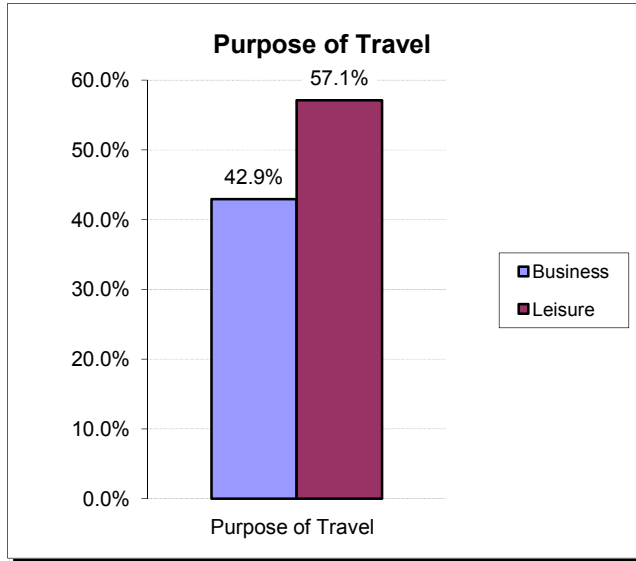
Overview

- ***Airportsurvey.com*** is an ongoing, all-inclusive online passenger satisfaction survey program from Canmark Research Center
- Invitations to take the survey are distributed at select airports across the country
- Over 30 airports participate
- Each survey invitation card is single-use, and must reference an actual flight
- Survey distribution occurs approximately three days per month
- Response scale is 1 through 5: Poor, Fair, Good, Very Good, Excellent
- Survey participants have a chance to win round-trip airline tickets
- Response rates vary from 10% to 20% based on location
- Facilities attributes are scored according to check-in airport
- Airports are grouped into three tiers according to DOT originating revenue*

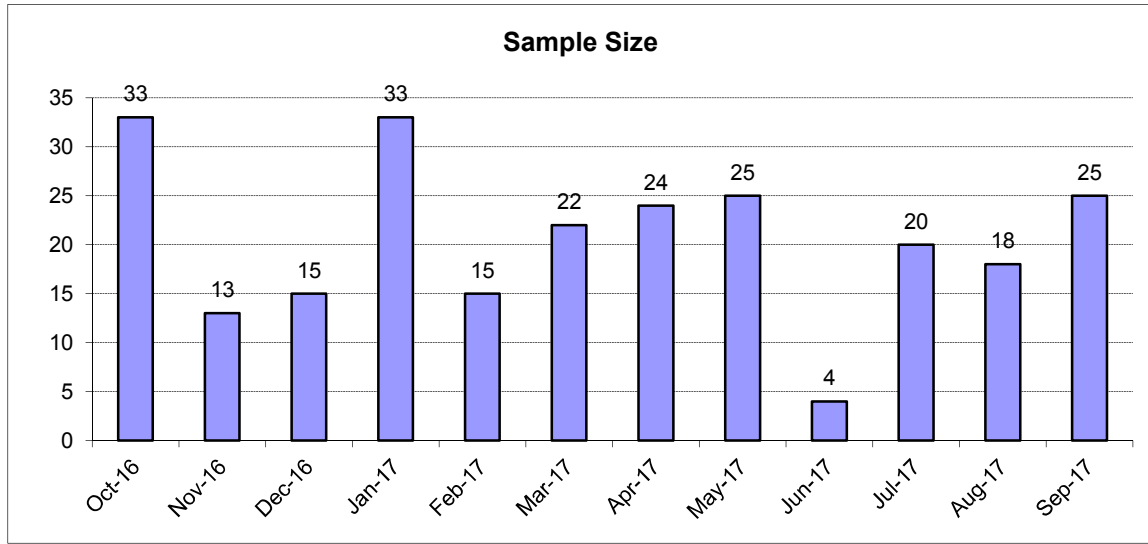
*Updated 4rd Quarter 2012

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Passenger Demographics



General Findings



Sample is clustered around airport invitation distribution dates.

Sample reflects passengers intercepted at arrival airports who rated check-in airport.

	AVL	Similar	+/-	Pct
Overall	4.26	4.29	(0.03)	-0.7%
Availability of parking	3.54	4.14	(0.61)	-17.2%
Cost of parking	3.48	3.67	(0.19)	-5.4%
Clear, easy to follow signs	4.25	4.29	(0.04)	-0.9%
Cleanliness	4.41	4.36	0.05	1.2%
Availability of restrooms	4.39	4.37	0.03	0.6%
Cleanliness of restrooms	4.32	4.29	0.03	0.6%
Concessions / restaurants	3.55	3.76	(0.20)	-5.7%
Transportation to your gate / concourse / terminal	4.26	4.18	0.08	2.0%
Airport Wi-Fi ease of use	4.17	3.95	0.21	5.1%
Overall airport Wi-Fi	4.03	3.88	0.15	3.7%
Overall departure airport concourse	4.24	4.22	0.02	0.4%
Security: Wait time at checkpoint	4.26	4.37	(0.10)	-2.5%
Security: Professionalism of personnel	4.37	4.41	(0.04)	-1.0%
Security: Confidence in airport security procedures	4.28	4.26	0.02	0.4%

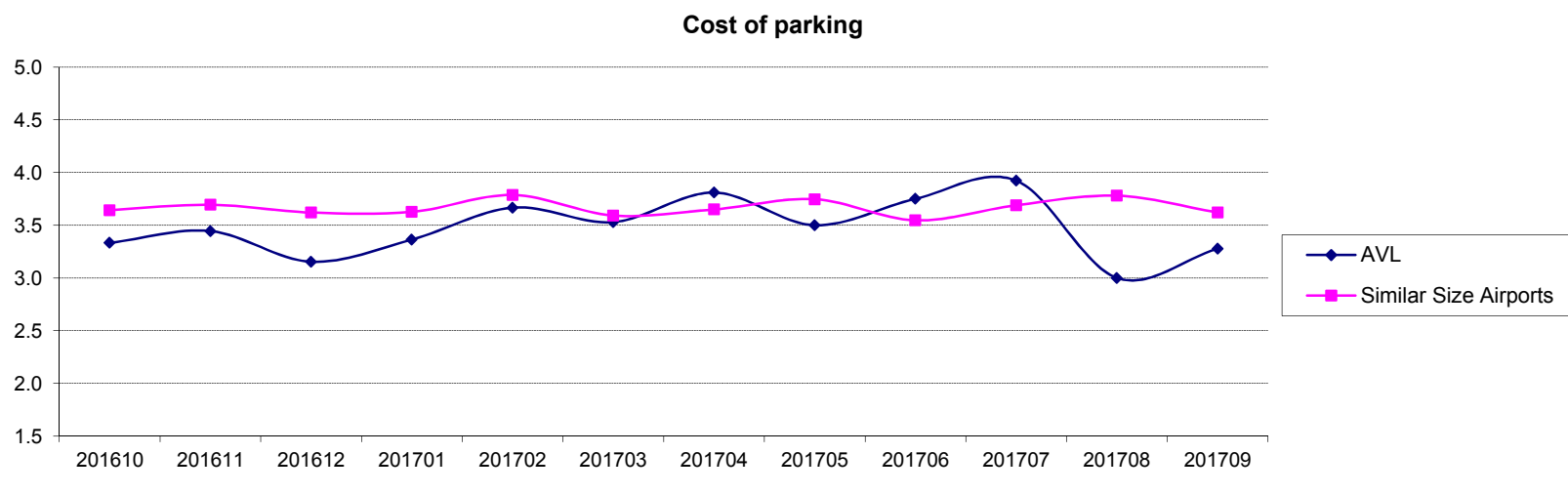
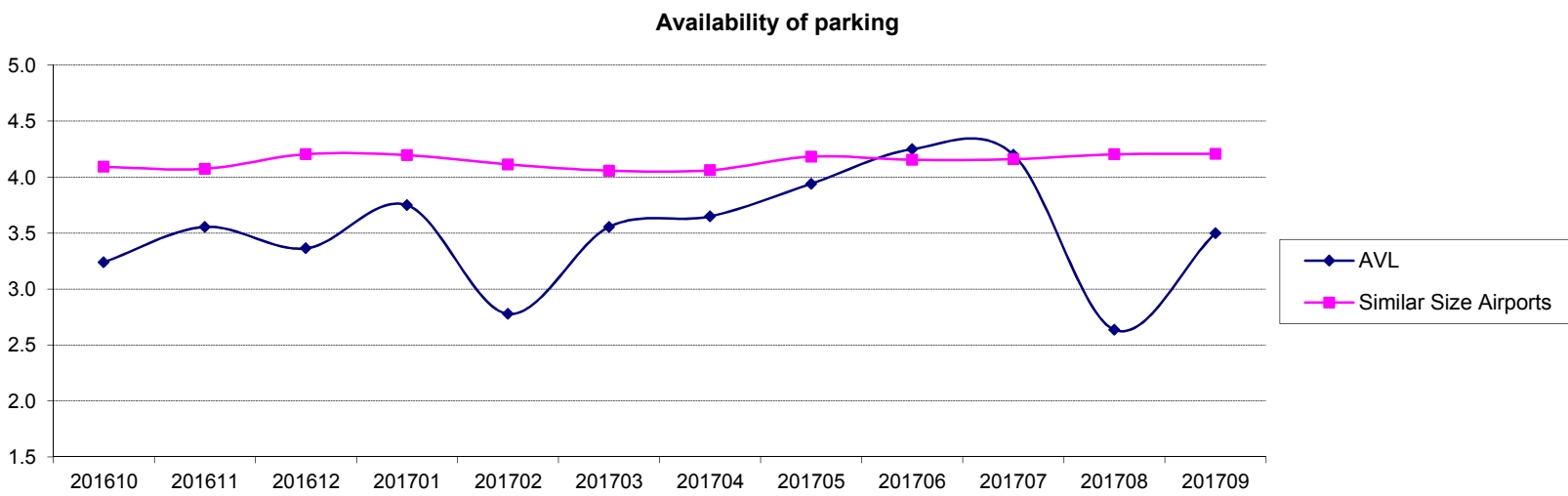
Statistical means testing not performed on results

AVL	
Responses	247

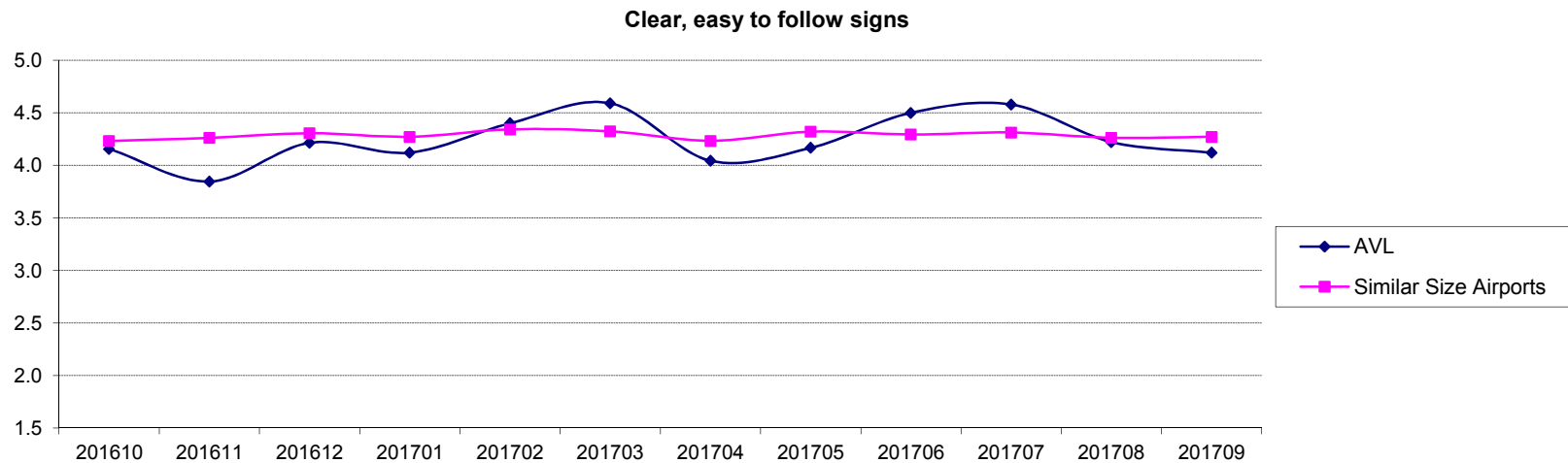
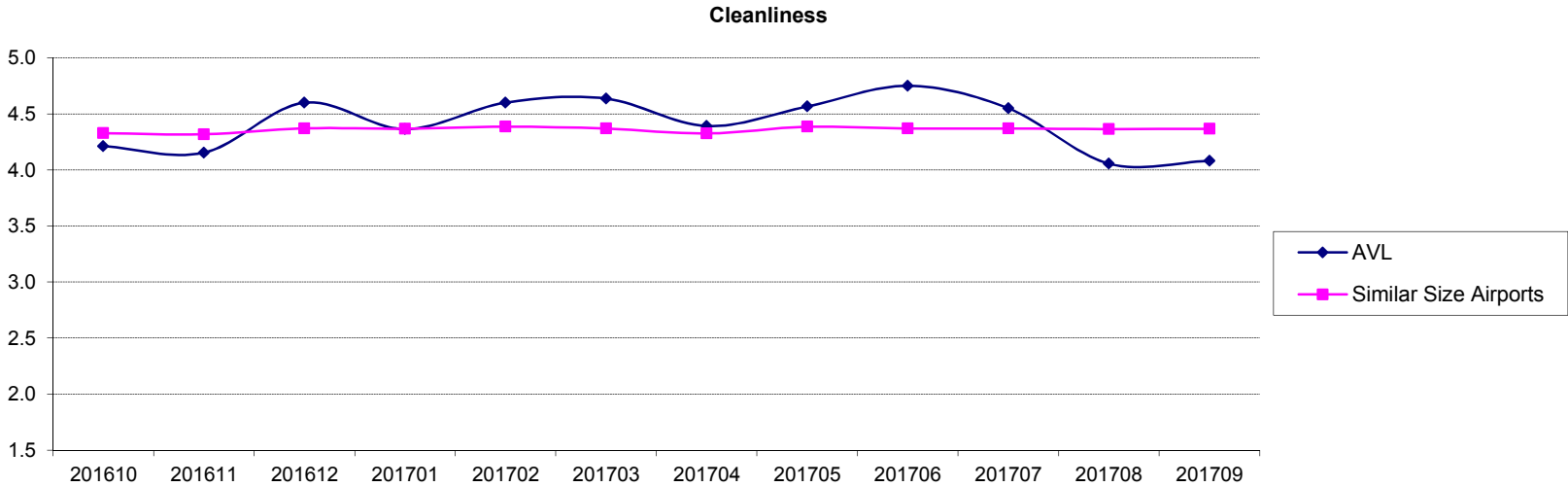
Overall Satisfaction with Airport Facilities



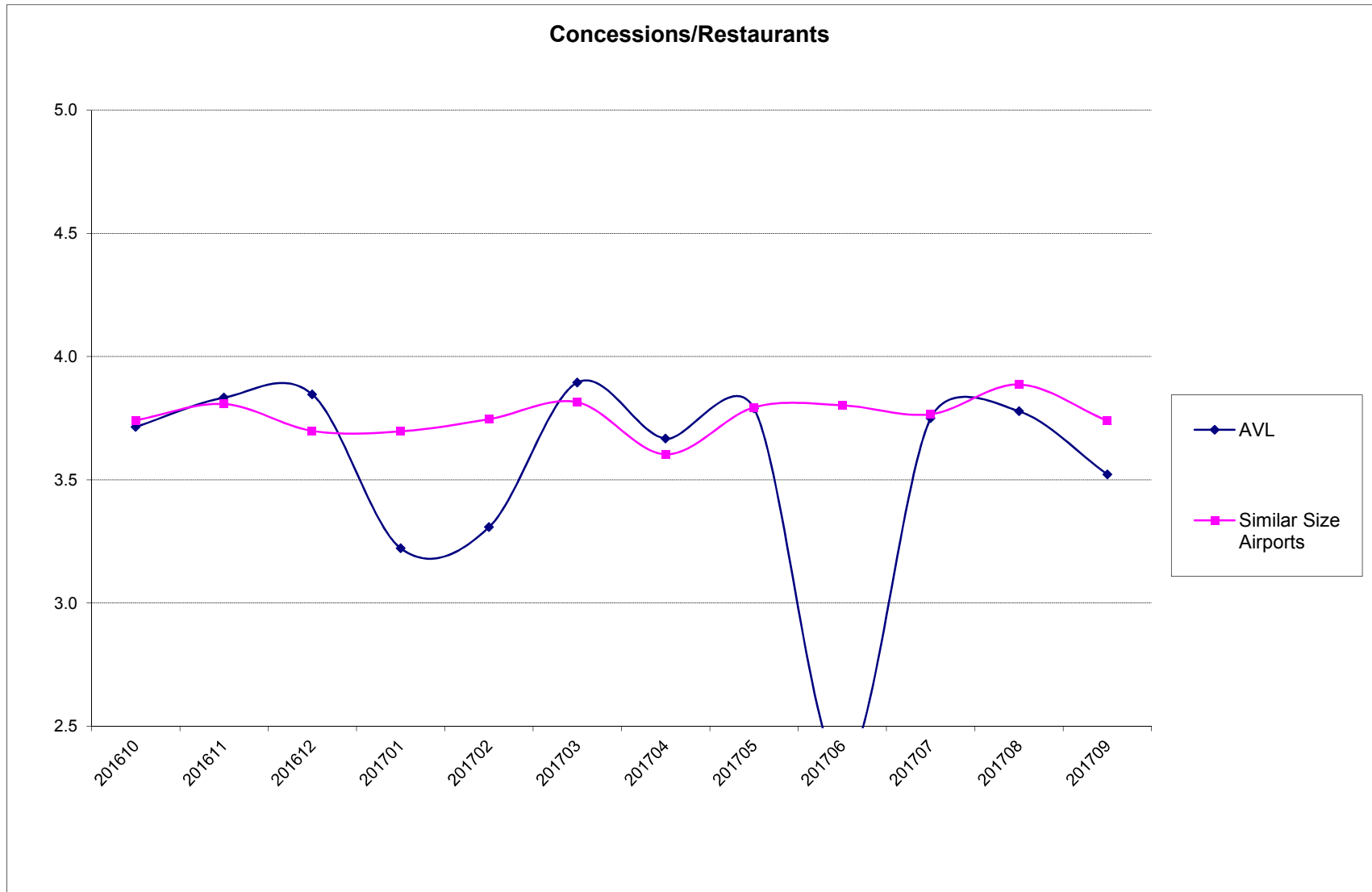
Parking Satisfaction



Cleanliness and Signage

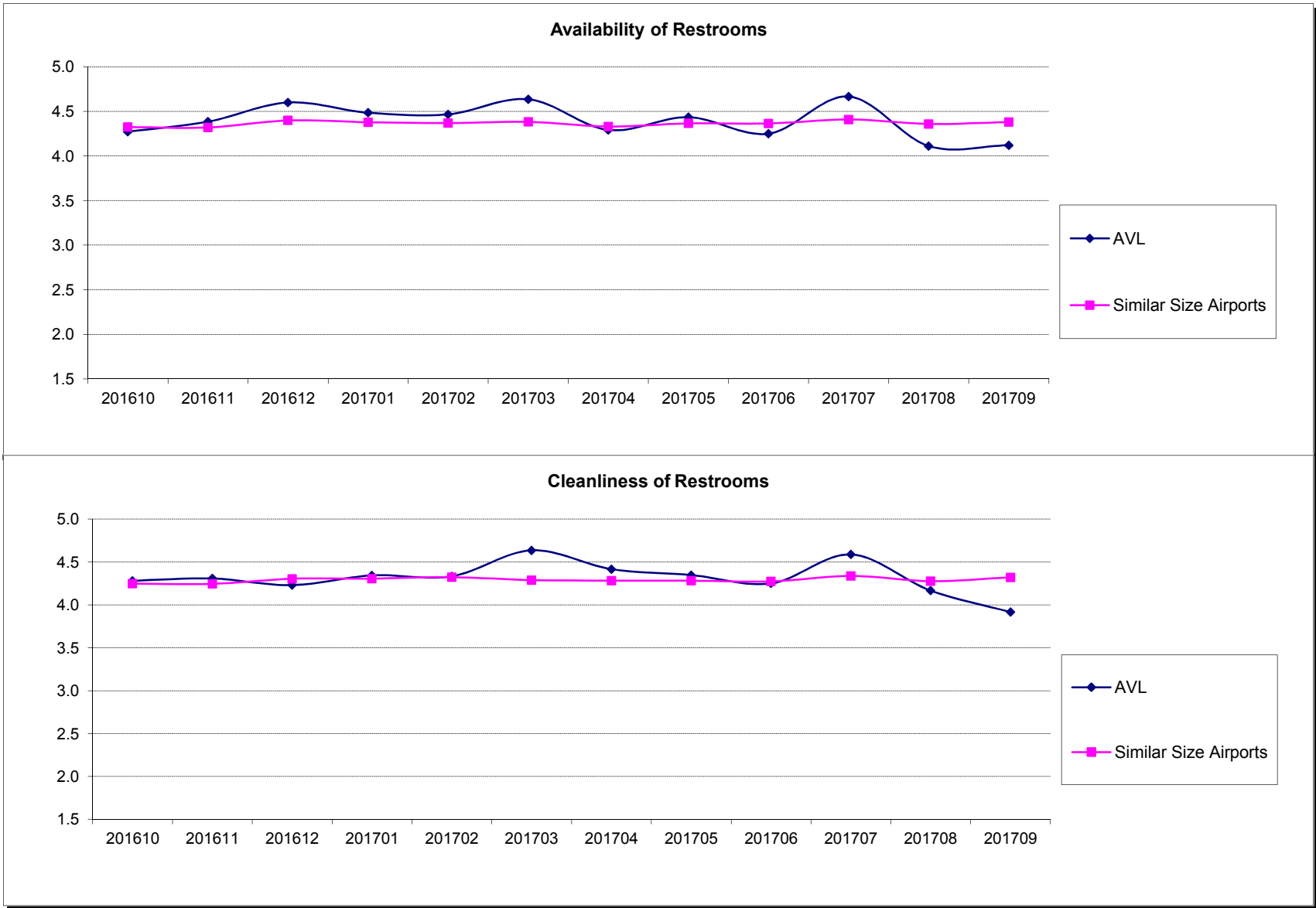


Concessions



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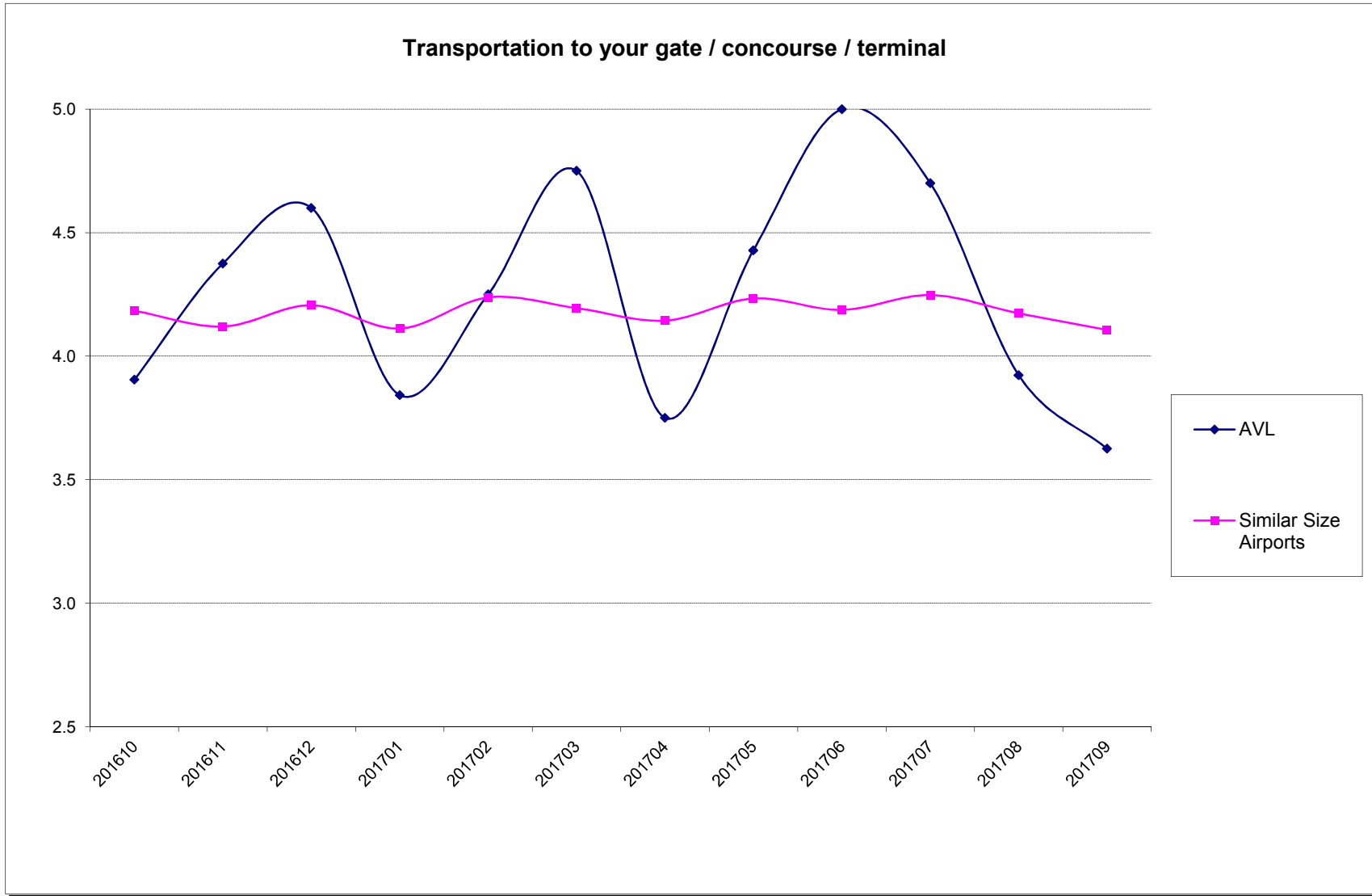
Restrooms



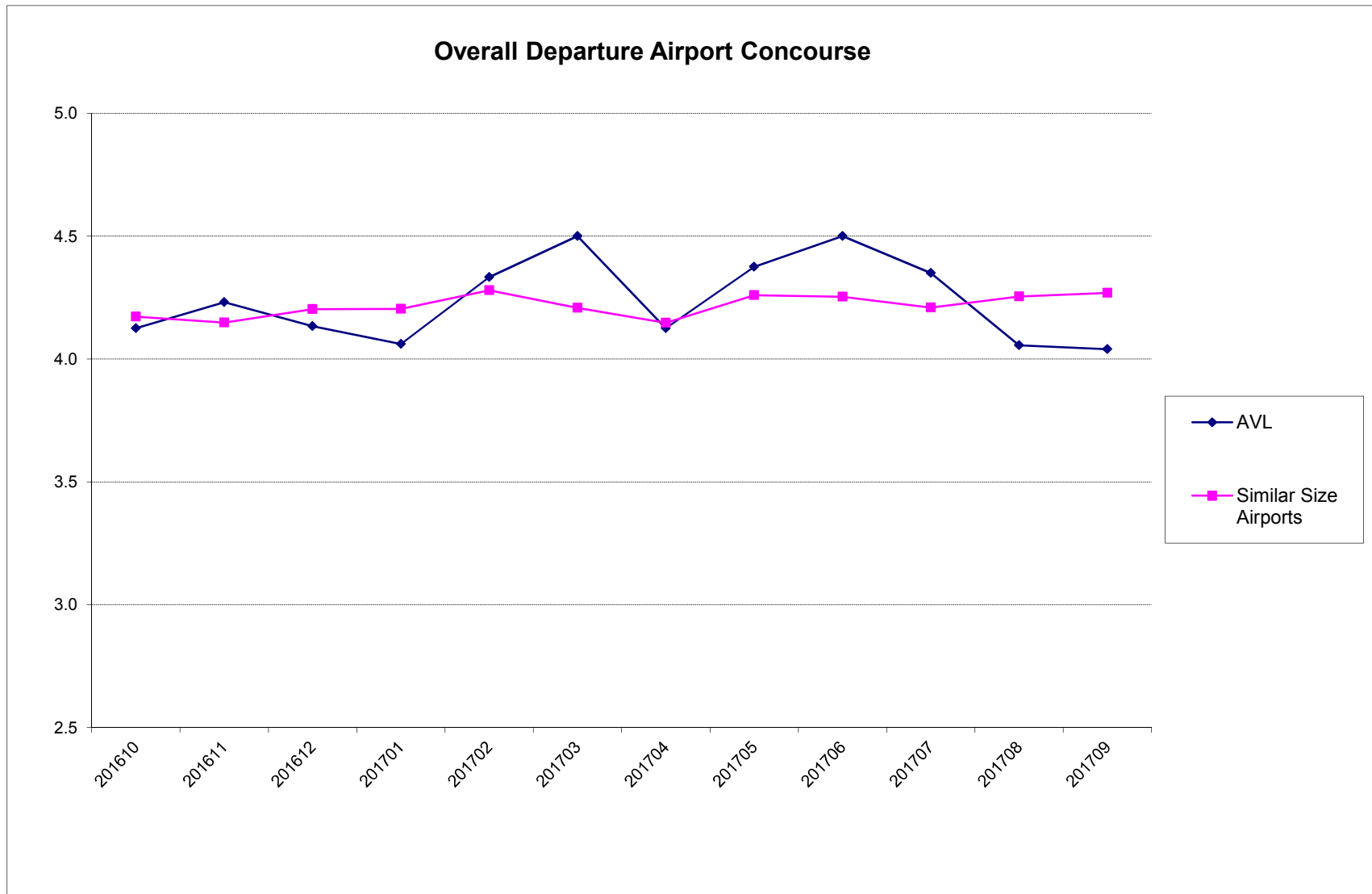
Wi-Fi



Transportation to Departure Gate

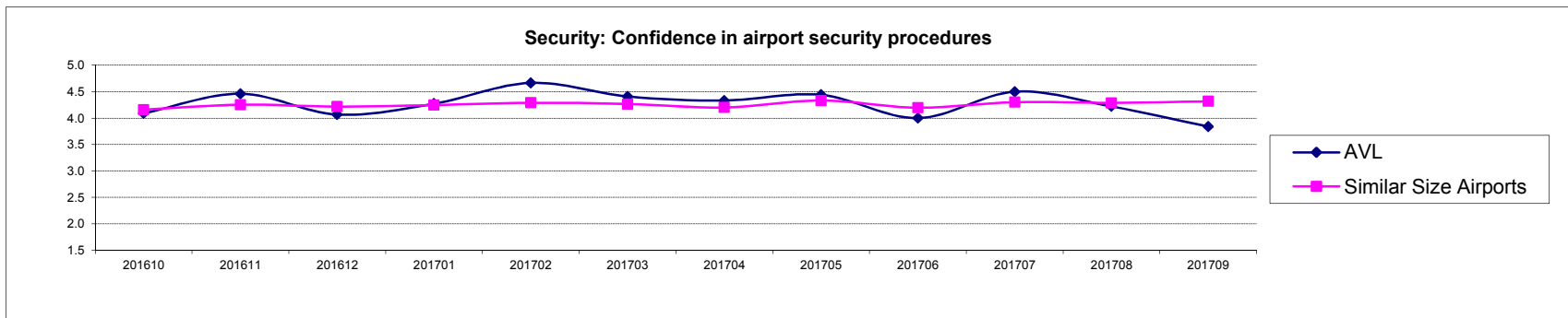
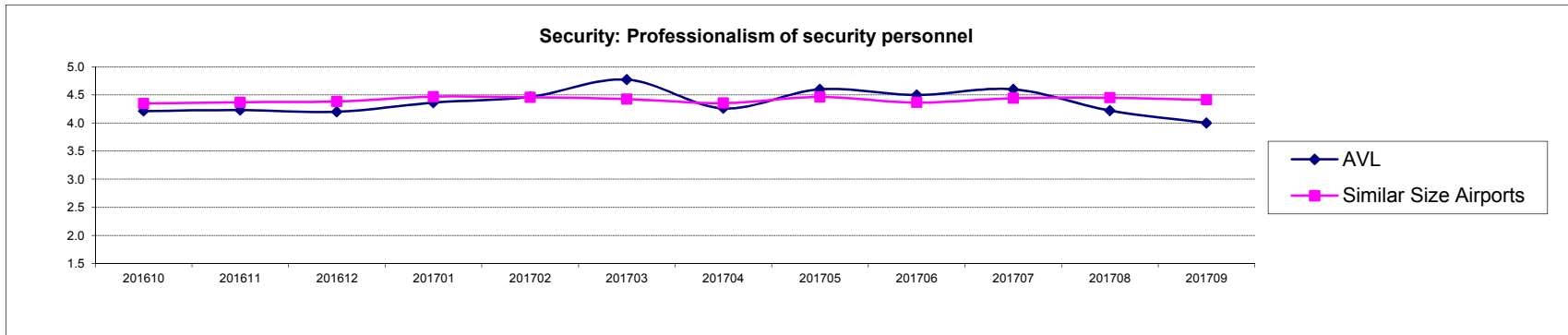
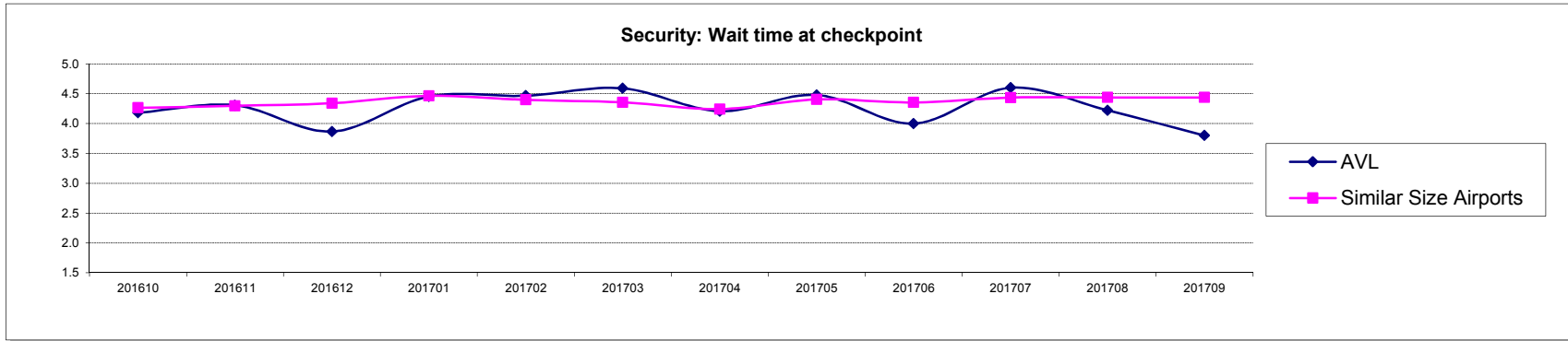


Overall Departure Airport Concourse



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Airport Security



Appendix A - Geographic Breakdown of Respondents

AZ	1	WA	3
CA	5	WV	1
CO	2	CANADA	3
DC	1	EUROPE	2
FL	24		
GA	2		
HI	1		
IA	1		
IL	1		
IN	1		
LA	2		
MA	6		
MI	3		
MN	3		
MT	1		
NC	146		
NE	1		
NH	1		
NV	1		
NY	7		
OH	1		
PA	2		
RI	1		
SC	5		
TX	6		
VA	2		

Note: Only includes passengers who indicated state of residence

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Appendix B - About Canmark

- Since 1993 Canmark Technologies has combined market research, programming, and technical expertise with thoughtful attention to client needs. Our problem-solving orientation has earned the respect of business clients and market researchers across North America.
- With an experienced staff of technical experts and project managers specializing in various fields of data capture and manipulation, programming and software development, web design and scripting, Canmark is able to leverage superior technology and know-how to support projects of all types and scope in the most cost-effective manner possible.
- Areas of expertise include survey development and delivery, project and data management services, requirements gathering, data sampling, paper and web forms management, custom lasering and printing, distribution logistics, data processing, custom programming for data cleansing, reporting and data analysis, and project consulting.
- We stand ready to meet your data needs, if you have any questions, please do not hesitate to contact us.

Appendix C - Contacts

Paul Isaacs, President
pisaacs@canmarktech.com
1-877-441-2057, ext. 11



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Key strategic priorities

Governance vs. Management : Focus on setting governing direction (“guard rails”) for the organizational and holding management accountable for the execution of operational tactics. Pursue continuous educational opportunities for Authority Member development.

1. **Organizational Relevance**: Remaining relevant in an era of airport consolidation
2. **Financial Stewardship**: Sustainability/Operating Performance/Audit & Compliance
3. **Municipal Relations**: Positive relationships with all municipalities surrounding the airport
4. **Stakeholder Relations**: Positive relationships with neighbors and other community organizations
5. **Community Image**: Public Perception/Public Relations/Customer Service/Legal Entity
6. **Facilities Stewardship**: Future Master Facilities Plan
7. **Environmental Stewardship**: Accountability/Awareness of Environmental Issues
8. **Economic Development**: Engage Community Partners/Airline Service Development
9. **Vendor-Partner Relations**: General Aviation/Rental Car Agencies/Vendors
10. **Public Safety**: Airport Emergency Safety/TSA Relations/Municipal Partners
11. **Organizational Accountability**: Executive Director Supervision